

**Porta Bella
Draft Specific Plan**

October, 1991

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Community Development Department
Lead Agency

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Owner

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1.0 THE SPECIFIC PLAN AS A PLANNING TOOL

The Specific Plan is a tool available to California public agencies to systematically implement their individual General Plans. Depending upon the intent of the Specific Plan, it may either replace or augment existing zoning regulations within the Specific Plan project area.

The Specific Plan is an excellent mechanism for California cities to ensure quality development over large areas of land. By entitling a master planned community, negative impacts associated with the alternative piecemeal or incremental development may be avoided. Elements such as open space systems, schools, parks, infrastructure and environmental preservation may be addressed by master planning an area. In addition, architectural and environmental character, as well as the elements which contribute to community pride may be woven into the concepts of the community.

The area referred to as Porta Bella within this Plan is indeed an extension of the existing fabric of Santa Clarita. As with any development, the actual construction of Porta Bella will occur with the market cycles; over a number of years. The difference is that this Porta Bella Planned Community Specific Plan is a unique opportunity to ensure the goals and objectives created within Santa Clarita's general plan are comprehensively implemented in a responsible and distinctive manner.



1.1 Legal Context of the Specific Plan

Specific Plans are established by the authority of California Government Code §65450 through §65457. Specific Plans are intended to implement the City's General Plan, therefore they may not be adopted nor amended unless they are consistent with the General Plan. That is, the General Plan document is the comprehensive "constitution" of the City and the Specific Plan is a method for implementation of that constitution. Consistency is important in that the General Plan is created by an exhaustive series of worksessions where citizens input their ideals for the City into a Plan with goals, policies and objectives.

Specific Plans are regulatory and binding, and therefore development standards adopted within the Plan replace those contained within the Municipal Code. If any conflict arises between this Specific Plan and the Municipal Code, the standard described within the Specific Plan shall rule.

1.2 Content of this Specific Plan

The California Government Code stipulates that a Specific Plan contain text and diagrams that discuss the following in detail:

Land Use: The Specific Plan must specify the distribution, location and extent of land uses, including open space, within the area covered by the Plan.

Public Facilities: The Specific Plan must show the proposed distribution, location, extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy and other essential facilities located within the area covered by the Plan and needed to support the land uses described in the Plan.

Development Standards: The Specific Plan also must include standards and criteria by which development will proceed, including standards for the conservation, development and utilization of natural resources, where applicable.

Implementation Measures: The Specific Plan must include a program of implementation measures, including regulations, programs, public works projects and financing measures.

Consistency with the General Plan: The Specific Plan must include a statement of relationship of the Specific Plan to the General Plan.

The Specific Plan may also address any other subjects which in the judgement of the Planning agency are necessary or desirable for implementation of the General Plan.

In order to obtain approval of local public works projects, tentative or parcel maps, and zoning ordinances within the Specific Plan area, those projects must be consistent with this Specific Plan.

1.3 Guide to the Porta Bella Specific Plan

Porta Bella means “beautiful door,” which is reflective of the Plan area’s relationship to the community, and the region. The Porta Bella Specific Plan document is intended to put forth the development concepts for this project area in a simple, rational manner. The following discusses the logical order of the Plan, as well as briefly describing the intent of each Section.

Section 1.0 - The Specific Plan as a Planning Tool

This Section defines the purpose and opportunities of this Specific Plan, as well as briefly describes the framework of this Plan.

Section 2.0 - Community Context & Integration

Describes the context of Porta Bella within region and community, and its relationship to the City General Plan's Valley Center area. This Section also discusses the existing conditions of the project site.

Section 3.0 - Master Land Use Plan

Establishes the distribution and general character of land uses within the Specific Plan area. Describes the planning areas of Porta Bella by District and land use designation.

Section 4.0 - Transportation & Circulation

Describes the transportation and circulation patterns within the community by introducing numerous forms of mobility, including an innovative commuter rail station and a unique "people-mover" escalator system. A significant step forward to the solution of the regional traffic problem is also offered in this section.

Section 5.0 - Master Landscape Plan

Illustrates and discusses the master landscape plan. The landscape of Porta Bella is based upon traditional, drought tolerant California landscape designs and materials which will reference the natural beauty of the Santa Clarita Valley.

Section 6.0 - Land Use & Development Standards

Outlines the development and land use standards for Porta Bella. These standards will further delineate and implement the community concepts described in earlier sections.

Sections 7.0, 8.0 and 9.0 - Parking, Signage & Lighting Standards

Respectively discusses and appropriately regulates the parking, signage and lighting components of Porta Bella.

Section 10.0 - Infrastructure & Public Facilities

Describes the infrastructure and public facilities which will be necessary to service Porta Bella and the surrounding community in a responsible manner.

Section 11.0- Design Review

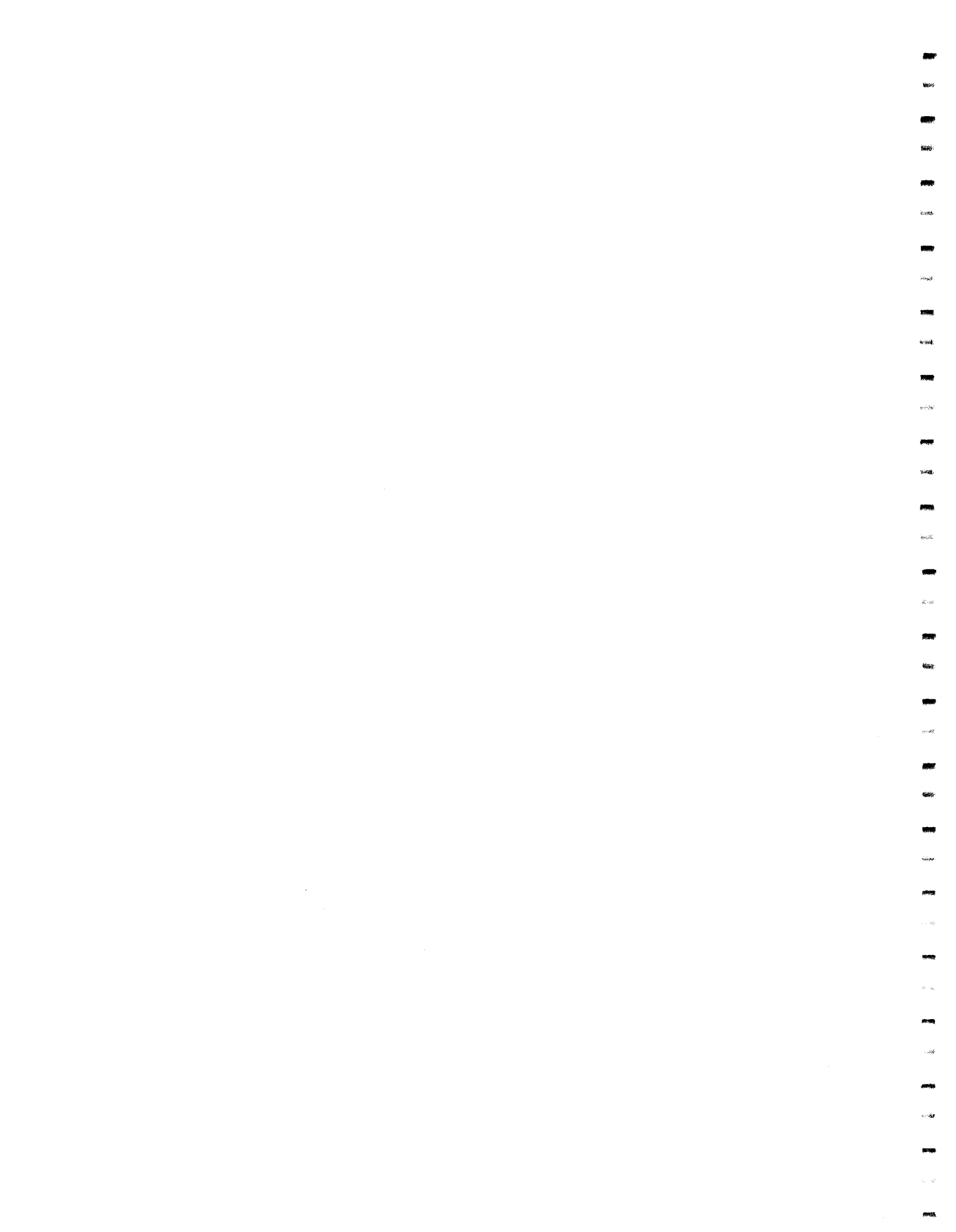
Establishes the public design review process for subsequent developments within Porta Bella. This is to ensure the design integrity described in this Plan is faithfully implemented over time.

Section 12.0 - Implementation

Outlines the implementation, financing and maintenance methods and procedures for the Specific Plan. This section also discusses how the elements of Porta Bella relate with the City of Santa Clarita's General Plan.

Section 13.0 - Environmental Impact Report

Contains the Environmental Impact Report (EIR) prepared for this project. This EIR discusses the proposed project, its impacts, and recommends mitigation measures for the impact. Many of the impacts have been mitigated by the thoughtful approach to the community from the earliest stage of design, and are incorporated within this Specific Plan.



1.0 *The Specific Plan as Planning Tool*

2.0 *Community Context & Integration*

3.0 *Land Use Master Plan*

4.0 *Transportation & Circulation*

5.0 *Master Landscape Plan*

6.0 *Community Design & Land Use*

7.0 *Parking Regulations*

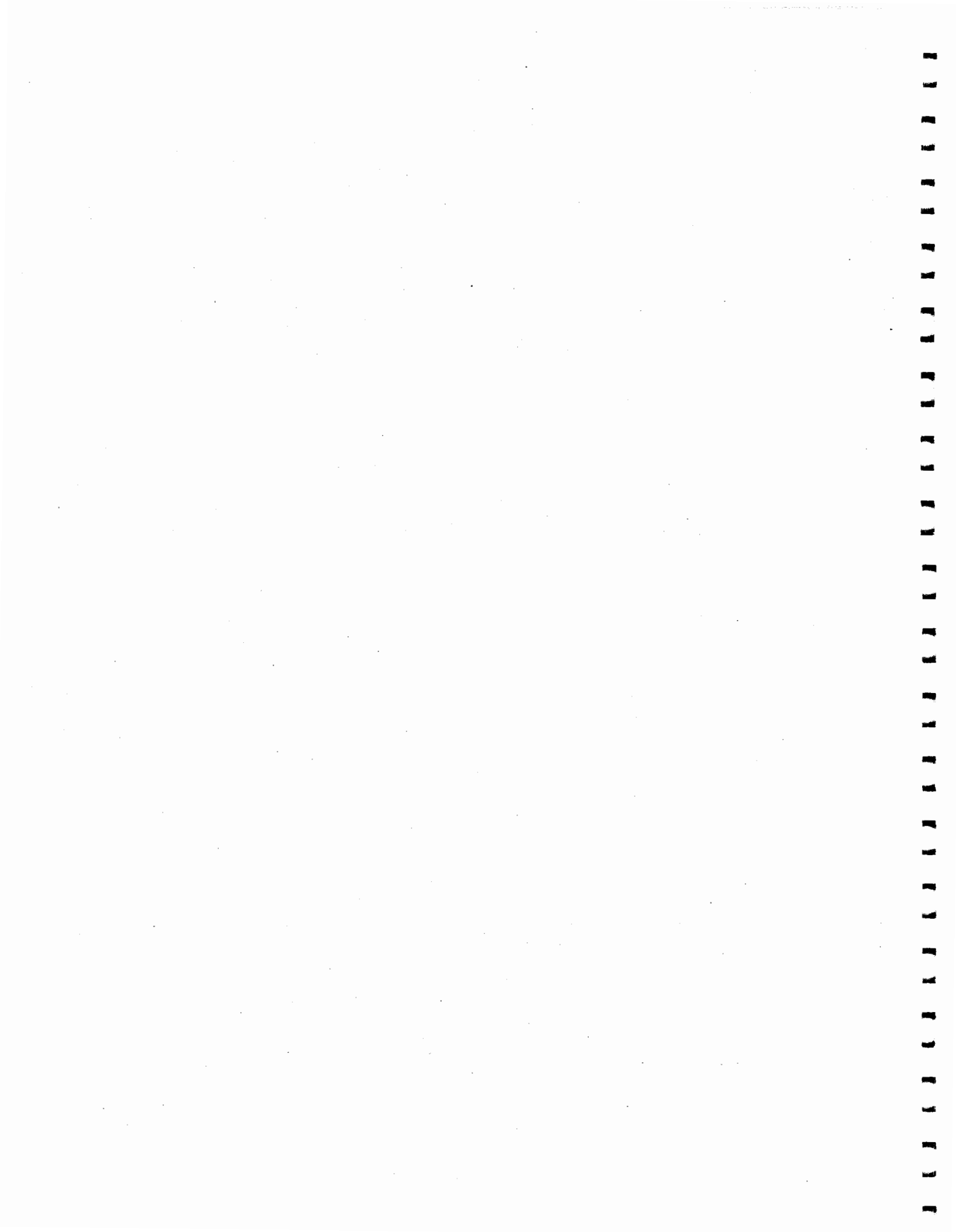
8.0 *Sign Regulations*

9.0 *Community Lighting Regulations*

10.0 *Infrastructure & Public Utilities*

11.0 *Design Review*

12.0 *Implementation*



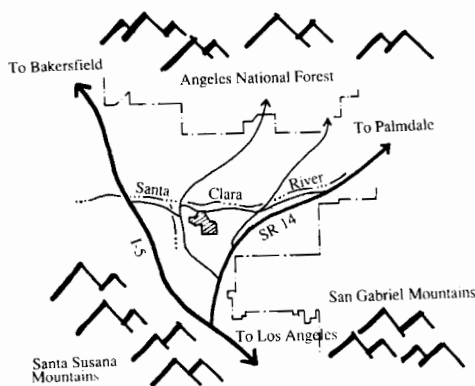
2.0 COMMUNITY CONTEXT & INTEGRATION

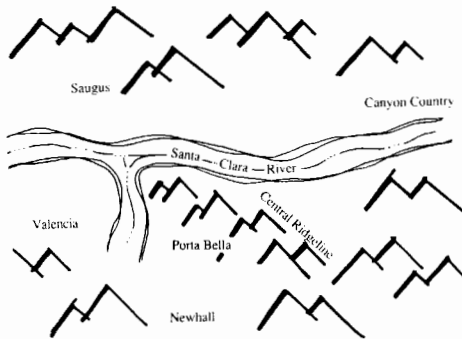
On December 15, 1987, the four communities which comprised the majority of the Santa Clarita Valley within the County of Los Angeles incorporated to become the City of Santa Clarita. One of the ideals behind this event was to gain local control of development within the Santa Clarita Valley. According to Santa Clarita's General Plan, total population within the valley is currently 150,000, and anticipated to reach over a quarter million residents within 20 years.

The draft General Plan for the City of Santa Clarita recognizes that growth in Santa Clarita is a part of a much broader trend. People are selecting to live in communities like Santa Clarita because it offers an alternative to the increasingly urbanized Los Angeles basin. The Santa Clarita General Plan charts a course to accommodate the anticipated growth of the of the Valley's population, as well as the resulting increase in influence this City will have as a place of regional importance. This Specific Plan will help to ensure the "charted course" is realized in a comprehensive manner, rather than by a series of smaller, unrelated developments.

2.1 Regional Setting

The City of Santa Clarita is located approximately 35 miles north of downtown Los Angeles, 70 miles south of Bakersfield and 40 miles east of the Pacific Ocean. The San Gabriel and Santa Susana mountain ranges separate the valley from the San Fernando Valley and the Los Angeles Basin to the south, and from the San Joaquin Valley, Mojave Desert and Angeles National Forest to the north. The Santa Clarita Valley is generally circumferenced by the Los Angeles National Forest.





The Santa Clarita Valley is an irregularly shaped area, roughly 500 square miles in area. The Valley is bisected by a discontinuous ridgeline which runs centrally through the valley from southeast to northwest. The Porta Bella Specific Plan area is located on this discontinuous ridgeline, near its terminus at the intersection of San Fernando and Soledad Canyon Roads. The City of Santa Clarita, comprises roughly 150 square miles within the heart of the valley.

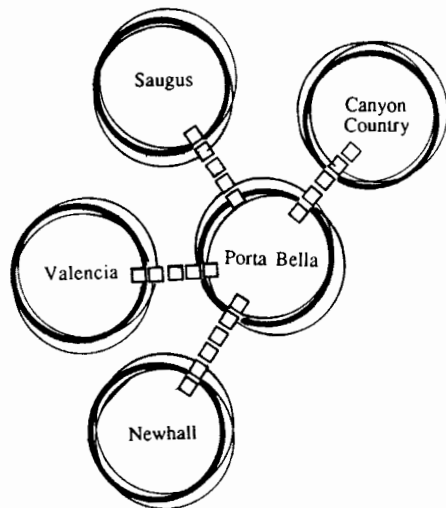
2.2 The Santa Clarita Communities

Gold was discovered in the Santa Clarita Valley in 1842, and with it came a population of minors, shopkeepers, farmers and settlers that began in earnest the settling of the Santa Clarita Valley. They brought with them their varied cultures and architectural ideas. In the 1870's, commercial oil wells and refineries were developed, which further rooted settlement in the Santa Clarita Valley. In the early 1900's, the infantile Hollywood movie studios utilized the rugged setting for Western movies. Four communities evolved from this influx of people to the area.

The following are excerpts from the City of Santa Clarita General Plan regarding the communities:

Newhall: Newhall is the earliest permanent settlement in the Valley, established in 1876 in conjunction with the construction of the Southern Pacific Railroad. This area was a typical Western town of oil, mining, and railroad workers. Large stands of oak trees are scattered throughout the community. Placerita Canyon is also a portion of this area.

Valencia: The community of Valencia is part of the original 37,500 acre Newhall Ranch, a Mexican land grant now owned by the Newhall Land and Farming Company. Named after its sister City in Spain, Valencia was dedicated in 1965 and developed as a planned community.



Saugus: Established in 1887, Saugus, named for the Massachusetts birthplace of founder Henry Mayo Newhall, also owes its existence to the Southern Pacific Railroad. The Saugus Speedway, a popular attraction and site of Saugus' famous Sunday Swap Meet, was originally designed in 1924 as a rodeo arena. The last great train robbery in the state took place behind the grandstands in 1928.

Canyon Country: With more than 30,000 residents, Canyon Country has the largest population of any community in the City. Canyon Country includes the area along Soledad Canyon Road east of Saugus and Friendly Valley. It contains a wide range of housing types. Commercial and manufacturing activities are concentrated along both sides of Soledad Canyon Road and along northern Sierra Highway. An industrial hub is also located in the Hornby and Golden Triangle areas. Sand Canyon is also a portion of this area.

The four existing communities are generally located around the central discontinuous ridgeline, creating an undeveloped center of the City. It is within this center area that the 996 acre Porta Bella extension to the communities of Santa Clarita is planned.

2.3 Existing Conditions of Porta Bella

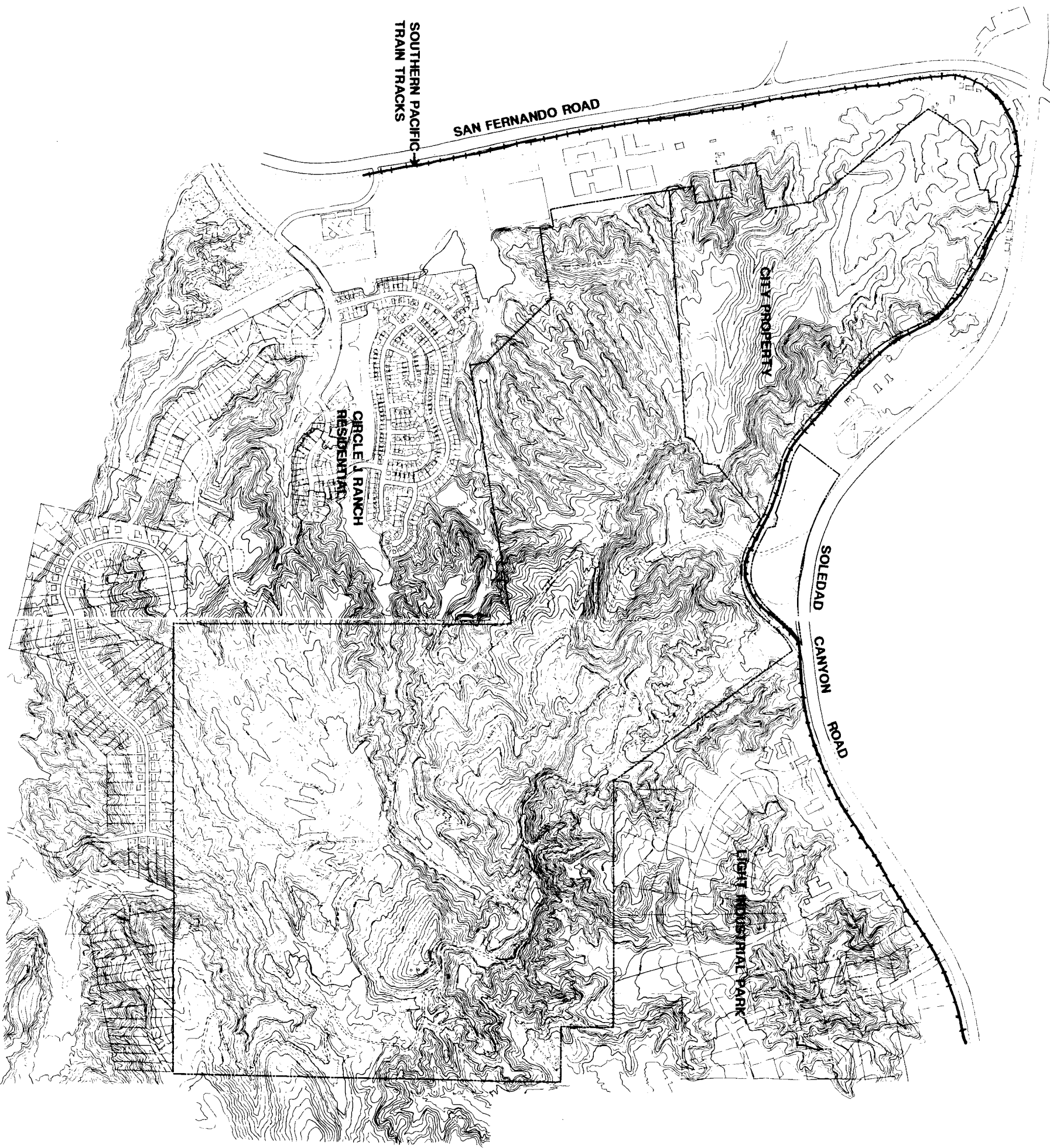
Existing Land Use & General Plan Designation - The 996 acre project site is largely vacant, with several buildings remaining utilized by the Whittaker-Bermite Company for site maintenance, security and offices. Most structures on the site have been torn down, or stand vacant and dilapidating. The existing buildings are within the lower elevation area near Soledad Canyon Road, as well as in the adjacent canyon area to the south. The area next to the Saugus Speedway is currently utilized for parking for activities at the Speedway.

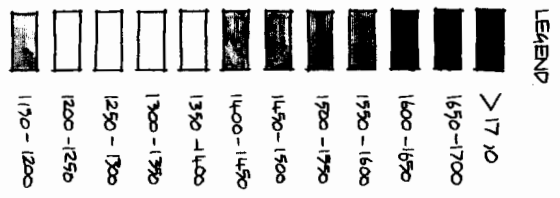
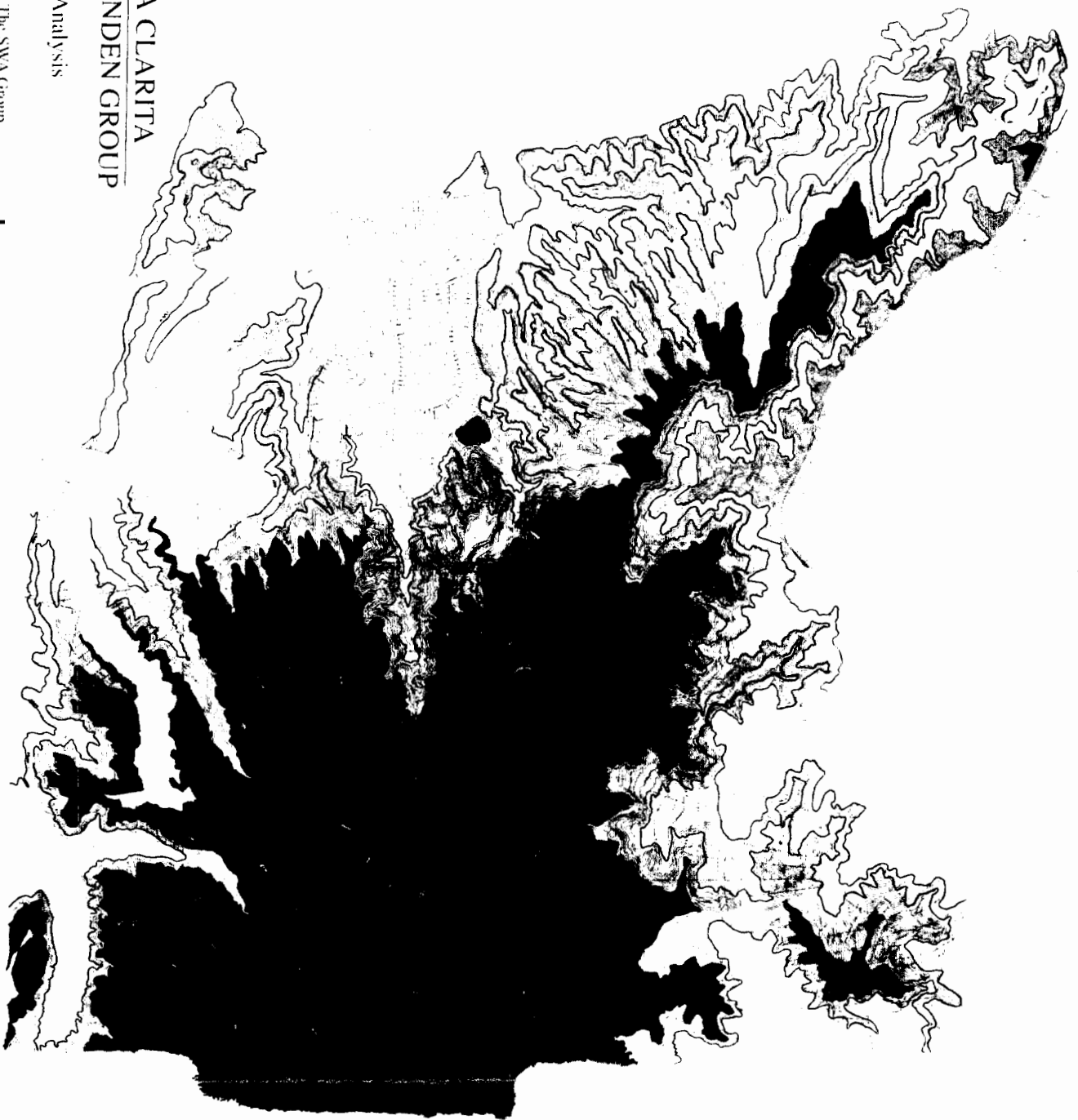
The City of Santa Clarita General Plan states regarding the Porta Bella project area:

This area of land consists of approximately 1,000 acres of land located in the geographic center of the planning area. The area is currently vacant and has environmental problems associated with contaminated soils which is currently undergoing corrective measures. A substantial opportunity exists for the City and the property owner to create a City focus of a mix of uses including but not limited to specialty retail commercial, office park, low to high density housing, interconnected open space and greenbelts, educational facilities, a commuter rail station, and public parks. Care must be taken to assure that traffic, visual, and other environmental impacts are properly mitigated and that the ultimate plan is in accordance with the goals and policies of the General Plan.

Topography - Topographically, Porta Bella can be characterized as an elevated plane, rising gradually from the river valley and proceeding north easterly to the ridgeline. From the ridgeline, it drops off rather dramatically to meet the lower river valley elevation. Most of the project area is located to the west of this ridge. To the interior of the project area, smaller multi-directional undulations project throughout the site.

From the exterior of the project, the interior is nearly non-visible, in that it is largely surrounded by steep slopes and ridges. The topographical map and the contour analysis maps illustrate the character of the existing landform.





SANTA CLARITA
THE ANDEN GROUP

Contour Analysis

Prepared by The SWA Group
May, 1990

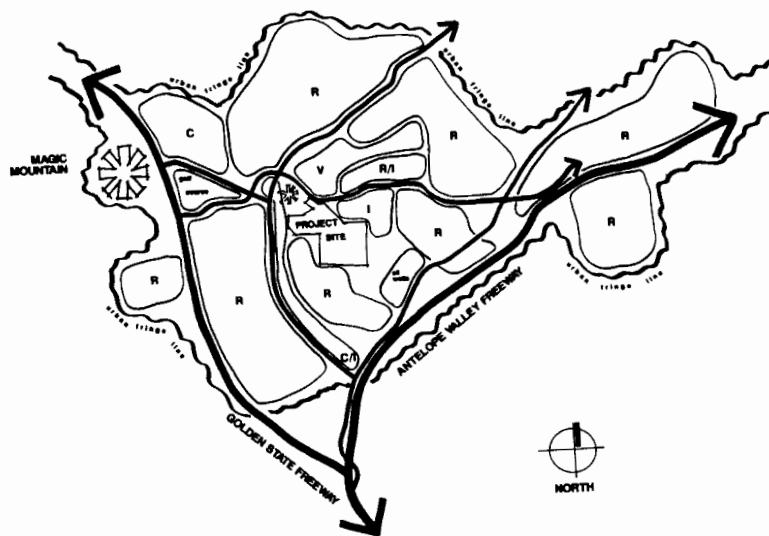


Previous Use - The Whittaker-Bermite Company operated the property from 1906 (Halifax, Bermite) until 1987 as a military explosives and flare manufacturing site. Some limited testing of the products were conducted on-site, and these areas of testing have been dismantled and inspected. Some materials associated with the use which are considered hazardous were utilized on-site. Of the 14 hazardous material sites, all but five have been cleaned up. Two of these sites require on-going monitoring, while the remainder are nearly cleaned. Detailed description of these sites and their mitigation process is contained within the attached Environmental Impact Report (EIR).

As a result of the previous use, much of the site's interior is severely damaged due to grading for roads, building sites and fire breaks. The ridgeline which traverses the site is scarred from grading, as well as many areas of the exterior oriented slopes.

Biology - Most of the vegetation on-site have been disturbed to a degree. Much of the vegetation has been removed, with limited pockets of trees to the interior of the site. Most of the trees, including oaks, are found on the exterior slope banks of the site. Within this Specific Plan and the attached EIR is further discussion which details the existing conditions, project impacts and proposed mitigations for the biology and wildlife found on site.

Surrounding Land Uses - North: A portion of the area north of the project site is the City owned "City Hall" property. This 237 acre parcel is topographically related to the proposed Porta Bella area, as it is also located above the surrounding river valley. The City property is the point of land generally located at the south-east intersection Soledad Canyon and San Fernando Roads. The edge slopes of this land is largely visible in Santa Clarita, with the less visible interior comprised of oak grasslands and rolling meadows. The Saugus Speedway is also to the north of the property, at the lower river valley level. The Southern Pacific Railroad is located between the northern edge of the site and Soledad Canyon Road. On the opposite side of Soledad Canyon Road is the Santa Clara River Plan's proposed greenbelt.



East: To the east of the project area is a partially developed business park. This development is located near Soledad Canyon Road, at the base of the ridge which rises up to the proposed development area. Most of the area east of the project site is vacant.

South: To the south of the project area is a mix of land uses, ranging from undeveloped land area, to oil fields, to a single family housing. The Circle J housing area shares much of the southern boundary of the project area.

West: To the west of the project area is the San Fernando Road redevelopment area, which is characterized by industrial and service land uses. Development along San Fernando Road was done incrementally over the years, and lacks unified character, order or aesthetics. Again the Southern Pacific Railroad is located between Porta Bella and the arterial (San Fernando Road). The Santa Clara River is located to the west of San Fernando Road.

Opportunities and Constraints Summary: The opportunities and constraints diagram summarizes the relationships of the elements discussed above with each other and the site. This diagram depicts several aspects of the site, being:

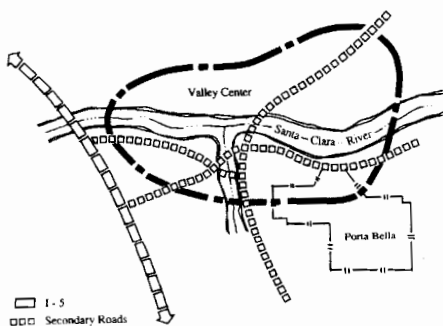
- Ridgeline
- California aquaduct
- Seismic setback
- Major slopes
- Vegetated areas
- Natural gas easement
- MWD water tunnel easement
- Existing trees & vegetation
- Heavily disturbed areas
- Developable areas

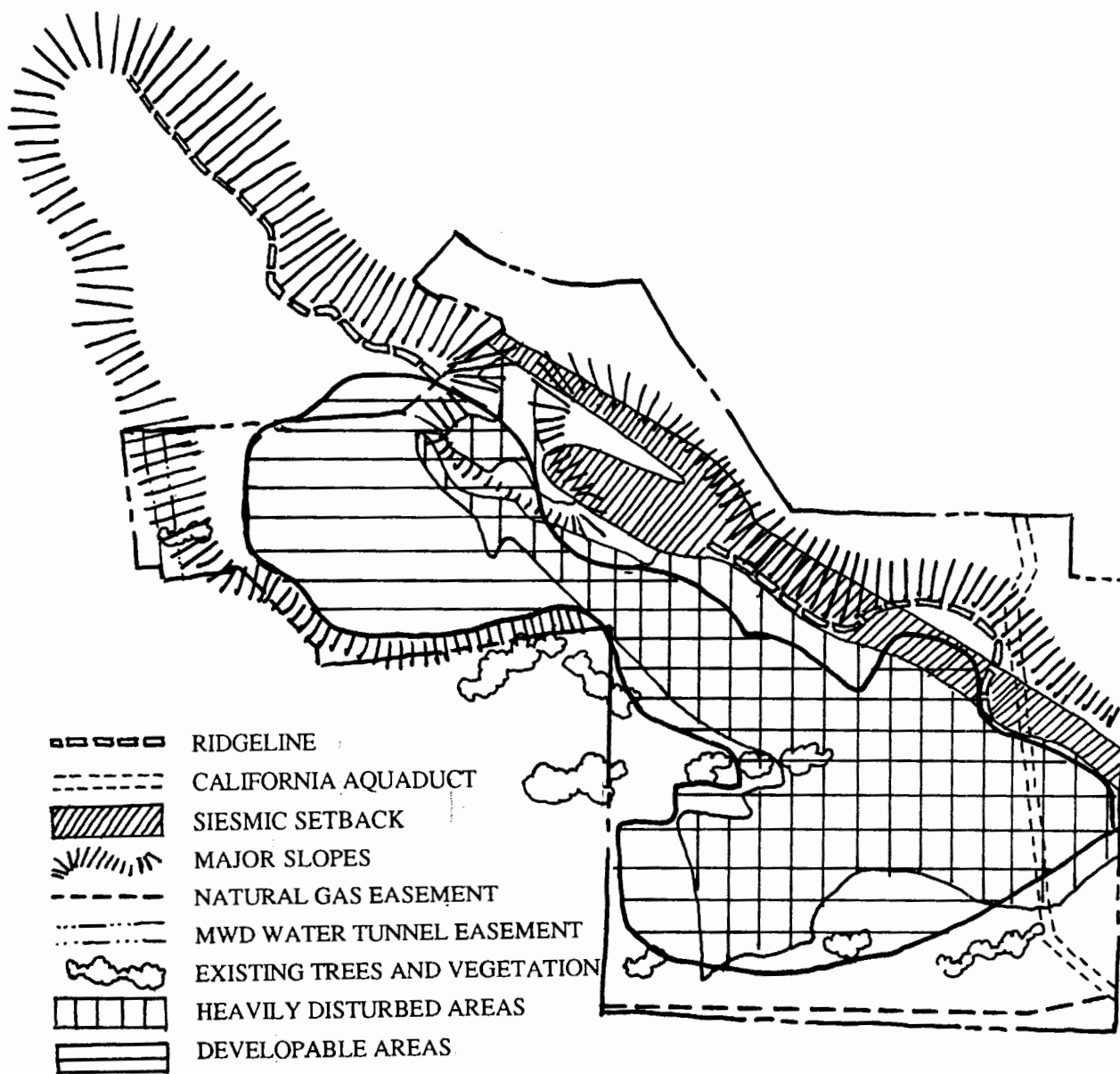
2.4 The Valley Center

The Porta Bella project area is, in part, a portion of a larger, relatively undeveloped special environment area identified by the City General Plan as the "Valley Center." This area extends roughly from Six Flags Magic Mountain on the west to the Golden Triangle area on the east generally between Valencia Boulevard and Newhall Ranch Road.

The City of Santa Clarita General Plan has stated about the Valley Center planning area:

Today the Valley finds itself with an outstanding opportunity to plan its center. Large blocks of vacant or under-utilized land in the core area of the City remain available. Unplanned and piecemeal development has not yet presented the ability to create a Valley Center. Development of a concept and direction from this area as described in this (General) Plan is essential now, before random development severely limits available opportunities.





The concept has been developed to build a Valley identity and unify the communities by designating the central "core" of the Santa Clarita Valley. The concept designates the central core area as the Valley's only higher density area for both residential and commercial office uses, thus maintaining lower profile, less intensive uses in the surrounding communities.

The concentration of higher intensities and densities of uses into a Valley Center Concept and only in the Valley Center area will foster City identity while at the same time preserve and protect outlying areas from overly intensive development. By focusing compatible higher intensities and densities of uses into the Valley Center and allowing mid-point densities or less elsewhere in the planning area, the Valley Center Concept becomes a growth management tool while achieving greater environmental quality objectives, both within and outside the Valley Center area.

The Valley Center area which intermingles with the Specific Plan area is called the "Civic Center/Entertainment/Information Component" by the City's General Plan. Within this area, the City of Santa Clarita General Plan envisions the following for this area:

This area is located generally southeast of Bouquet Junction and incorporates the City's "City Hall" property, a portion of the Bermite (Porta Bella) property, and surrounding commercial street frontages. Starting from the assumption that the City's future City Hall and Civic Center will be located here, a theme of Information and Government was developed. The type and variety of uses which relate to the City Hall/Civic Center and information center theme would include libraries, museums, cultural arts centers, education, conference centers, communication centers, government, and other offices and services grouped together with an interesting mix of shops, restaurants, plazas, and other uses in a pedestrian oriented village center. The concept envisions this mix clustered on the upper elevations of the property, commanding exceptional views of the Valley.

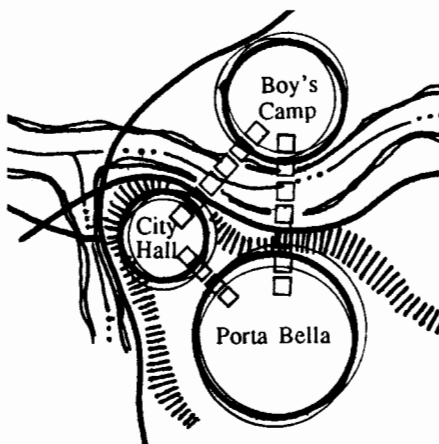
On the lower level of this area along Soledad Canyon Road, anchoring the eastern end of an office/financial corridor, would be an exciting mix of entertainment oriented uses, including a major restaurant cluster, performing

arts center, theaters, nightclubs, and other day and night-time activities. At this location would be a multi-modal transit center containing the commuter rail station and a major stop for a light transit loop system serving the entire Valley Center as well as the Valencia Industrial park and Six Flags Magic Mountain. Office and employment uses should also be attracted in this location due to its superior transit accessibility. An escalator system could be constructed to connect the two elevations of this site and further strengthen the attraction and theme.

It should be noted here, as with all the other Valley Center components, that inclusion of higher density residential development within and along the edges of the Valley Center and its components are an essential ingredient to the vitality and success of the Center. As such, higher density residential should be incorporated, even encouraged, in and around all these areas. Higher density could mean up to 50 units per acre provided that the development accommodates City goals and objectives.

The relatively undeveloped nature of the Valley Center and the closely related Porta Bella area offers the opportunity to plan meaningful systems for public open space, mobility, infrastructure, and recreation, as well as a special mixed-use Town Center planning concept. These systems will be discussed in further detail within this Plan.

2.5 Physical & Visual Connections



Because of the unique opportunity to plan an integrated mixed-use "Center", the City has suggested that the project site and the City owned property be planned in a coordinated manner. The City has also pointed out the opportunity to integrate other visually connected properties which surround both sites. One of these opportunities relates to the visual relationship of the Bouquet Canyon Boy's Camp to these properties.

Currently, physical linkage between these areas from the project site does not exist due to the separation created by the river valley, the wide and busy Soledad Canyon Road, the Southern Pacific Rail tracks, and the steep hillsides which lift the project site up from the Valley floor. However, it is these same topographical characteristics which create a strong visual connection between these parcels of land.

The design proposed by this Specific Plan suggests the possibility of both visually and physically connecting the river valley and the three related parcels of land through a coordinated series of land uses, connected pedestrian trails and an environmentally compatible landscape plan. An objective of this Plan is to establish an overall environmental design character for the entire Valley Center as well as a setting for the Town Center. This will require coordination with both public and private support.

2.6 Valley Center Design Character



The Santa Clarita General Plan suggests the Valley Center character should be based upon existing environmental conditions, creating a distinctive central community embodied with elements which will unify the undeveloped properties and the surrounding communities, yet not detract from the individual community attributes. One potential element to accomplishing this objective is that this character be established by incorporating and reestablishing the aesthetic environmental conditions withing the Valley.

Beginning with a historical perspective of the Santa Clarita Valley's environmental condition, it is apparent that the Valley could lose some of the natural elements which comprise a major component of the Valley's charm. Areas of the Santa Clara River banks have been degraded with concrete embankments, some hillsides have been graded with little respect to landform, and the oak trees which were once abundant are now more rare, and therefore more precious.

The City of Santa Clarita is correcting these less sensitive development methods by preparing laws which ensure that development occurs in better harmony with the living environment. These steps toward better planning are seen in the City's new General Plan, Zoning Code, and through ordinances like the Oak Tree Preservation and the Ridgeline Protection ordinances.

The Vicinity Landscape Character which is a part of the Master Landscape Plan (Section 5) of this Plan incorporates and builds upon the environmental character goals described in the City's General Plan and River Study. This is done by establishing four special environment treatments based upon the natural landscape character. Those treatments are:

- The Riparian River Treatment;
- The Oak Woodland Treatment;
- The Oak Grassland Treatment;
- The Natural Ridge Treatment.

Each of these treatments has associated with it a special landscape which is based upon the natural landscape of the Santa Clarita Valley. The intent of identifying these treatments is to enable an underlying, unifying City image based upon the natural environ-

ment to be implemented by the City of Santa Clarita. That is, to help reinforce the City's image upon its natural landscape qualities, instead of relying solely on architectural themes.

These four special environment treatments are discussed in more detail within the landscape section of this Plan.

2.7 Porta Bella Community Systems

Some places are better to live in than others. In planning the living environment within the Specific Plan area, several elements (systems) have been recognized as influences in creating the desired living environment. It is the intent of this Plan to address the components of each element as separately and together. These interrelated systems are:

- The Natural System
- The Mobility System
- The Lifestyle System

2.7.1 The Natural System

Good places to live almost always have a "natural system" accessible to the residents. This system may range from rugged wilderness to formal courtyards, depending upon the setting of the development and the desires and special needs of the residents. Within Porta Bella, the intended natural system is one which compliments the established oak-grassland character of the Santa Clarita Valley. The natural system will include athletic parklands, riparian wetland and oak grassland areas, and trails for hikers, mountain bikers, and equestrian enthusiasts.



The natural open space system will always be an integral component of life at Porta Bella. In addition, the Open Space areas will provide foreground and backdrop when viewing Porta Bella. The surrounding natural hillsides will be preserved, as well as the central ridgeline. The Natural System woven into the community will enable continuous interaction with the natural environment.

2.7.2 The Mobility System

It is recognized within the design of this Plan that the vehicular system must be accommodated in a convenient and safe manner. However, one of the objectives in designing the circulation & transportation facilities for Porta Bella was to provide meaningful alternatives to use of the automobile, as well as reducing the number of trips and distances necessary to travel when utilizing the automobile. To implement this goal, the Plan proposes a more pedestrian Town Center oriented with a closer (fine grain), mixture of land uses. In addition, streets are of the size and arrangement to allow people to enjoy walks and bicycle rides without feeling threatened by the automobiles. In Porta Bella, the size of neighborhoods, the scale of the streets, and the character of the landscape have all been interwoven to create an enjoyable pedestrian and street level experience. Porta Bella will be a community that “feels right” when visiting or residing there.



In the contemporary sense, the automobile's movement in and out of the community will be accommodated by the following elements:

- Regional road system circumferencing the community.
- Community road systems serving the inter-neighborhood travel within the community

- Neighborhood road system serving the intra-neighborhood travel within the various neighborhoods
- Convenient on-street and off-street parking for all land use areas.

In the traditional sense, mobility will be accommodated by:

- Interconnected neighborhoods and land uses, which will promote walking between communities and uses.
- Traditional sidewalk layout and landscape design with people in mind. Logical linkages, parkways with trees, minimal conflict with autos, reasonable street widths.
- Direct relationship and mixture of land uses, which will enable a pedestrian alternative to the automobile for basic services. This will also create a more pedestrian size for the neighborhood blocks within the neighborhoods.



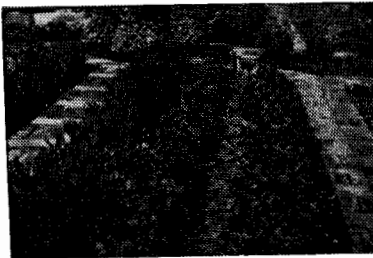
Land uses within the community are arranged for ease of mobility and interaction. Some of the areas enjoy a mix of uses, to create a vibrant and energetic town. Residential neighborhoods interact with one another, creating a sense of community. Private gated communities should not conflict with this interactive nature of Porta Bella. This mix of land uses is evidenced within all of the districts within the community, however it is most pronounced within the Town Center district. Here, a range of residential products relates to and intermingles with non-residential uses such as commercial, civic, service and entertainment. The Town Center area will offer an opportunity for residents to live, work and relax in a dynamic, affordable environment.

Employment will be created for the Santa Clarita Valley, as well as the first light rail commuter system to link Santa Clarita to Los Angeles and Long Beach. No longer will the automobile be the only alternative for people to visit these areas. This multi-modal transportation system will accommodate the commuter rail, buses,

automobiles, bicycles and pedestrians. A very significant component of mobility in Porta Bella will be the special escalator system which will connect the multi-modal transportation system with the Town Center. This is especially notable because it will be one of the first Cities in California to tap the market serviced by the expanding rail system.

2.7.3 The Lifestyle System

The Lifestyle System is a concept which recognizes that an increasing amount of people desire more than the basic necessities or standard amenities to enjoy a “livable environment.” The lifestyle system suggests a more environmental and socially oriented way of life is possible and desirable in contemporary development. A goal of this Specific Plan is to allow and encourage environmental and socially conscious practices to be conducted within Porta Bella. Another goal is to create a sense of community pride and interaction within Porta Bella. Social barriers such as walls, cul-de-sacs and auto dominated streets have been replaced in Porta Bella with open, interactive neighborhoods supported by human-scale rights-of-way and buildings. The Porta Bella lifestyle will allow individual expression, social interaction and environmental protection.





- 1.0 *The Specific Plan as Planning Tool*
- 2.0 *Community Context & Integration*
- 3.0 **Land Use Master Plan**
- 4.0 *Transportation & Circulation*
- 5.0 *Master Landscape Plan*
- 6.0 *Community Design & Land Use*
- 7.0 *Parking Regulations*
- 8.0 *Sign Regulations*
- 9.0 *Community Lighting Regulations*
- 10.0 *Infrastructure & Public Utilities*
- 11.0 *Design Review*
- 12.0 *Implementation*



Illustrative Plan
The Anden Group
Prepared by The SVA Group
August 1991
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3.0 LAND-USE MASTER PLAN

Porta Bella is a hillside community, reflective of turn-of-the-century California in urban form, architecture, and landscape. This imagery revisits the heritage of Southern California and the Santa Clarita Valley. This new central community is based upon proven planning principles. The result will be an enduring “hometown” setting with distinctive character.

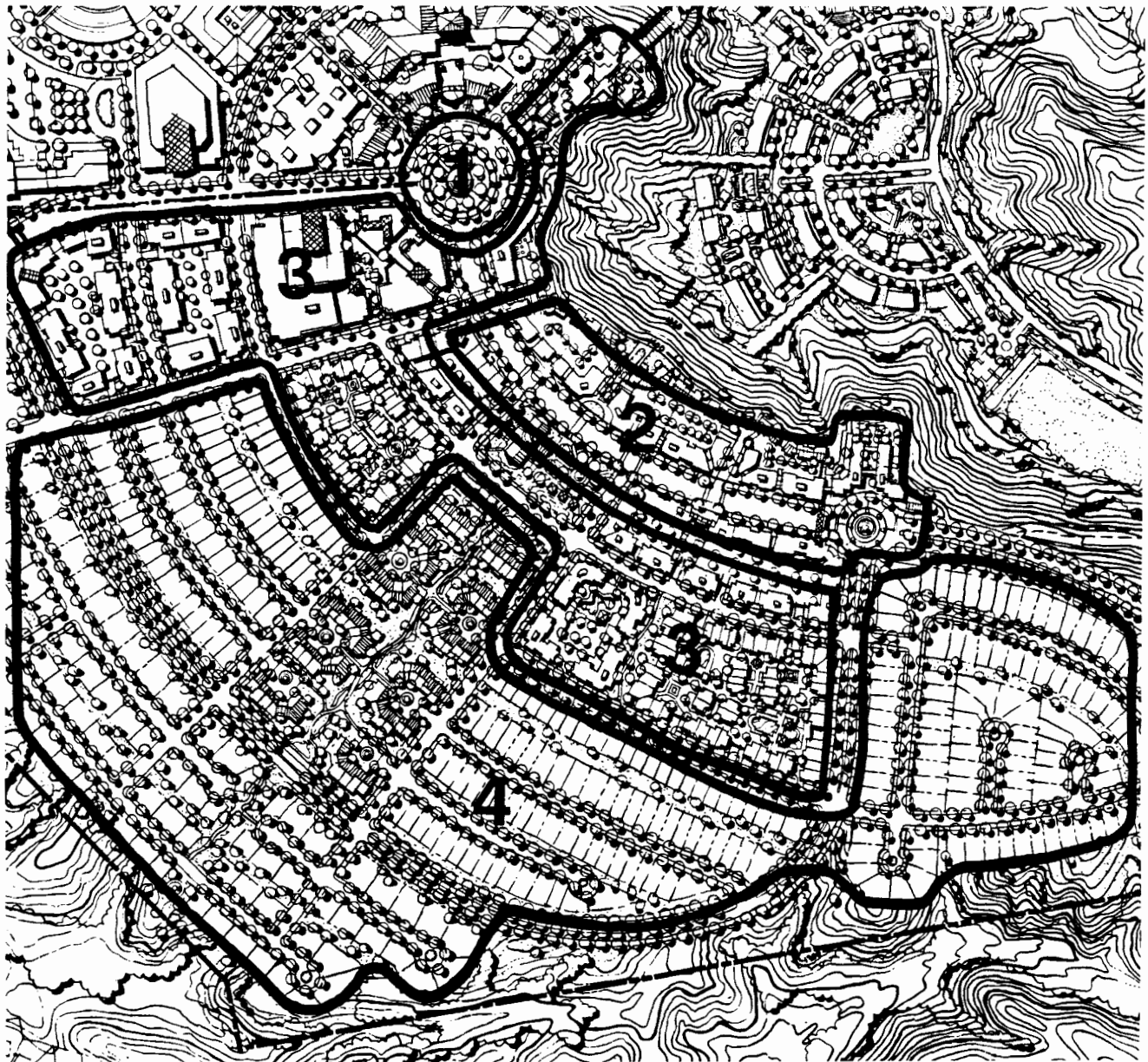
3.1 The Districts of Porta Bella

Even though Porta Bella is an interactive community, its scale and topography divide the site into four districts. The following sections will describe the characteristics of Porta Bella’s four districts.

3.1.1 Town Center District

The Town Center district of Porta Bella is located in the heart of the City of Santa Clarita, and will provide a small pocket of urban character and vitality within an turn-of-the-century downtown.

The Town Center is located in the saddle of the canyon rim within the boundaries of Porta Bella, approximately 200' above the commuter rail station commercial center on Soledad Canyon Road. These activity centers will be connected by a people moving escalator which will climb the hillside. Offering panoramic views of the valley, the escalator stops at informal patios, ultimately arriving at the Town Center above. The Town Center consists of a mixed-use area with a central common green that will join the future Civic Center and set the stage for an entertaining experience of shopping, dining, and cultural events clustered together within a stroll of one another.



KEY

1. TOWN CENTER CIRCLE
2. MAIN STREET
3. PERIPHERAL AREAS
4. RESIDENTIAL AREAS

TOWN CENTER DISTRICT



The Town Center is oriented along Main Street which forms a graceful crescent along the canyon rim with restaurants, movie theaters, galleries, boutiques, hobby shops, ice skating rink, dance area and sidewalk cafes, as well as tailors, dentists, shoe repair and more. Pedestrians will pass by newsstands, pushcarts, pocket parks, plazas and special street furniture surrounded by unique one, two and three-story buildings with distinctive heritage-inspired architecture. More of a specialty shopping center, the village will be a self contained commercial/residential district. This area will have a public atmosphere which will naturally be the setting for holiday events.

Glimpses of the elevated village will be visible to the surrounding area, and the design theme will reflect the heritage of Santa Clarita by utilizing formal and naturalized landscape treatments. Nearby office buildings, hotel and residential dwellings atop ground floor office space will add architectural intrigue to the scene.

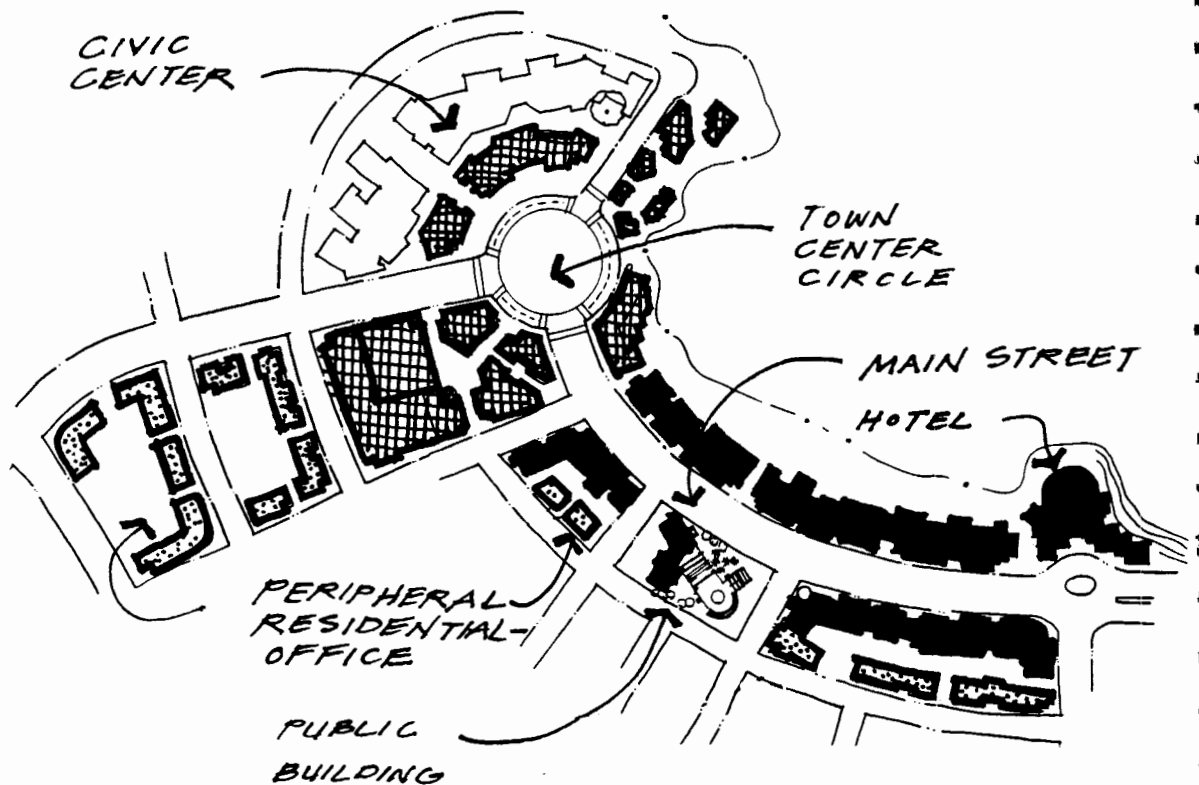
While the Town Center is strongly focused towards pedestrian activities and accessibility, vehicular access will be an necessary part of its design. The automobile traffic and on-street parking will contribute to the energy, activity and security of the primarily pedestrian oriented design.

Historical models for this type of urban center abound. Vibrant cities have developed with mixed-use centers including residential uses above retail/commercial functions. These areas develop into "downtown" as cities grow. Many American cities still have a downtown of this type. Cities such as Pasadena and Santa Barbara have retained this part of their heritage as the city grew beyond

them. Starting in the 60's, "contemporary" planning philosophies did not encourage these vital elements of town planning. Porta Bella re-introduces mixed-use neighborhoods.

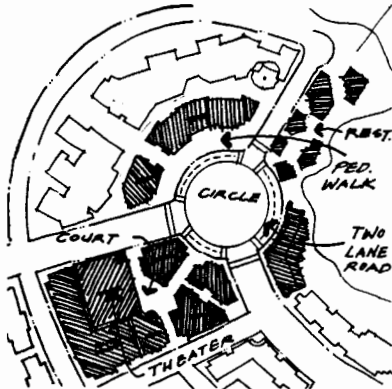
The Town Center is best understood by breaking it down into four separate components.

- A. Town Center Circle - the commercial & civic core
- B. Main Street - a concentrated mix of commercial, service & residential uses.
- C. Peripheral areas - multi-family residential & office uses.
- D. Residential areas - single-family & multi-family homes within walking distance of the downtown.



A. *Town Center Circle*

Along with a mix of uses, cities have created focal points around plazas, piazzas, or town squares for centuries. The Town Center Circle is to provide this focus for Porta Bella.



The Town Center Circle area is designed in a series of concentric rings. At the center is the park circle itself. The circle is to be a place of activity, a generally non-structured space that can change as the users change. It is a focal spot, a place for gatherings or holiday parades, an area for "Market night," or the site of a car show.



Within the circle is a two lane road, carrying local and visitor traffic at a reduced speed. Access across this street will be via several well-defined cross-walks.

The next "ring" is a comfortable pedestrian area supporting the retail and entertainment uses beyond, including a palodion, and ice skating rink. Cafes and restaurants will have tables spilling into this area. This is where pushcart vendors will locate. The visual and physical connections around the central ring should encourage pedestrians to move from one to the other.



The theater complex will be fronted by a courtyard to allow for queuing and ticket sales. Retail uses facing this interior court will consist of small food uses and secondary entertainment functions such as video arcades. Many of these shops may front the street as well as the courtyard.

The “restaurant row” portion of the Town Center will be a pedestrian corridor running from the park circle at the southwest to the top landing of the exterior escalator. This pedestrian corridor will be created by a series of small plazas bounded by the restaurants. The restaurants will have outdoor dining to augment the pedestrian activity and take advantage of the view the east.

The Civic Center Complex, to the north, becomes a natural participant in the pedestrian experience of the Town Center. It will generate an added level of activity which will further support the mixture of uses.

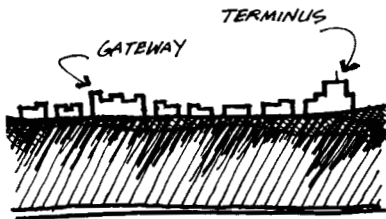
Parking for all the Town Center Circle land uses noted above will occur in structured parking at the theater/retail complex. Evening and weekend parking will also be shared with the train station to the northeast with access to the Town Center via the escalator.

B. Main Street



Along Main Street, the Town Center will take on its most urban feel. The buildings and sidewalks will form an edge with retail or commercial uses. Upper floors will be primarily residential with some office use. Cafe's, deli's, news racks, and portable carts will extend into the sidewalk. Stores will be encouraged to stay open into the evening to compliment the entertainment uses after 5:00 p.m., and encourage evening use by local residents and visitors. Parking will be provided on Main Street, surface lots and some structured parking beneath the buildings.

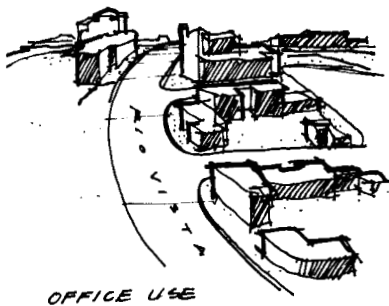
The buildings are three stories at the south end, marking the entrance to the Town Center. They step down to two- and one-story structures moving north. The increased scale of the Civic Center



beyond the Town Center Circle will become the visual and physical anchor at the north. From the surrounding areas the skyline of the Town Center will be identifiable by the height of buildings at either end. The hotel at the south end of Main Street will reinforce the sense of entry to Main Street and the Town Center.

The public-use site along the west side of Main Street will be the terminus to the pedestrian greenbelt running uphill from the southwest. The site will be occupied by a house of worship, a library, a park or similar.

C. Peripheral Areas



The residential street paralleling Main Street will consist of multi-family dwellings. A senior citizens housing complex may also be located on this street. The office buildings to the west will be served primarily by Rio Vista and will not depend on pedestrian activity from Main Street. Residential parking will be on surface lots with supplemental street parking. The office portion will have surface and structured parking.

D. Residential Areas



Single-family and multi-family residential uses are located within the Town Center District. Magic Mountain Parkway serves the residential areas, and sweeps southeasterly, climbing to meet Main Street at a turning circle. Neighborhood roads connect Magic Mountain Parkway and Main Street in a regular rhythm, creating blocks. The central block within this neighborhood consists of Multi-family housing arranged around a linear park area. This park

extends from the mixed use area of the Town Center, to the southerly property edge. Connection to the park from the exterior of the multi-family area is through the individual auto courtyards associated with the clusters of development.

Single family neighborhoods are also located within the Town Center Neighborhood, in 4,000 square foot lots and Z-lots. These are located in neighborhoods, where the roads & sidewalks continue directly into the downtown.

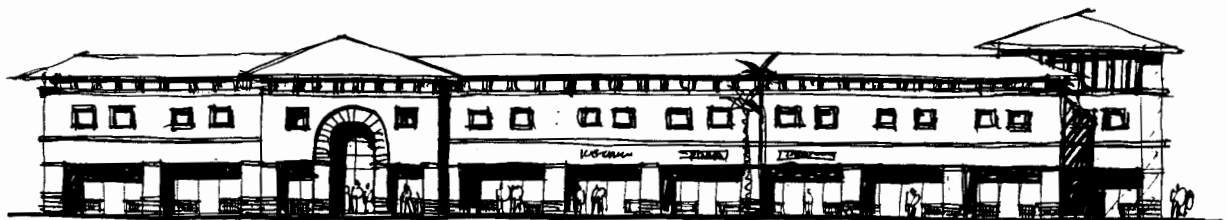


3.1.2 Soledad Commercial District

The Soledad Commercial District will be characterized by six activities:

1. The Rail Station
2. Multi-family Residential
3. Office/Business/Retail Park
4. Sports Club
5. People Moving Escalator
6. Business Park

The focal Santa Clarita rail station which will be located there. This area, near Soledad Canyon Road, will have the station as the central and predominant building in the area. The architectural character will resemble the Town Center District. The escalator may be incorporated into the rail station structure as it begins its ascent up the hillside. Classic ideals and images of rail travel should permeate the area, such as they do at the stations of Europe and the East Coast. Anchoring the westerly portion of the rail complex will be mixed use commercial in support of the rail station. At the other end, closer to the proposed Santa Clarita Parkway, will be a cluster of office buildings. These office buildings will frame both sides of Santa Clarita Parkway at its intersection with Soledad Canyon Road. Continuing up the canyon is the business park which may accommodate an outlet center and bulk merchandise stores, such as furniture, garden, appliances. The site may also contain office or business park uses.

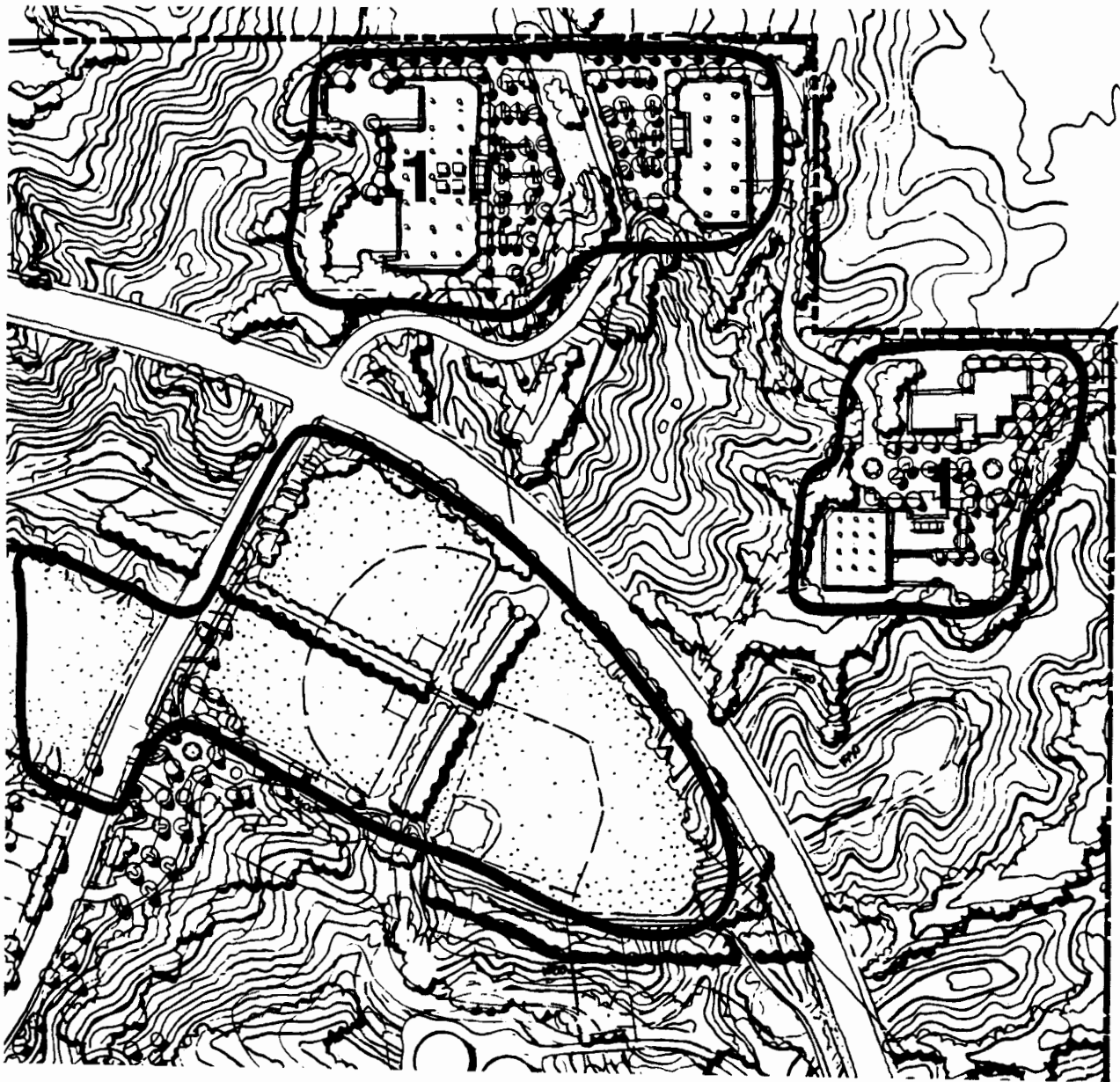




KEY

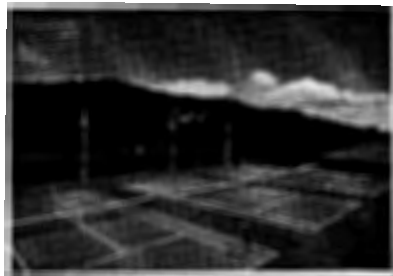
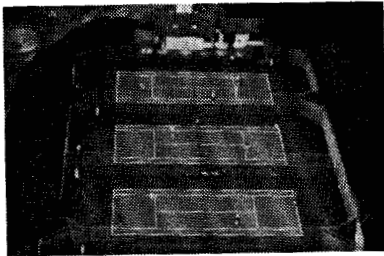
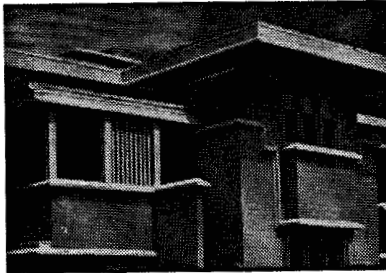
1. RAIL STATION AND COMMERCIAL
2. OFFICE / BUSINESS PARK
3. MULTI FAMILY RESIDENTIAL
4. SPORTS CLUB / GOLF DRIVING RANGE
5. PEOPLE MOVER / ESCALATOR

SOLEDAD DISTRICT



KEY

1. BUSINESS PARK

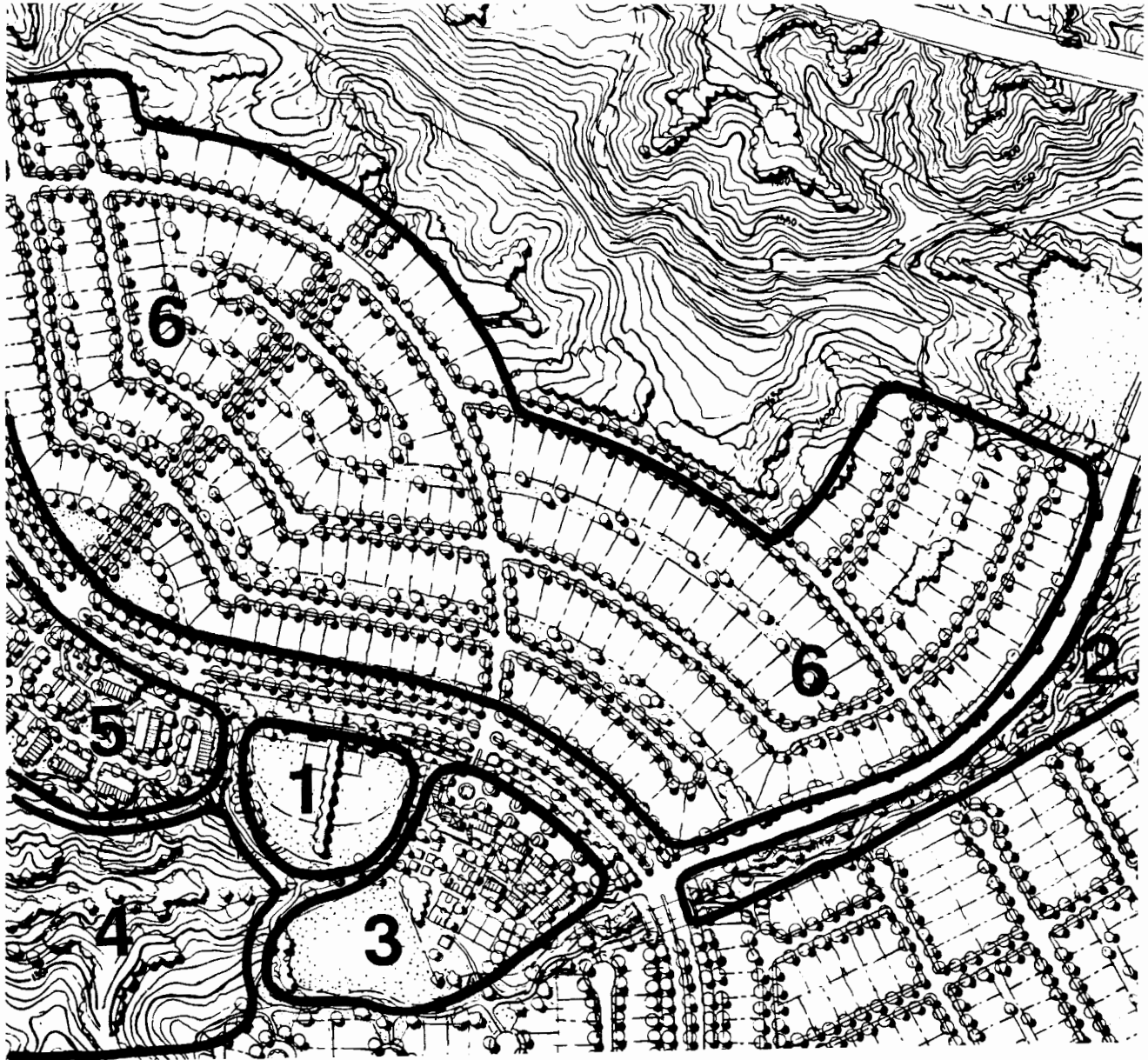


Near the station will be a multi-family residential neighborhood. This higher density area will enjoy comfortable walking distance to the rail station via a grade separated crossing of the rail line which will allow pedestrian access to the station area. This neighborhood will be separated from the rail tracks by a sports club, including a stadium tennis court. This complex will be open to the surrounding community & office workers. Near the access road to this development there will be a golf driving range. These recreational amenities will provide a "club" atmosphere to the neighborhood, as well as allowing the adjacent office workers to enjoy tennis, swimming or golf during lunch or after hours.

Further to the east, and most removed from the general development of Porta Bella, will be the Business Park land use area. This area will offer employment generation for the community. The Business Park area will continue the business park development which has begun near Soledad Canyon Road. Access to this area will be both from below, near Soledad Canyon, and above, from the central project road.

3.1.3 Central District

The Central district is located between the Town Center and the South districts, adjacent to west side of the ridge. The Central area is comprised of various housing types consisting of single family and multi-family homes. The neighborhoods have been designed to ascend the hill offering views to the west from future residences. An elementary school and park is located at the end of the Oakdale Canyon extending from the west. Within this area, a portion of the canyon will be utilized as a riparian rehabilitation zone to mitigate any riparian habitat disturbance done elsewhere on the site, and provide an opportunity for a nature interpretive or arboreum center.



KEY

1. NEIGHBORHOOD PARK
2. COMMUNITY TRAIL
3. ELEMENTARY SCHOOL
4. RIPARIAN REHABILITATION AREA
5. MULTI FAMILY RESIDENTIAL
6. SINGLE FAMILY RESIDENTIAL

CENTRAL DISTRICT

Adjacent to the park land, on the north side of the school, will be a Multi-family residential area which will also overlook the canyon, and enjoy convenient access to the school and park lands.

Crossing Main Street from the school to the pedestrian system located on the east side will be safely accommodated by an underpass near the school. Also on this side is an area of single family homes. The appearance of the Central district will be that of a hillside town which blends with the natural environment. Architectural techniques will be utilized in this area to be sensitive to landform.

A 3.5 acre park is located to the easternmost area of this district within the Open Space system. This park will be level, connected to the ridge trail system and is large enough for active playfields.

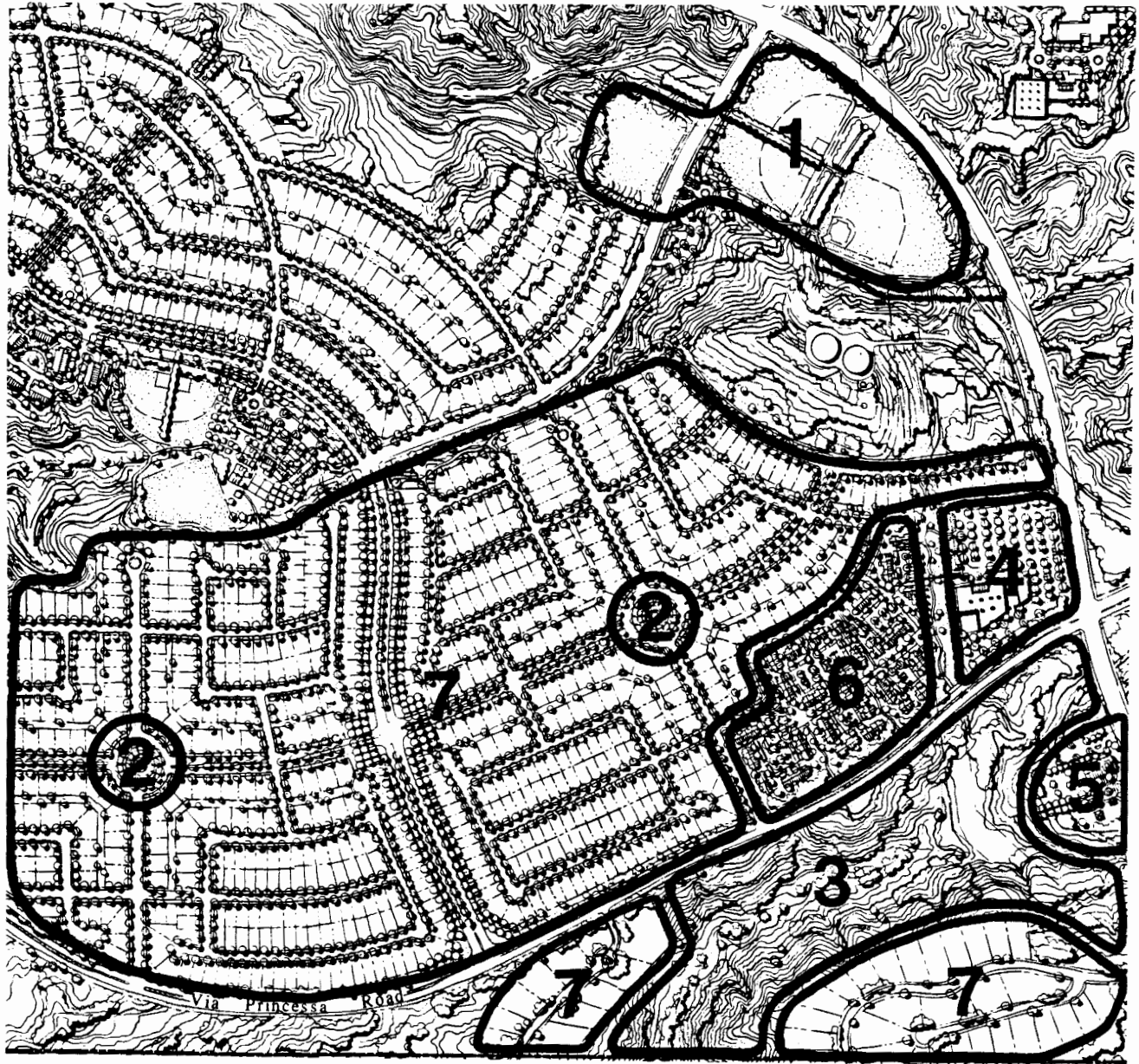
3.1.4 South District

Within the South District will be an east-west streetscape entailing a classically designed landscaped boulevard and parkway with two focal community roundabouts at opposite ends. Midpoint along this boulevard (Greene Street) is an intersection with the Main Street Collector which leads to the Town Center. The intersection, centered with the turning circles, consist of complementary landscape and landmark identity contributing to the traditional town character. Fronting the Greene Street are paired single family homes resembling large stately houses. Continuous lawns and front porches result from the use of rear yard garages served by alleys.



Perpendicular to the east-west aligned Greene Street and radiating from the turning circles are complimentary streetscapes that access other neighborhoods where the homes are not served by alleys. In

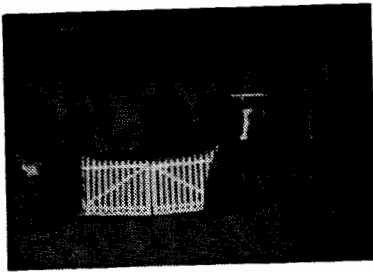
Porta Bella Specific Plan



KEY

1. COMMUNITY PARK
2. COMMUNITY CIRCLES
3. RIPARIAN REHABILITATION AREA
4. NEIGHBORHOOD COMMERCIAL
5. INSTITUTIONAL
6. MULTI FAMILY RESIDENTIAL
7. SINGLE FAMILY RESIDENTIAL

SOUTH DISTRICT



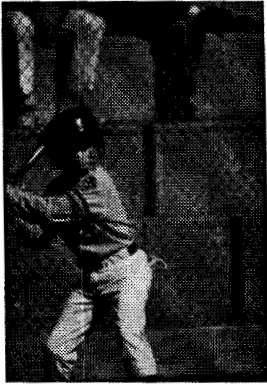
these areas, a single lane sideyard or shared sideyard driveway is encouraged that may also eliminate the garage from the front and continue the streetscape character. Here as in other areas of Porta Bella, where garages may occur in the front, garage setbacks are slightly greater than the dwelling unit's, to encourage the residential architectural interaction with the street scene. Housing in these neighborhoods consist of varying size single family homes. The integration of the various dwelling unit types have been arranged in an urban form to eliminate perimeter walls. The absence of these walls provides for interacting neighborhoods with traditional streetscapes of varied architecture. Trail heads from the residential cul de sacs will connect to the elementary school and parks in addition to the community park trail and ridge runner trail.



A Neighborhood Commercial center is located at the corner of Santa Clarita Parkway and Via Princessa in the south-eastern portion of this district. This center is intended to have neighborhood services such as a grocery store, dry cleaner, video store and bank. Pedestrian trail systems will connect the commercial area to the residential areas from all sides.

Conveniently located adjacent the neighborhood commercial center are multi-family homes. This multi-family village incorporates a 100 foot wide open space buffer area between the commercial area. Minimal use of walls will be utilized within the project, to increase the visual and functional connections with the surrounding community.

On the eastern side of the bermed water reservoirs and ridge from the residential area will be a community park of 22 acres. This park will be connected to the other parks and opens space areas, including the school via a trail system which runs adjacent to the



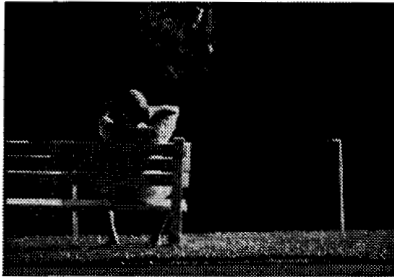
connector road between Santa Clarita Parkway and Main Street (Via Porta Bella). This park may have lighted ball fields, for night play, and parking for use by the City residents. The ridge will buffer the residents from the park's activities thereby providing longer use periods. The park location is also part of the ridge runner trail system.

Along the southern boundary of the South district, near the existing Circle J Ranch neighborhood, will be the single family area with minimum 10,000 square foot lots. This area will enjoy views of the canyons. Although the lot sizes are smaller than the Circle J Ranch lots, they have been designed to incorporate large pad sizes to resemble the adjacent Circle J Ranch lots. This reduced lot size will ensure the canyons are preserved as natural open space, and not impacted by private ownership. Oro Fino Canyon, which extends between Via Princessa and the residential areas, will be one of two riparian habitat mitigation areas. This canyon will be revegetated according to the procedures established within this document and the Environmental Impact Report prepared for the site. Equestrian trails may be located in this canyon if compatible with the environmental goals of the EIR.

Located with access from Santa Clarita Parkway is the Institutional land use area. This four acre pad may be utilized as a fire station, house of worship, day care center, equestrian center or similar publicly oriented use.

3.2 Porta Bella Objectives & Concepts

Several fundamental objectives have guided the preparation of this Specific Plan. They are:



- Blend the community with its environment.
- Blend this community with the other communities of Santa Clarita.
- Provide a land use mix, offering employment, services, cultural, entertainment, residential and recreation within an environmental setting.
- Provide for a social and economic mix of residents
- Provide a setting for interaction among residents.
- Provide meaningful mobility alternatives to the automobile.
- Create continuously flowing open space systems which connect to the communities.
- Create a road system which will serve the city, region, and the community with character and environmental sensitivity.

To implement the objectives outlined above, the following concepts have been incorporated into the this Plan.

- The environmental conditions will guide the setting for development.
- The Plan will be comprised of both contemporary and traditional methods for development.
- The Plan will compliment Santa Clarita's existing communities.

The following sections are more detailed descriptions of the concepts described above.

3.2.1 *Environmental Setting for Development*

Developable areas within the project area were determined after an environmental investigation of the project site was conducted. This environmental review considered such elements as:

- Ridgeline preservation & enhancement
- Visual impact of development
- Open space systems
- Existing vegetation & wildlife
- Local & regional circulation impacts
- Geologic & seismic conditions
- Landform & topography
- General Plan goals and policies

3.2.2 Traditional and Contemporary Planning Techniques

The urban design idea within the Specific Plan area is to incorporate the best standards from traditional and contemporary developments, with standards to achieve both a functional and aesthetically pleasing environment.



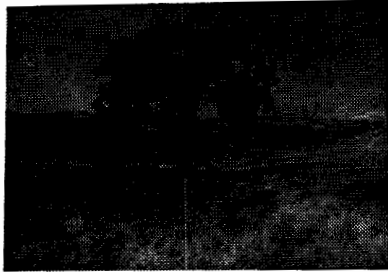
Some popular traditional neighborhoods exist throughout Southern California. Generally, they are located in areas developed prior to 1940, and have a character that is different than the suburbs that were built after 1950. One of the most distinguishing factors of the traditional neighborhoods is their pedestrian scale, whereas contemporary neighborhoods are designed for convenient automobile access. Among the factors that distinguish these traditional neighborhoods are street widths, which are often narrower and pedestrian-landscaped; a more patterned street layout; homes which formally address the streets with porches and front doors instead of garages and walls; and the incorporation of public and mixed use commercial opportunities. All of these elements contribute to an overall setting for social interaction, which is also a primary goal of Porta Bella.

Contemporary neighborhoods have been developed since the 1950's and display differences from the character established in the early twentieth century. Curvilinear road layouts and cul-de-sacs are common. Neighborhoods are often designed as "pods" of land use areas, separate and distinct. The purpose for this development style is to incorporate graceful aesthetics, privacy, and safety in the residential neighborhoods, while providing a setting for contemporary merchandising and business operations to occur in distinct and separate areas.

3.2.3 Compliment Existing Communities

The project area is encircled by four existing communities within Santa Clarita, each with a distinct character and historical perspective. From the onset, an objective of this project has been to create a community which will service and compliment each of the surrounding communities without detracting from them.

The planning approach to this issue has been to maintain and enhance the natural setting of the community. This will be accomplished by; 1) preserving and enhancing the hillsides contained within the project area with an oak tree planting program; 2) remediating the landform which has been scarred by previous activities on the site; 3) providing a regional road through the site which results in the least amount of environmental and character depletion to the vicinity; 4) providing a flexible Town Center district which may by design create a mixed-use district with minimal reliance on the automobile.



The proposed Santa Clarita Parkway regional road alignment through the eastern project boundary area will enable the preservation of the oak grasslands on the adjacent City Hall property. This City owned land aesthetically rivals the best of Southern California's public parks in magnitude, setting and natural beauty. The preservation of the land could provide a major central park or expansion of the Civic Center, and be a benefit to the existing communities within the entire Santa Clarita Valley.

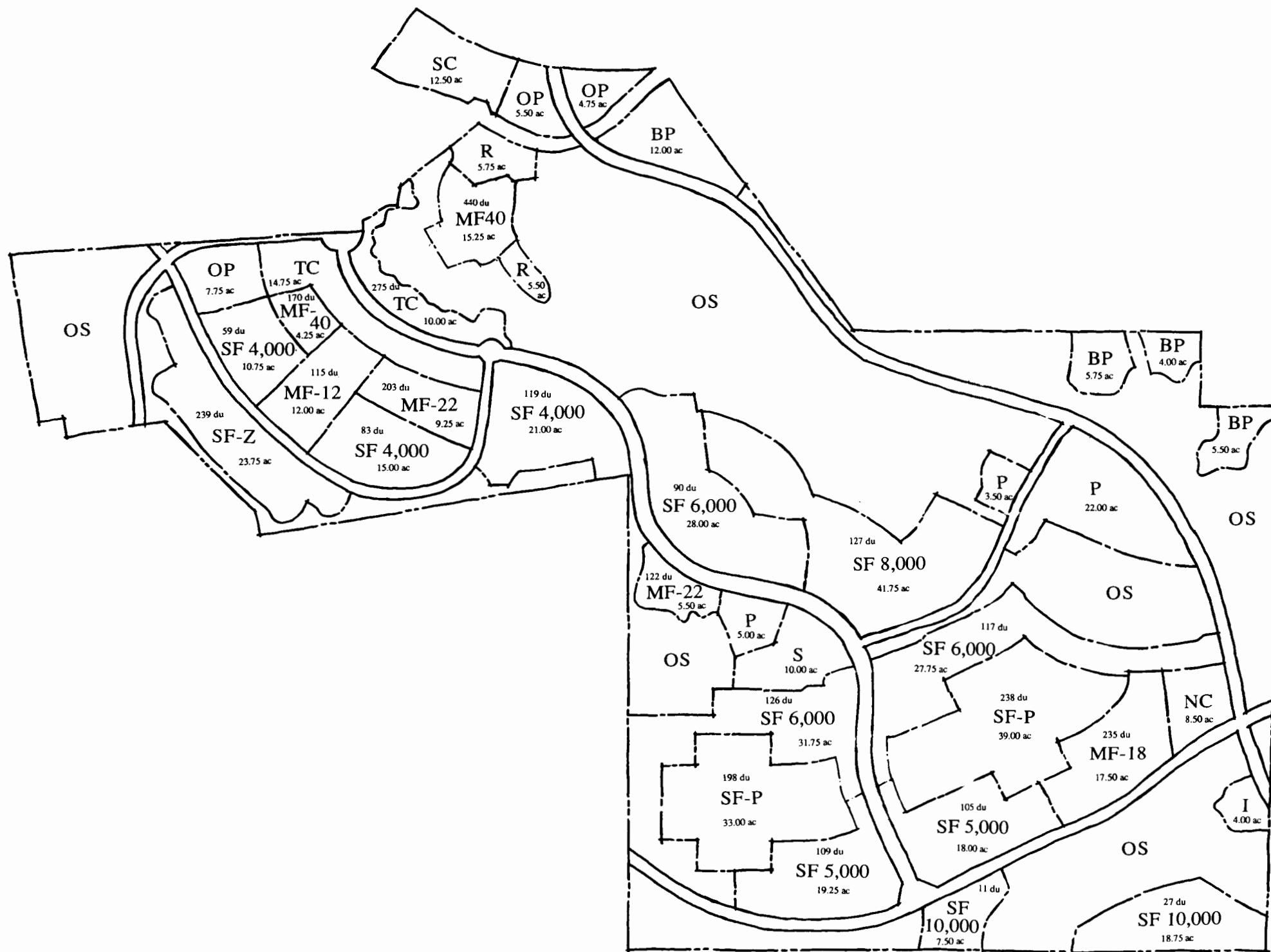
3.3 Land Use Plan Diagram & Summary

The Land Use Plan Diagram illustrates the location of land uses and roads within the Specific Plan area. The accompanying Land Use Summary Table is a numerical summary of land use characteristics depicted within the Land Use Plan Diagram. The purpose of this diagram and table is to provide a guideline or "target" for development within the Specific Plan area.

Porta Bella Specific Plan

<i>Land Use Designation</i>	<i>Map Designation</i>	<i>Density Range</i>	<i>Land Use Area Acres</i>	<i>Target # of Units</i>	<i>% of Total Dwellings</i>	<i>% of Total Area</i>
Open Space	OS	na	392.25	na	na	39.4%
Parks & Recreation	P, R	na	41.75	na	na	4.2%
Subtotal of Open Space, Parks & Rec.			434.00			43.6%
Schools	S	na	10.00	na	na	1.0%
Master Streets	na	na	58.00	na	na	5.8%
Subtotal of School & Master Streets			68.00			6.8%
Single-Family Residential						
SF 10,000	SF 10,000	2-4 du/a	26.25	38	1.2%	2.6%
SF 8,000	SF 8,000	3-5 du/a	41.75	127	3.9%	4.2%
SF 6,000	SF 6,000	4-6 du/a	87.50	333	10.3%	8.8%
SF 5,000	SF 5,000	5-7 du/a	37.25	214	6.6%	3.7%
SF 4,000	SF 4,000	6-8 du/a	46.75	291	9.0%	4.7%
SF Z-Lot	SF Z	9-11 du/a	23.75	239	7.4%	2.4%
SF Paired	SF P	6-8 du/a	72.00	436	13.5%	7.2%
Subtotal of Single-Family			335.25	1,678	51.8%	33.7%
Multi-Family Residential						
MF 12	MF 12	10-14 du/a	12.00	115	3.6%	1.2%
MF 18	MF 18	16-20 du/a	17.50	235	7.3%	1.8%
MF 22	MF 22	20-22 du/a	14.75	325	10.0%	1.5%
MF 40	MF 40	38-42 du/a	19.50	610	18.8%	2.0%
Town Center	TC	12-18 du/a	na	275	8.5%	na
Subtotal of Multi-family			63.75	1,560	48.2%	6.4%
Commercial						
Town Center	TC	na	24.75	na	na	2.5%
Soledad Comm.	SC	na	12.50	na	na	1.3%
Neighborhood	NC	na	8.50	na	na	0.9%
Office Park	OP	na	18.00	na	na	1.8%
Business Park	BP	na	27.25	na	na	2.7%
Institutional	I	na	4.00	na	na	0.4%
Subtotal of Commercial			95.00			9.5%
Total of Project Area			996.00	3,238	100.0%	100.0%

LAND USE SUMMARY TABLE

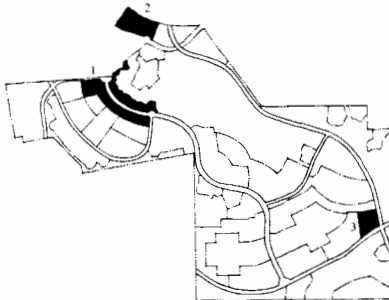


LAND USE PLAN

3.4 Non-Residential Land Uses

Several non-residential land uses coexist within the community as commercial/services, entertainment, employment base or recreational. In addition, a multi-modal transportation system is located within the community, which will link the Los Angeles light rail “Blue line” with autos and buses. These non-residential land uses represent 9.5% of the total land area of Porta Bella.

A fundamental concept utilized for commercial uses within the project is to ensure that individual commercial land uses do not compete, and potentially defeat each other. The following is a brief description of the individual commercial characters within the separate districts:



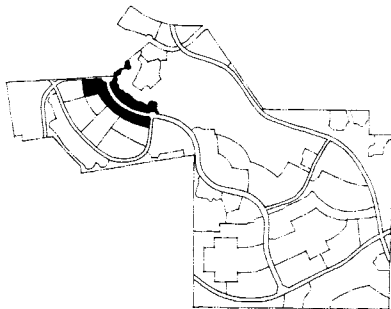
The Town Center Mixed-use area is comprised of civic uses, specialty commercial & services, entertainment, restaurants, offices and cultural facilities such as the amphitheater and performing arts center.

The Soledad Mixed-use area is comprised of services which support the function of the multi-modal transportation, such as convenience food/commercial/services, vehicle repair, day care, office park and business park.

The Neighborhood Commercial area located within the South District is located at the intersection of Santa Clarita Parkway and Via Princessa, which is a site well fit for consumer shopping needs, such as a food items, pharmacy, financial institution and similar.

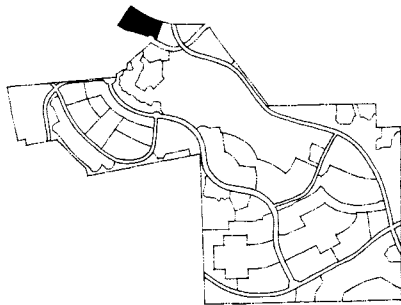
The tenant mix of the commercial facilities shall remain individual so as not to compete, and therefore dilute, the other area's feasibility. The following paragraphs describe in greater detail the market of each commercial area.

3.4.1 Town Center Mixed Use (TC)



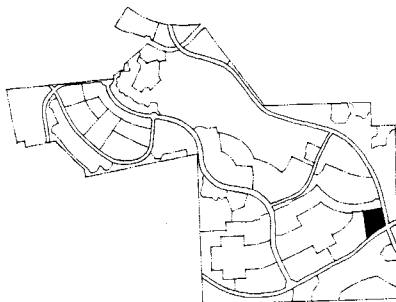
The Town Center Mixed-use area is located south of the adjacent to the City owned parcel. The layout of this Town Center area is such that implementation of the City's development program may be readily woven into a civic/commercial central area with goods & services available to resident & visitors with destination and festival orientation. Within the Town Center mixed-use area, 275 residential units will contribute to an "around-the-clock" social presence to the area. The Town Center Mixed-use area encompasses 24.75 acres, which is 2.5% of the Porta Bella.

3.4.2 Soledad Mixed-Use Commercial (SC)



Located within this land use area is the multi-modal transportation center, and support commercial services and offices which will mainly service and support the users of the station. Also included within this area is the people-mover escalator system which connects the Soledad and Town Center Districts. The Soledad Mixed-use area encompasses 12.50 acres of land, which is 1.3% of the total land area of Porta Bella. Tenants may include small scale convenience shopping, fast food and sundry items

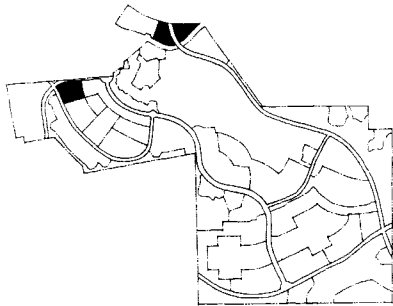
3.4.3 Neighborhood Commercial (NC)



The Neighborhood Commercial land use area is located within the South District, at the intersection of Via Princessa and Santa Clarita Parkway. It is anticipated that neighborhood commercial uses such as a grocery store, pharmacy, bank and/or service uses may be located in this area. The orientation of development on this parcel shall accommodate easy access from the adjacent residential

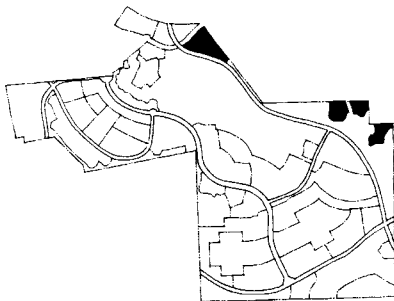
neighborhoods in an effort to promote pedestrian, as well as vehicular mobility. The Neighborhood Commercial Parcel is 8.5 acres in size, and represents 0.90% of the total project area.

3.4.4 Office Park (OP)



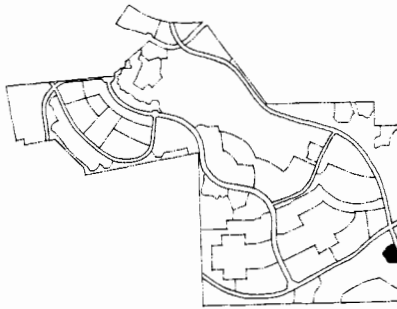
The Office Park land use area is located adjacent to Soledad Canyon Road, east and west of Santa Clarita Parkway, and within the Town Center District. The Soledad District portion of this land use area encompasses 10.25 acres over two parcels. These parcels are separated by the Santa Clarita Parkway. The Town Center Office Park area encompasses 7.75 acres, located on Rio Vista as it leads into the Town Center mixed-use area. Business office uses will be located within this land use area, which are intended to employ people within the Santa Clarita Valley, as well as to act as an employment destination for residents of the Los Angeles Basin via the light rail Blue line. The Office Park land use area represents 1.8% of the overall project area.

3.4.5 Business Park (BP)



The Business Park land use area is located in two areas at the northeasterly base of the central ridge, both along Santa Clarita Parkway. The BP land use area to the east of the site shares a strong relationship with the planned Business Park area adjacent to this project. Uses within the Business Park will be similar to those planned adjacent to the site, such as light manufacturing and business offices. The twelve acre site near Soledad Canyon Road may also accommodate large retail sales structures, such as bulk merchandising and outlet merchandising uses. The Business Park land use area occupies 2.7% of Porta Bella.

3.4.6 Institutional (I)



The Institutional land use area is located off of Santa Clarita Parkway, southerly of its intersection with Via Princessa Road. This area encompasses approximately 4.00 acres, and represents .4% of the Specific Plan area. Uses within this area are intended to be public or non-profit, such as fire station, houses-of-worship, meeting halls, clubs and similar.

3.4.7 School (S)



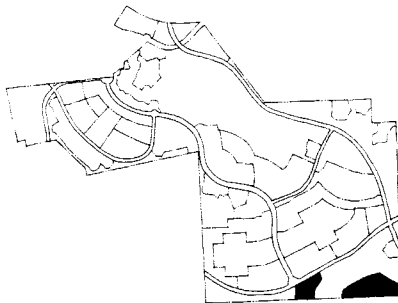
An elementary school site is centrally located within the community, adjacent to a 5 acre park site and the Oakdale riparian canyon. This school site occupies 10 acres of land, and has a pedestrian separated street crossing with Main Street, and trail heads to adjacent residential neighborhoods. The area encompassed by the elementary school site represents 1.0% of Porta Bella's area. The provision of community infrastructure such as this school site is one of the reasons why the planned community of Porta Bella will better integrate with Santa Clarita, in contrast to incremental, non-comprehensive development.

3.5 Residential Land Uses

Residential land uses comprise the largest land use area within the Plan, occupying approximately 40% of the overall land area. Residential opportunities are diverse, with a full range of homes for varying incomes and life-styles. To accommodate the varying types of dwellings envisioned by the Plan, twelve residential designations have been established. Density ranges are proposed upon gross planning area's acres to guide the housing type in each neighborhood. However, minor deviations should be permitted to

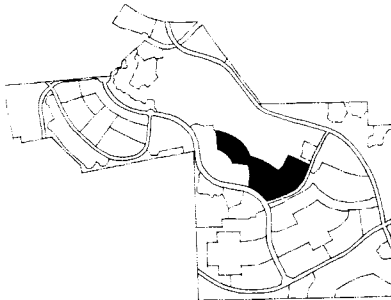
accommodate grading constraints, streets and other considerations which may alter acreage calculations. The twelve residential land-use designations are described in the following paragraphs.

3.5.1 Single Family 10,000 (SF 10,000)



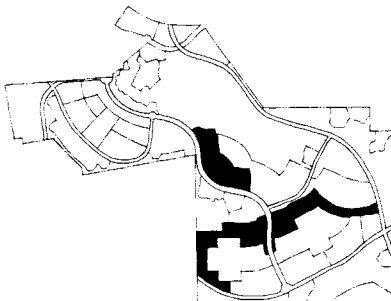
The Single Family 10,000 land use occupies 26.25 acres, containing 38 units. This land use area is the lowest density land use in the Specific Plan area, with a range of 2-4 dwelling units per acre. The SF 10,000 land use area is located within the southeastern corner of the Plan area, adjacent to the Oro Fino Canyon. SF 10,000 comprises 1.2% of the total residential units, and 2.6% of the overall Plan area.

3.5.2 Single Family 8,000 (SF 8,000)



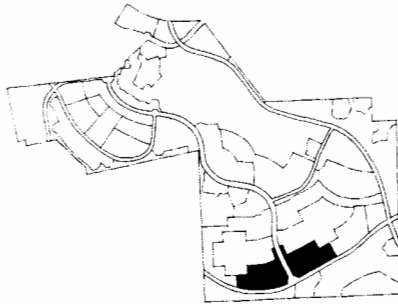
The SF 8,000 designation is the second lowest residential density in the Specific Plan area, allowing a range of 3-5 units per acre. Locationally, the SF 8,000 land use is within the Central district, between Main Street and the ridgeline open space area. This land use designation will be largely comprised of view lots. There are 127 SF 8,000 lots within the community, occupying 41.75 acres of land. The SF 8,000 land use comprises 3.9% of the total residential units, and 4.2% of the land in Porta Bella.

3.5.3 Single Family 6,000 (SF 6000)



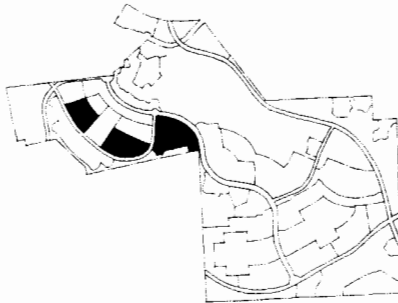
The SF 6,000 land use contains the second largest number of single family homes within the community with 333. This land use product is located in the Central and South districts, occupying 87.50 acres. The density range for this land use area is 4-6 units per acre. The SF 6,000 land use comprises 10.3% of the total residential units and 8.8% of the land.

3.5.4 *Single Family 5,000 (SF 5,000)*



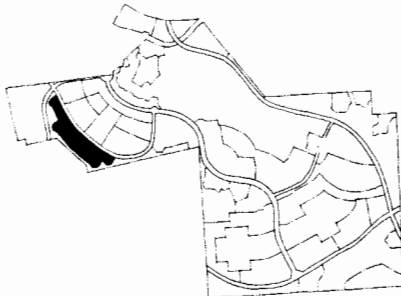
The SF 5,000 land use area frames Main Street's southerly entrance from Via Princessa, in the South District. This land use area represents the fifth largest number of single family units, with 214. Total acreage is 37.25. The density range for this area is 5-7 dwelling units per acre. SF 5,000 comprises 6.6% of the total residential units, and 3.7% of the land.

3.5.5 *Single Family 4,000 (SF 4,000)*



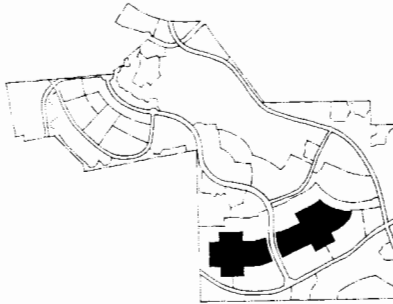
This land use area is located within the Town Center District of Porta Bella. The typical lot is characteristic of downtown lots in earlier "traditional" communities. The density range within this land use area is 6-8. Total acreage for this land use area is 46.75 which contains 291 units. The SF 4,000 land use comprises 9.0% of the units within the Specific Plan area, and 4.7% of the overall area.

3.5.6 *Single Family Z-lot (SF-Z)*



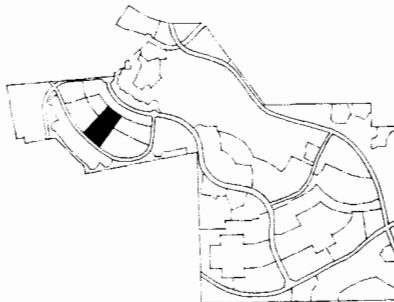
The Z-lot land use area contains the fourth largest number of single family residential units, with 239. Densities within the Z-lot land use ranges from 9-11 du/a. Total acreage for the area is 23.75 acres. The Z-lot land use area is located within the Town Center District, and comprises 7.4% of the residential units within the Plan area and 2.4% of the land.

3.5.7 *Single Family Paired (SF-P)*



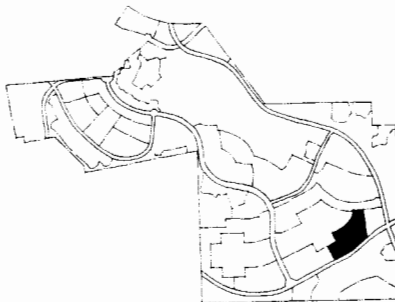
The Single Family Paired residential land use is located within the South District. This product type is based upon a traditional community image, with detached rear-yard garages served by an alley. This land use area comprises 72 acres and contains 436 homes. The density range for this product type is 6-8 du/a. The Single Family Paired residential land use comprises 13.5% of the total residential units within the project area, and 7.2% of the overall area.

3.5.8 *Multi-family 12 (MF12)*



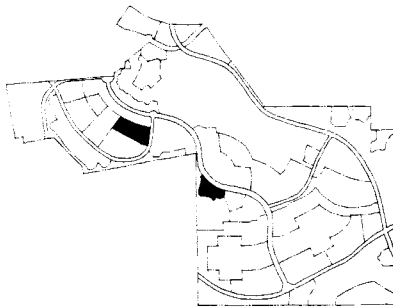
This land use product type is located within the Town Center District, and is characterized by “cluster” housing congregated along a linear public open space. The MF12 land use area encompasses 12.00 acres and contains 115 units. The product type range for this project area is 10-14 units per acre. The MF12 land use area comprises 3.6% of the total residential units within the project area, and 1.2% of the overall Plan area.

3.5.9 *Multi-family 18 (MF18)*



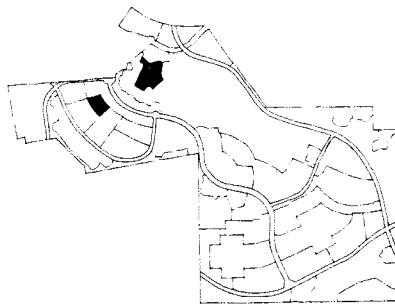
The Multi-family 18 land use area is located within the South District, adjacent to the neighborhood commercial center at the intersection of Via Princessa and Santa Clarita Parkway. This multi-family area will encompass 17.50 acres and contain 235 units. The product type range for this designation is 16-20 units per acre. The MF18 land use area comprises 7.3% of the total residential units within the Plan area, and 1.8% of the overall land area.

3.5.10 Multi-family 22 (MF22)



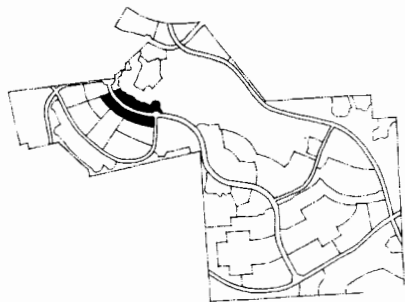
The Multi-family 22 land use designation is located within two areas of the plan. Within the Central District, a 5.5 acre site adjacent to the neighborhood park and Main Street contains 122 dwelling units. The MF 22 area located within the Town Center District consist of 9.25 acres and 203 units. Total land use area comprises 14.75 acres and contains 325 units. The product type ranges from 20-24 units per acre. Senior Citizen residential housing may also be utilized in this area. The Multi-family 22 land use area comprises 10.0% of the total residential units within the Specific Plan area, and 1.5% of the overall land.

3.5.11 Multi-family 40 (MF40)



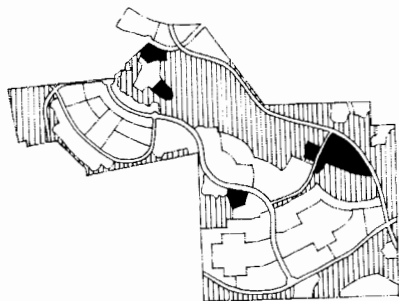
This land use designation is intended to introduce residents into the Town Center and Soledad Districts. One MF 40 land use area is located next to the entertainment and office areas within the Town Center, entailing 4.25 acres and 170 units. The other area occurs near the commuter rail station in the Soledad District, and consists of 15.25 acres and 440 units. Due to its location adjacent the sports club, a club-house is permitted within this area. The total Multi-family 40 land use designation occupies 19.50 acres of land, and contains 610 units. The product type ranges from 38-42 units per acre. A senior citizen housing center may be used in the Town Center Districts MF-40 area. The Multi-family 40 land use area comprises 18.8% of the total units within Porta Bella, and 2.0% of the land.

3.5.12 Town Center Mixed Use Residential (TC)



Residential units within the Town Center Mixed-use area will occur nearly exclusively above the retail and service uses on the ground floors. The Town Center Mixed-use area occupies 24.75 acres of land, and will accommodate 275 units. These units may be located in both areas indicated as Town Center, except in the area designated as Town Center Circle. Product type ranges from 12-18 units per acre. The Town Center Mixed-use area comprises 8.5% of the residential units within the Plan area.

3.6 Open Space, Parks & Recreation



The Open Space, Parks & Recreation system comprises the largest land use area within the Porta Bella, encompassing nearly 44% of the overall project area (434.00 acres). Of this, nearly 42 acres within this system are planned for active parkland & recreation. The remaining 392.25 acres are natural open space. This large and meaningful open space system is one of the most compelling reasons which illustrate the advantages of the master plan of Porta Bella over incremental and piecemeal development. It is through comprehensive planning that large, logical areas of land are preserved for the environment, parks and recreation.

The Open Space areas are a part of the natural system which exemplify the environmental qualities of Santa Clarita. The fundamental purposes of the Open Space areas are: 1) to retain significant landform and environmental features; 2) to preserve the green hillsides which surround the project area; and, 3) to provide a strong unifying character for the community based upon environmental features.

In addition to the Open Space areas creating a green visual quality, they will also provide recreational opportunities. Pocket parks, view overlooks and an interlinking trail system for hiking and biking will be located within the open space area. Open Space within Porta Bella is large, interconnected, natural and close to housing. The combination of these elements creates an natural setting conducive to exploration and play for all ages.

- 1.0 *The Specific Plan as Planning Tool*
- 2.0 *Community Context & Integration*
- 3.0 *Land Use Master Plan*
- 4.0 *Transportation & Circulation*
- 5.0 *Master Landscape Plan*
- 6.0 *Community Design & Land Use*
- 7.0 *Parking Regulations*
- 8.0 *Sign Regulations*
- 9.0 *Community Lighting Regulations*
- 10.0 *Infrastructure & Public Utilities*
- 11.0 *Design Review*
- 12.0 *Implementation*



4.0 TRANSPORTATION & CIRCULATION

The concept of transportation and circulation within the community is a synthesis of traditional and contemporary town design. In the traditional sense (pre-1940's), communities are arranged in smaller neighborhood blocks, with emphasis towards the interaction of various land uses rather than the segregation of them. Commercial and recreational areas interrelate with residential land uses via convenient pedestrian access. Streets within traditional neighborhoods are often predictable and interconnected.

The more contemporary communities (post 1950's) are often comprised of separated land use areas and more private and isolated housing clusters. A hierarchy of roads exist whereby the interior local traffic empties to larger roads via a limited number of intersections.

The difference between the two vehicular circulation approaches can be compared as a funnel versus a sieve. The funnel allows larger volumes through a limited point at a higher speed, the sieve dispersing the volume through numerous points at lower speeds.

Within Porta Bella, the best components of both design techniques will be utilized. A number of alternative transportation systems will also be incorporated to enhance community linkage and mobility. The following sections will discuss the various systems and their function within the Specific Plan area.

4.1 Vehicular Systems



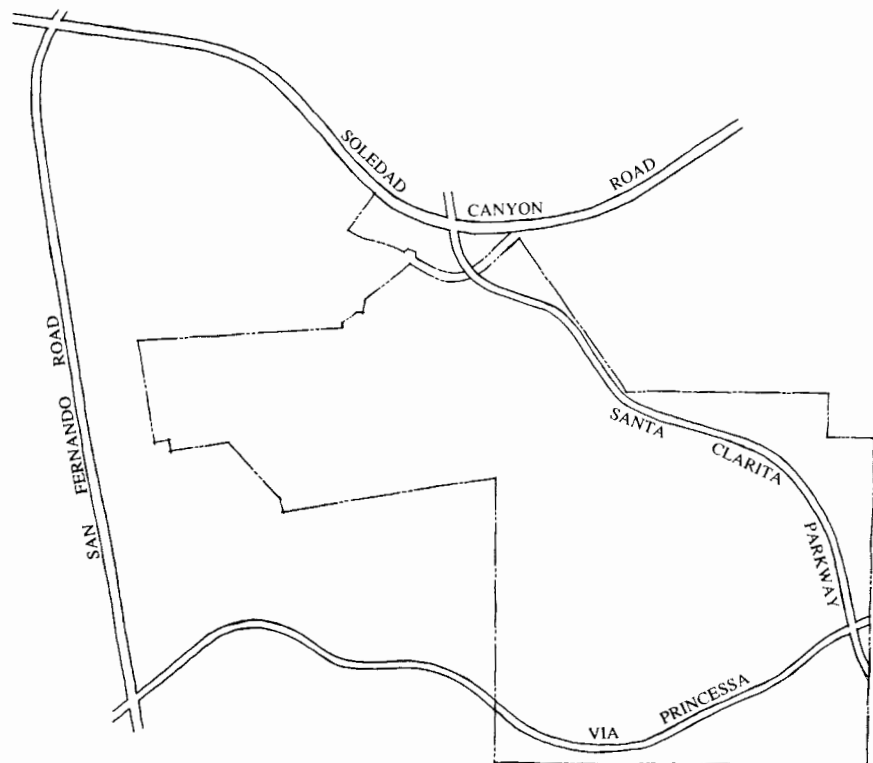
Both the traditional and contemporary mobility ideas will be incorporated depending upon the desired function and character of the area serviced by the roads. To the interior of the neighborhoods, smaller, interconnected blocks of streets akin to the traditional design will be utilized. These “webs” of local collector streets will overlap the slightly larger collector streets, which connect with the more remote and swift existing and planned regional roads.

A major contribution of the comprehensively planned Porta Bella community to the existing community will be the development of large segments of two regional roads, one with a north/south and one with an east/west alignment. Further, off-site traffic which traditionally accompanies incremental or piece-meal development will instead be largely handled on-site, with the inclusion of a multiplicity of land uses.

A hierarchy of streets has been designed to provide convenient automobile access throughout the community, as well as provide much needed regional roads for the Santa Clarita Valley. Vehicular mobility within and through Porta Bella will be accomplished by a system of roads which are of varying sizes depending on their function. There are five general levels to this hierarchy, being; 1) six & four lane highways 2) four lane collector, 3) two lane collector, 4) two lane local street, and 5) a two lane alley.

4.1.1 Six & Four Lane Highways

Porta Bella is adjacent to two existing regional highways, Soledad Canyon and San Fernando Roads. To alleviate the traffic which is currently congested on both of these regional roads, Porta Bella will develop and help to be developed two regional roads both on and off the site. These two regional highways are; 1) Santa Clarita Parkway and, 2) Via Princessa. The following is a brief description of both highways.



1. Santa Clarita Parkway

Santa Clarita Parkway will traverse the site in a north/south alignment, and be located in the draw along the eastern slope of the ridge landform. By locating Santa Clarita Parkway in this easterly draw, a natural sound and sight buffer (the landform) is provided between the surrounding communities and the regional road. Due to the hillside location of Santa Clarita Parkway, its actual paved width should be minimized in an effort to minimize cut and fill associated with this road. The use of a median in Santa Clarita Parkway is discouraged in that it would cause an increase in the paved width, and would necessitate additional cut and fill which may be visible to the surrounding community. The 100 foot right-of-way of Santa Clarita Parkway may be four or six lanes depending upon the final traffic evaluation. Actual pavement may range from 64 to 92 feet. Excess right-of-way which is not necessary for travel or breakdown areas should not be graded level, but should compliment the natural slope and landscape occurring in the area.

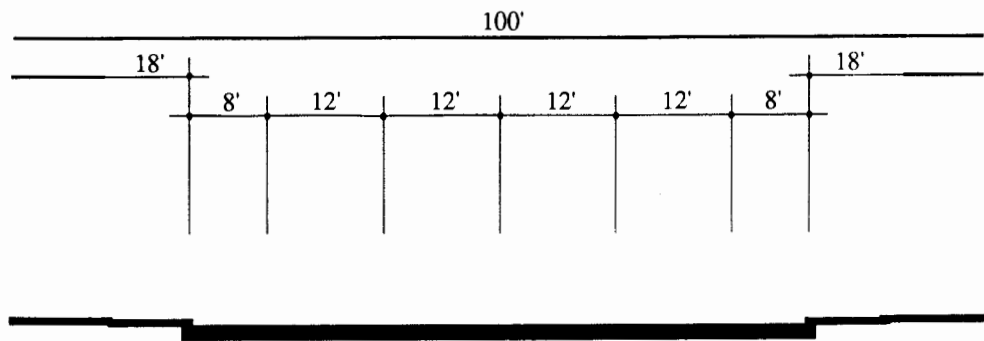
Santa Clarita Parkway will eventually originate at Bouquet Canyon Road and proceed southerly over the Santa Clara River to intersect with Soledad Canyon Road approximately 600 feet east of the planned commuter rail station. This regional road will proceed into the Specific Plan area, rising above the Southern Pacific railroad tracks. The road will continue up the draw east of the ridgeline, intersect with Via Princessa, and exit the site near the south-eastern corner of the project site. This regional road will eventually connect with the Placerita Canyon intersection of the Antelope Valley Freeway (SR14).

2. *Via Princessa*

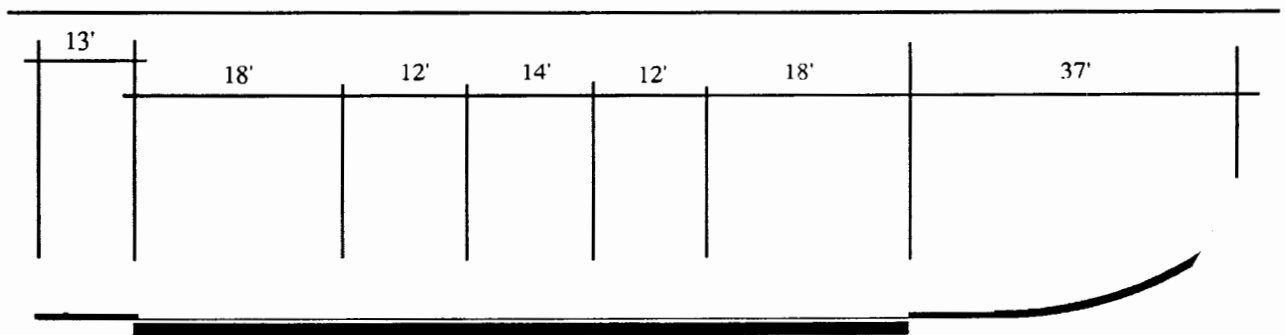
Via Princessa is a four lane highway which will traverse the site in an east/west alignment near the southern boundary of the project site. Its alignment to the south of the site is necessary to utilize the natural “break” through the central ridge near Oro Fino Canyon. A short portion of Via Princessa has been completed westerly of the site. A right-of-way width of 124' will progress through the site, which includes a landscaped median and necessary slope, landscape & setback easements to the exterior edge of the right-of-way. Actual curb-to-curb of Via Princessa will be 84 feet. Via Princessa will be slightly depressed from the adjacent residential areas in an effort to minimize its potential acoustic and visual impacts, as well as afford a more aesthetic relationship of housing to street.

Via Princessa will eventually intersect with the existing Wiley Canyon Road to the west, proceed over the Santa Clara River, San Fernando Road and the Southern Pacific Railroad tracks, with interchange ramps to San Fernando Road. Via Princessa will then traverse Porta Bella as depicted. After exiting the project site near the south eastern corner, Via Princessa will eventually intersect with the planned Golden Valley Road, and proceed easterly to connect with the Antelope Valley Freeway.

Santa Clarita Parkway



Via Princessa
124' R.O.W.

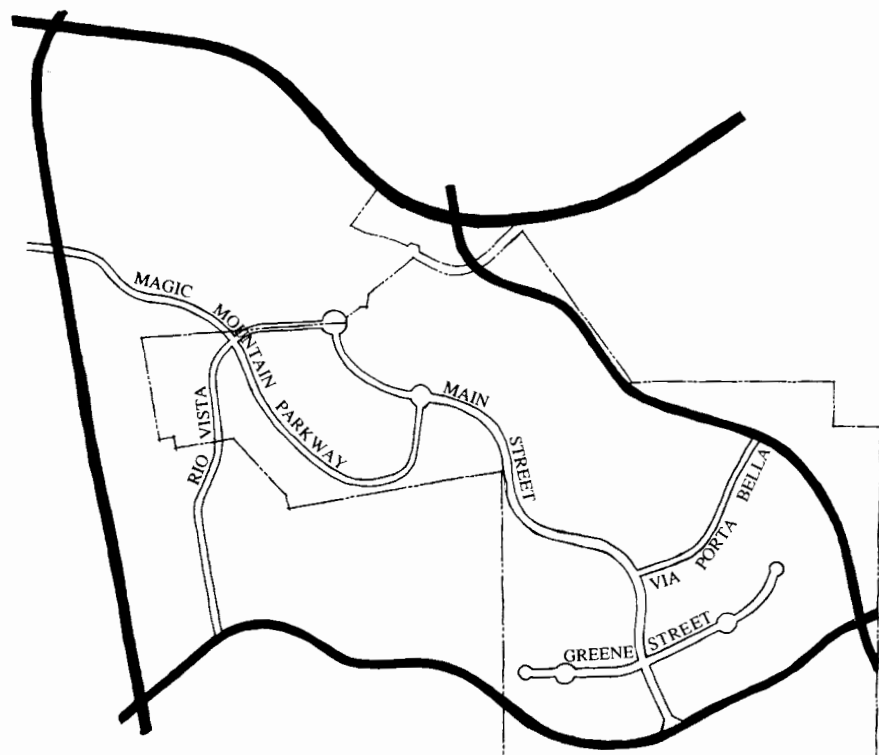


4.1.2 Four-Lane Collector Streets

The four-lane collector street establishes the framework from which the interior community streets connect. The purpose of the four-lane collector is to collect traffic from the local streets and the two lane collectors, and carry it to the community commercial areas or the regional highways.

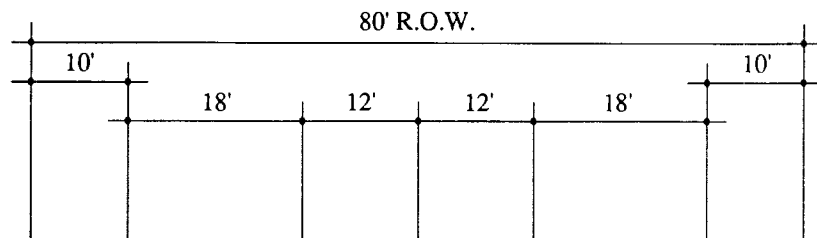
There are five four-lane collectors within the community; 1) Main Street; 2) Greene Street; 3) Rio Vista; 4) Magic Mountain Parkway, and 5) Via Porta Bella, which connects Santa Clarita Parkway to Main Street.

The Four Lane Collector Street has a range of right-of-way widths, depending upon the location and character of the road. The following cross diagram depicts the location of the Four Lane Collector Roads within the community, and their relation to the surrounding regional roads.

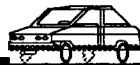
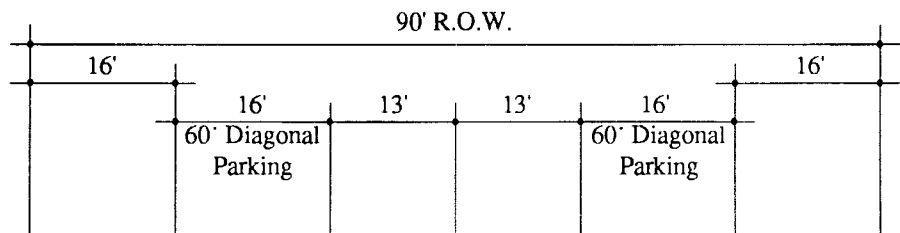


The following are cross-sections which illustrate the typical conditions of the individual Four Lane Collector Streets.

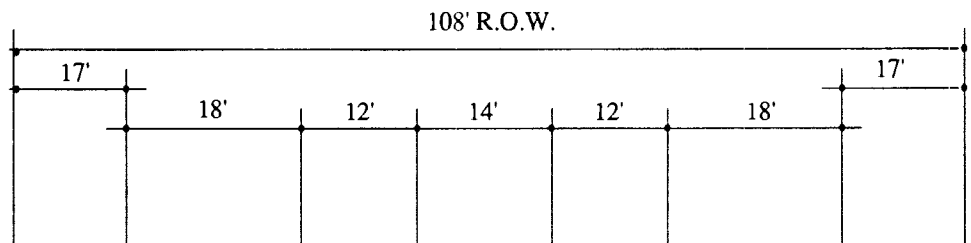
Magic Mtn. Parkway, Rio Vista Roads (into Town Center); Via Porta Bella



Main Street & Rio Vista in the Town Center



Magic Mountain Parkway



Due to the expanded parkway for the landscaping along portions of Greene and Main Streets, the rights-of-way will be 100' and 140' respectively. This will accommodate a curb-to-curb section of 74' with respective landscape setbacks of 13' and 33'.

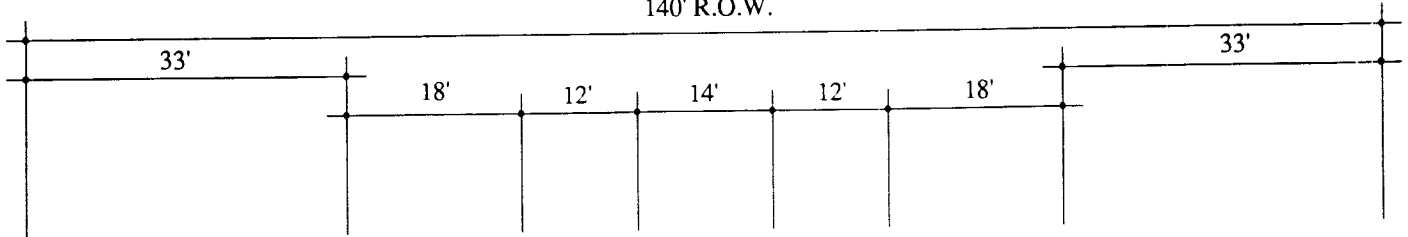
For Main Street, this larger right-of-way will occur between Via Princessa and the intersection of Magic Mountain Parkway. At Magic Mountain Parkway, the character of Main Street changes from a grand collector street to that of a Town Center Main Street, with buildings creating pedestrian scale and containment at the sidewalk's edge. Accordingly, Main Street's right-of-way will narrow to 88 feet through the Town Center.

Through the neighborhoods of the South District, Greene Street will utilize a right-of-way of 100 feet. Greene Street will be two travel lanes in each direction, separated by a landscaped median. Additionally, two roundabouts will further ensure that residential traffic speeds are maintained along these collector streets. Greene Street is named to celebrate the "Old Pasadena" imagery evoked by the architectural work of Greene and Greene, whose distinctive work is an inspiration for the imagery of the South District.

The following are sections of the special character portions of Main and Greene Streets.

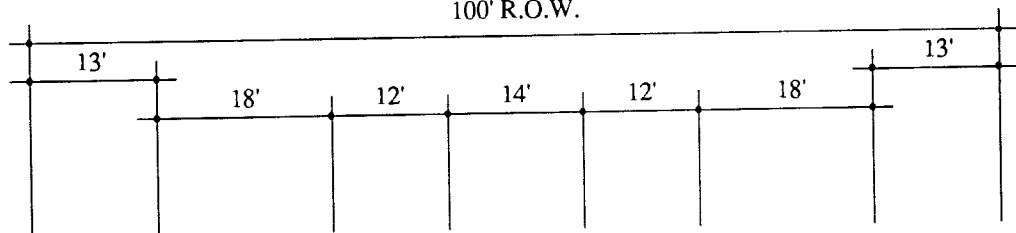
Main Street from Via Princessa to the Town Center

140' R.O.W.



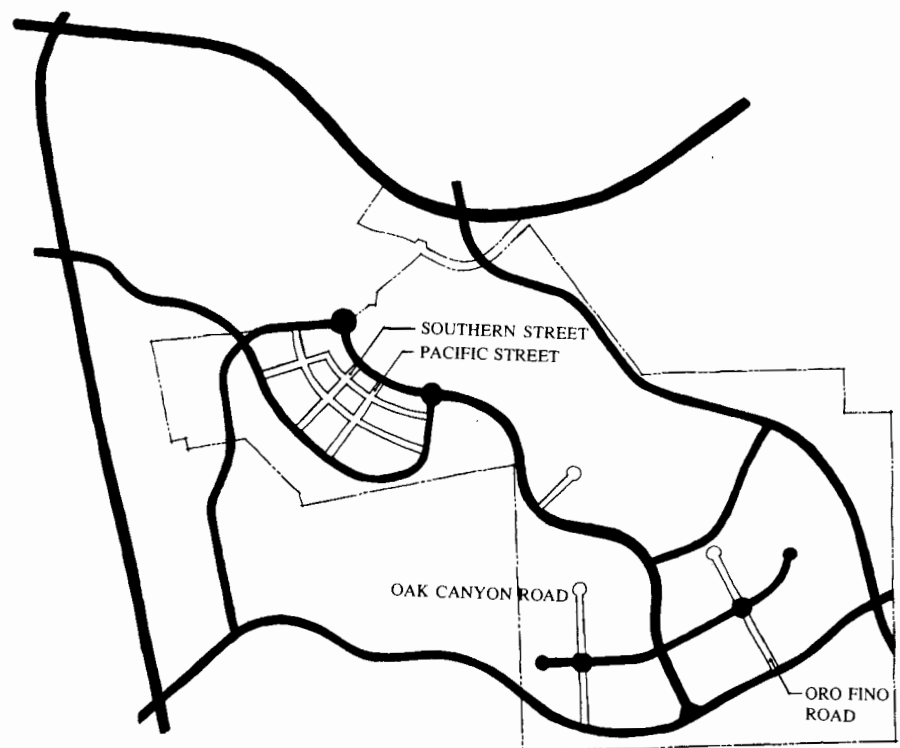
Greene Street

100' R.O.W.

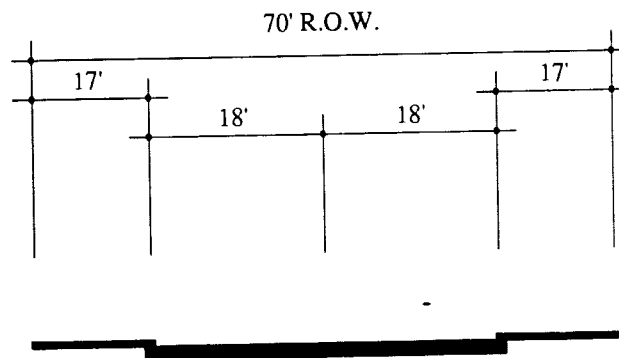


4.1.3 Two-Lane Collector Streets

The primary purpose of this type of road is to provide ingress and egress of traffic from individual neighborhoods, and carry it to the larger collector streets and highways. They will occur in three of the four districts, and are shown on the following diagram. The two lane collector has two travel lanes within a 36 to 40 foot paved travel way. The total right-of-way will be at least 60 feet wide. The following cross section shows the typical conditions.

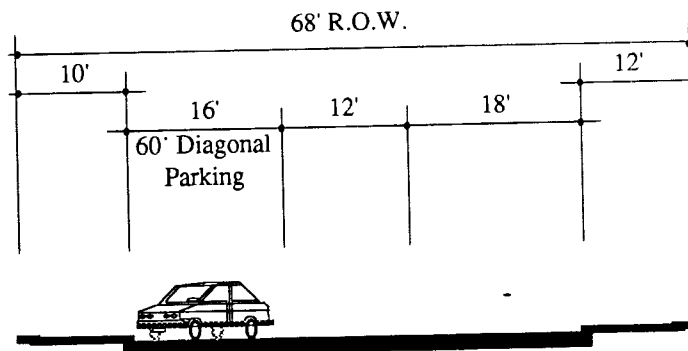


Oakdale Canyon & Oro Fino Canyon Roads



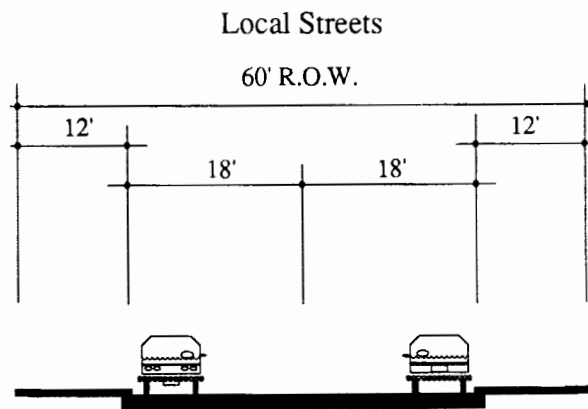
Southern and Pacific Streets are also two-lane collectors, however they will accommodate angled on-street parking for the housing units, and will therefore have the following typical section.

Southern & Pacific Streets



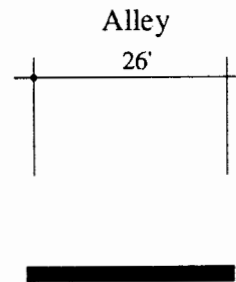
4.1.4 Local Public Streets

Local Streets are intended to serve the local neighborhoods and will be publicly owned and maintained. The width of the local street and the right-of-way will depend on the need for on-street parking and the aesthetic character of the neighborhood it is intended to serve. The following cross-section illustrates the condition that may be found on these public streets.



4.1.5 Alleys

Some neighborhoods within the Specific Plan area will be served by the traditional alley. These alleys will take access from local streets. The following cross-section illustrates the alley condition which may occur.



4.1.6 Private Streets

Private Streets will be permitted in Porta Bella and may be built to a variety of standards depending on the individual need. Any street built to public street standards may become a private street, providing adequate provision is made for long-term maintenance. Private Streets designed to special standards will be evaluated at the time they are proposed. Among the standards that may be considered for private streets in the Plan area are: 1) Special curb treatments, including rolled curbs, battered curbs and no curbs, 2) reduced pavement widths for streets and drives that serve less than 12 dwellings; and 3) special paving treatments, including stamped concrete or unit pavers.

4.2 Commuter Rail

The Soledad District will be the location of the first commuter rail station in the Santa Clarita Valley. This station will be the north-erly terminus of the Metro Rail “Blue Line,” which runs through Los Angeles and terminates in Long Beach to the south. In addition, a 700+ car parking lot is proposed to be provided adjacent to the rail station for weekday commuter parking.

The commuter rail station will serve as a multi-modal transportation station, interfacing with other forms of mobility discussed within this section, including; 1) private bus carriers, 2) local public transit, 3) automobile, 4) bicycle & pedestrian. Also at this station will be the people mover escalator system which will link the station with the Town Center area.

To accommodate the various means of mobility at this station, the following features should be incorporated within its design.

- an automobile parking lot for commuters and ride sharing
- enclosed bicycle storage units
- a drop-off area for commuters (kiss-n-ride)
- an enclosed public transit area
- public telephones/direct free line to local taxi or dial-a-ride
- commercial bus carrier facility
- pedestrian escalator linking the upper area of Porta Bella
- automobile rental car facility

4.3 Public Transit

Santa Clarita Valley Transit (SCVT) currently provides local public transit service throughout the Santa Clarita Valley. Bus stops and shelters will be provided and appropriately located as directed by the SCVT. Bicycle racks for locking of bicycles should also be located at key bus stop areas.

4.4 Escalator System



An escalator system is planned to link the multi-modal transportation center in the Soledad District with the upper level Town Center, which includes the planned Civic Center area. This feature will serve to connect the districts including parking for upper and lower activity areas. The escalator will be designed as a convenient and entertaining pedestrian experience with intermediate patios.

4.5 Pedestrian & Bicycle

A pedestrian & bicycle system has been incorporated into the community with connections to similar off-site trail systems. Bicycle storage and locking areas should be located in key areas of the commercial areas.



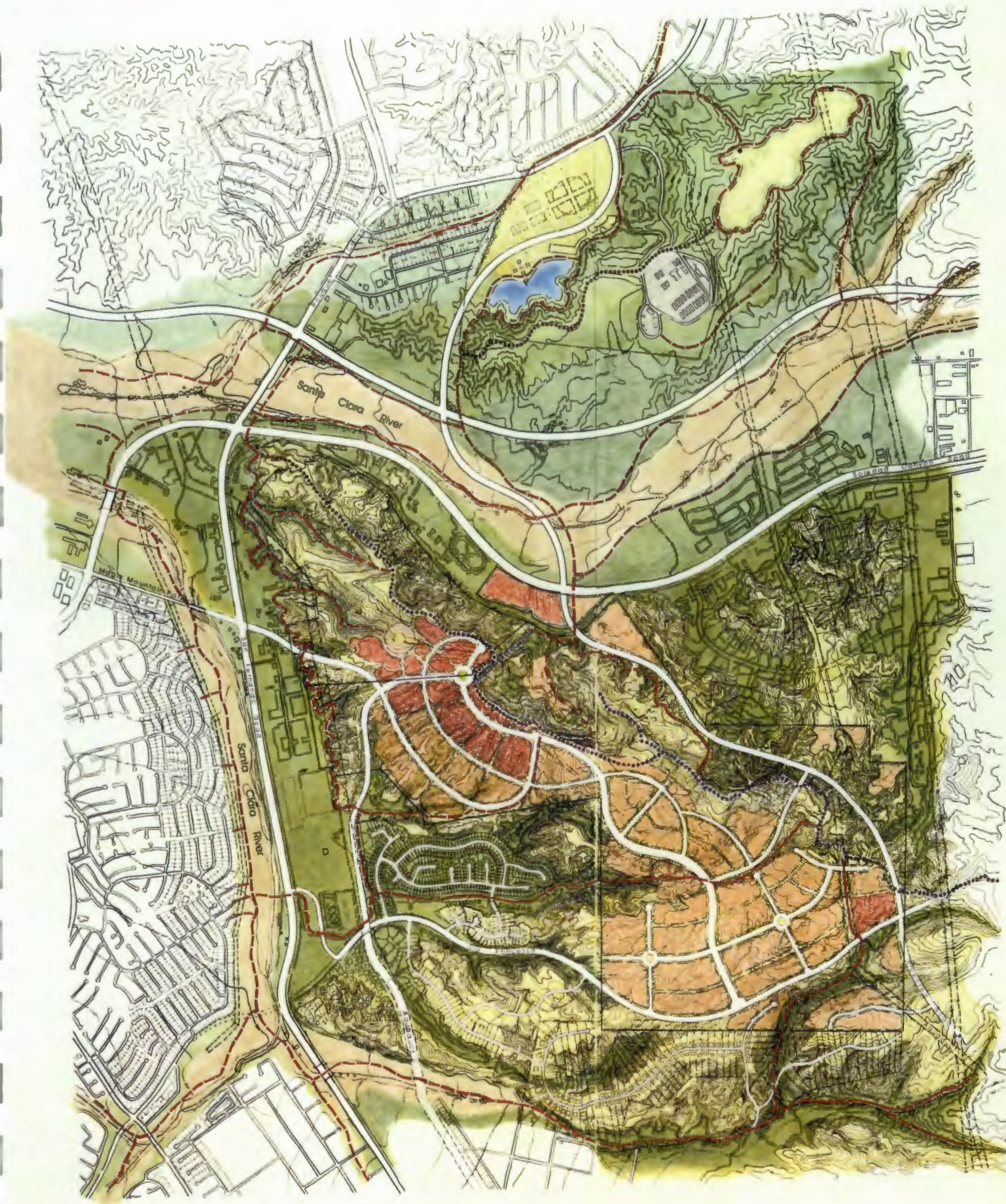
Within the communities, public rights-of-way will accommodate pedestrian oriented sidewalks. The street rights-of-way also are conducive to bicycling, as they are interconnected and encourage lower vehicular speeds. The larger through-streets have incorporated bicycle lanes to encourage recreational and convenience cycling.

An extensive “ridge-runner” trail system has been placed atop the ridge within the Open Space areas of the Specific Plan area. This trail system is intended to be of a natural surface, except where it connects with the Town Center area’s “Crescent Walkway.” This trail system will include vista points, linkage to parks, neighborhoods, Town Center, and the Civic Center.

The Crescent Walkway is intended to be a scenic walkway along the canyon rim in the Town Center paralleling Main Street. View points, gardens, benches, and accent lighting will be located along this romantic walk.

4.6 Equestrian Trails

The equestrian trails depicted on the vicinity trails diagram link with the more regional equestrian system. These trails are intended to be of natural surface, and will blend with the surrounding environment. While no equestrian homes are a part of this facility, accessories for equestrian use, such as trails, motorcycle barriers, horse crossing signage and striping, and other elements may be included along these trail ways to improve the equestrian activity.



LEGEND

- Ridgeline Trail -----
- General Use Trail -----

CENTRAL SANTA CLARITA
VICINITY PLAN

The Anden Group
Prepared by The SWA Group
May 1991

For conceptual design purposes only
Not for construction



- 1.0 *The Specific Plan as Planning Tool*
- 2.0 *Community Context & Integration*
- 3.0 *Land Use Master Plan*
- 4.0 *Transportation & Circulation*
- 5.0 *Master Landscape Plan*
- 6.0 *Community Design & Land Use*
- 7.0 *Parking Regulations*
- 8.0 *Sign Regulations*
- 9.0 *Community Lighting Regulations*
- 10.0 *Infrastructure & Public Utilities*
- 11.0 *Design Review*
- 12.0 *Implementation*

5.0 MASTER LANDSCAPE PLAN

Some of the most “imagable” cities within California are memorable in large part due to their unifying landscape. Monterey’s pines and cypress, Rancho Santa Fe’s eucalyptus forest, La Jolla’s Torrey Pines . The objective of this Master Landscape Plan is to create an equally identifiable and appropriate landscape for this central community.

5.1 Vicinity Landscape Concept

To accomplish this task, a Vicinity Landscape Concept identifies the character of the Valley’s natural landscape and landforms. This plan illustrates the Santa Clarita Valley’s richness in its physical environment. The Valley’s landform and landscape may be characterized by four distinct components. They are:

- 1) River Basin,
- 2) Oak Grasslands,
- 3) Hillside Grasslands, and
- 4) Central Ridgeline

The following are brief character statements and planning approaches regarding each of the landform components:

5.1.1 Rivers Basins:

The Santa Clara River carves gentle and broad paths through the valley in both westerly and northerly directions. Generally, the river encircles the prominent central ridgeline of the City. The Santa Clara River is a true Southern California river, characterized by a broad expanse of flood plain area with a very narrow and usually dry flow line meandering within the flood plain area. The flood plain is almost always dry, and is habitat to durable plants

which as a species can withstand periods of long drought punctuated by torrential flood water inundation. Green pockets of sycamore, cottonwood and willows are interspersed along the river basin. The bulk of the river and tributary system is relatively undisturbed at this time, however channelization and fill projects have begun to modify the natural character of the river.

The Santa Clara River and its tributaries are recognized within a report entitled the *Santa Clara River Recreation and Water Feature Study* (BSI Consultants, Inc., 1991) as having the potential to become one of Santa Clarita's most "prominent and positive" features. The following is an excerpt from the report:

As the river winds its way westward it passes through various environs, some abundant with riparian vegetation and rich in visual quality. Other areas visually convey a sparse and dry sandy river bottom. The various environs within the river basin are valuable components that support abundant wildlife. Some species within the river are listed as endangered by the federal and state governments.

The Santa Clara river is unique in that it serves as one of Southern California's last major watersheds in its natural state. The watershed covers an area of approximately 400 square miles (on its way from the San Gabriel Mountains to the Pacific Ocean). Most of the rivers journey westward is not artificially deviated from its natural course. The river bottom remains in its natural state with broken stretches of concrete levees and rip-rap banks to (constructed as a method of protecting against flood) at certain points along the river.

River Basin Planning Approach:

The Specific Plan project area does not encompass any areas which could be considered River Basin, however it is recognized that the River is an integral component of the character of the Valley. The trail systems proposed within Porta Bella shall connect with the regional river trail system as proposed by the City.

5.1.2 Oak Grasslands:

The oak grasslands are the areas which start from the river basin and go to the hillside areas. The oak grasslands area has been historically most affected by development within the Santa Clarita Valley, due to its ease of construction and access as well as its lower susceptibility to flooding. In undeveloped areas surrounding the valley, good examples of this oak grasslands system may be found. Oaks of various ages speckle the flat and rolling lowlands, and progress in more concentrated fingers up the canyons, especially those with northern exposures.

Oak Grasslands Planning Approach:

The preservation of existing oaks is already a stated objective of the citizens of Santa Clarita. As a part of this Specific Plan, an extensive area for the rehabilitation of the oak grasslands character is proposed. The purpose of this rehabilitation area is to instill for future generations the imagery of Santa Clarita as synonymous with a majestic oak heritage.



This Oak tree re-habitation area may be utilized for mitigation planting of oaks for projects within the valley, under the direction of the City of Santa Clarita. This area has been selected because of its Valley-wide significance due to its visibility.

5.1.3 Hillside Grasslands:

In its natural state, the hillside area is characterized by a sprinkling of oak trees along its northern exposure areas, with primarily chaparral and sturdy grasses occurring on its southern exposures. Also, small canyons and barrancas wind their way up some of the hillsides.

In a historical context, significant development of the hillsides of the Santa Clarita Valley occurred upon the introduction of mass grading techniques to the development industry. Some developments within the hillside area are of a larger context and scale and are more contemporary-conventional in character than the older developments of the valley floor areas. In some instances, unsophisticated hillside grading practices have flattened highly visible areas for development.

Hillside Grasslands Planning Approach:

Within the project area, the landform can be characterized as a table top rising gradually from the southwest up to the ridge, where it drops off suddenly and steeply. The highly visible steep “edges” of the project area are generally natural in vegetation and landform. The protection of these edges was identified early on in the planning stages as essential to keep the green community character. On the more gradual “table top” side of the project area, several neighborhood pockets have been designed in the landform, as depicted in Section 3.0 (Master Plan).

5.1.4 Central Ridgeline:

The central ridgeline on which the Specific Plan project area is located is visible to varying degrees from most areas of the City of Santa Clarita. The landform originates in the San Gabriel mountains to the southeast, and proceeds northwesterly to the intersection of San Fernando and Soledad Canyon Roads. Within the project area near the City property, a “crescent” shape forms along the canyon rim which sweeps into a dramatic physical space in the

“saddle” between the two higher discontinuous ridges. At the terminus of the landform at Soledad and San Fernando Roads, the point has been graded away as a part of a rail line track alignment around the point.



This feature is characterized by sturdy grasses, with occasional oak trees accenting the ridgeline. Past activities of the site have resulted in the cutting of significant portions of the ridgeline within the project area. Vehicular access trails have been cut along and through the landform.

Central Ridgeline Planning Approach:

The Central ridgeline is a unifying visual element with importance to the entire city. Structures within the project area have been kept away from ridge areas of the landform to preserve the natural condition of the viewscape. Within the saddle structures are visible (the crescent area), which will be architecturally relative to the symbolic location which they will occupy and accent the higher ridge on each side.

The landform within the project area has been heavily scarred with grading for firebreaks and roads. Areas where previous activities have damaged the landform will be resculptured and landscaped to blend with the surrounding natural areas.

5.2 Landscape Treatments

To implement the landscape master plan, the Specific Plan area has been categorized into several "Landscape Treatments." These treatments generally correspond with, (1) the natural opens space areas (2) street cross-sections, (3) entry focal points, (4) fuel modification areas, and (5) special considerations within the four districts. These special considerations are diagrams which illustrate the entire district, and highlight district landscape elements with standards and guidelines.

The following sections will explain in more detail the landscape treatment for each category outlined above.

5.3 The Native Landscape Treatment

The Native Landscape Treatment will occur in natural or remediated open space areas. It will consist of native plant materials retained in their existing condition. Also, some areas within the project site have been heavily disturbed by previous land use activities. This is evidenced in that the site is relatively unpopulated by trees and significant vegetation, previous uses have created deep scars in the landform and slope banks, and the majority of the site has been subject to severe grading for fire breaks. As part of this proposal, the landform and slope-bank areas which have been disturbed by previous activities will be naturally contoured and vegetated to match the surrounding landscape.

The Native Landscape Treatment will be the largest landscape area within the community. No water other than natural rainfall will be applied other than to establish young plants, and the maintenance will consist only of trail maintenance and occasional maintenance to reduce fire danger.

There are four landscape characters contained within the Native Landscape Treatment areas on-site, as described in the Landscape Vicinity Plan being: (1) Riparian, (2) Oak grassland, (3) Hillside grassland, and (4) Ridge areas. The plants noted on the master plant palette should be utilized in each area. It is the natural character of the area which determines how many and in what concentration the individual plant species should be applied.

5.3.1 Riparian Woodland Landscape Treatment:



The Riparian Woodland Landscape Treatment has been developed to enrich the quality of riparian habitat on the Specific Plan site. Willows and cottonwoods currently found on-site in limited areas where artificial ponding of water has occurred will augment the coast live oak, elderberry and holly-leaf cherry in the drainage areas of the Riparian Woodland restoration areas. Sycamore and Valley Oaks will be incorporated into the riparian woodland areas to increase the diversity and habitat value of these important riparian areas.

A Riparian Woodland Restoration Plan (RWRP) will be developed to guide the restoration of the riparian woodland areas. As necessary, the RWRP will provide recommendations regarding target numbers for each principal plant species for diversity ratios, guidelines for ideal planting conditions including spacing, distribution and moisture requirements, and long-term maintenance re-

quirements if necessary. The RWRP for the Specific Plan site will be formulated in cooperation with the California Department of Fish and Game and the U.S. Fish and Wildlife Service.

The Riparian Woodland Landscape Palette for the Specific Plan site incorporates a combination of plant species indicative of a diverse riparian woodland habitat. The Master Plant Palette found later within this Section describes which plant materials may be planted within this area.

5.3.2 Oak Grassland Landscape Treatment

The landscape treatment for the Oak Grassland Landscape incorporates species which are presently found within the Santa Clarita Valley in similar situations. Refer to Master Plant Palette for allowable plant materials within this area.

5.3.3 Hillside Grassland Landscape Treatment



The Hillside Grassland Treatment is typically located on exposed south facing slopes, and the upper reaches of north facing slopes above the majority of oaks. Landscape within this area is typically comprised of chaparral, sage, buckwheat, grasses, elderberry and occasional oaks. Refer to the Master Plant Palette for plant materials allowable within this area.

5.3.4 Central Ridgeline Area Landscape Treatment

The ridgeline landscape treatment is typified by grasses and low shrubs. Very few trees grow on the ridgelines within the Santa Clarita Valley due to lack of moisture, transpiration caused by the wind, and generally poorer soils. An occasional oak or similar tree's silhouette will crest the ridgelines, however not in great numbers or concentrations.

The landscape palette for the Ridge Area will consist of species which are presently found within the Santa Clarita Valley in similar situations. Refer to the Master Plant Palette for materials allowable within this area.

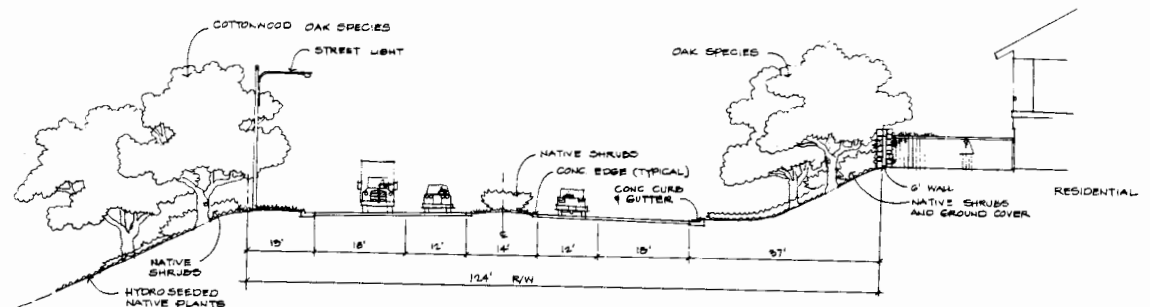
5.4 Landscape Treatments - Streetscapes



Streets within the Specific Plan area have been carefully arranged and dimensioned to establish a traditional community character. The curb-to-curb dimensions of the street are reasonable for vehicles, yet they are not so large as to overpower the other character elements of the community. Parkways have been expanded to accommodate increased landscape and features within all of the street rights-of-way. In some instances, medians have been included in the design to reinforce a street's importance or character. Each streetscape has been carefully considered for its function, its imagery and its relationship to the surrounding product types.

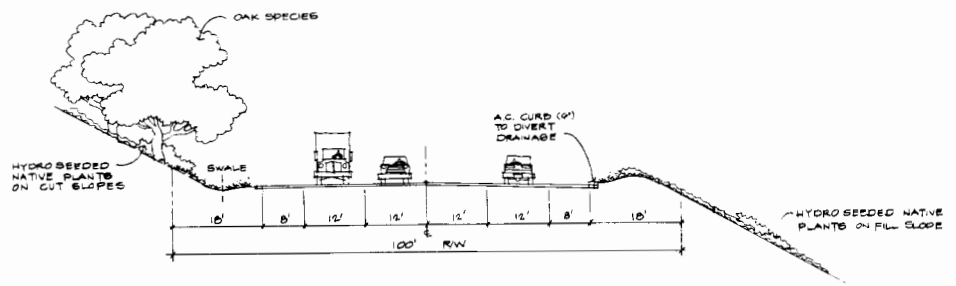
5.4.1 Via Princessa Road Treatment

Via Princessa will be landscaped to reflect the natural landscape and lower density of Porta Bella. Low native shrubs will be placed in the center median, with oak trees defining the northern edge of the road adjacent the slightly elevated community. Cottonwoods will be planted along the southern edge to underscore the Riparian landscape area of the adjacent Oro Fino Canyon. Native shrubs and groundcover will be planted and hydroseeded along the parkways to compliment the native landscape of the area.



5.4.2 Santa Clarita Parkway Treatment

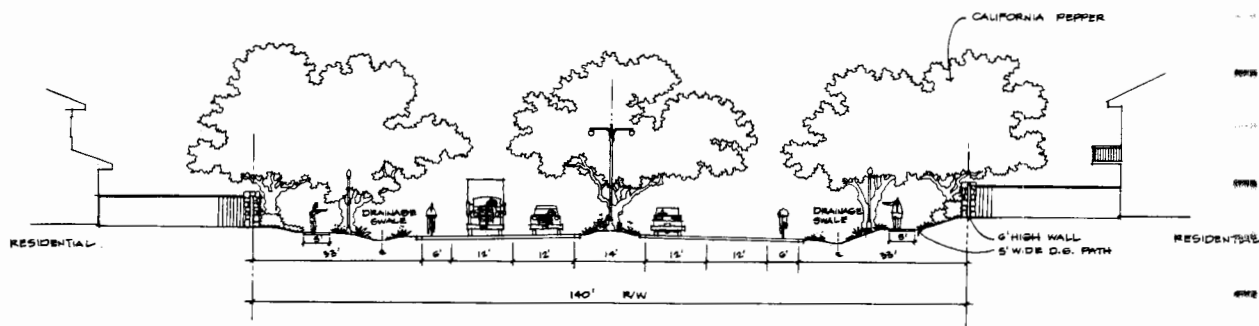
Santa Clarita Parkway will be landscaped with sensitivity to the environment through which it passes. Native groundcover and shrubs will be hydroseeded into areas disturbed by the alignment of the road. As discussed within section 4.0, the use of a center median is discouraged as it will cause additional cut and fill to occur to construct the roadway. However, if a center median is installed for any reason, it should have a minimum width of eight feet, and be landscaped with low native shrubs and groundcover. Trees are not recommended along this road as they would contrast the native landscape. To reinforce the scenic qualities of this highway, exterior curbs will only be utilized in areas which require the diversion of water to the numerous natural water courses.



5.4.3 Main Street Treatment

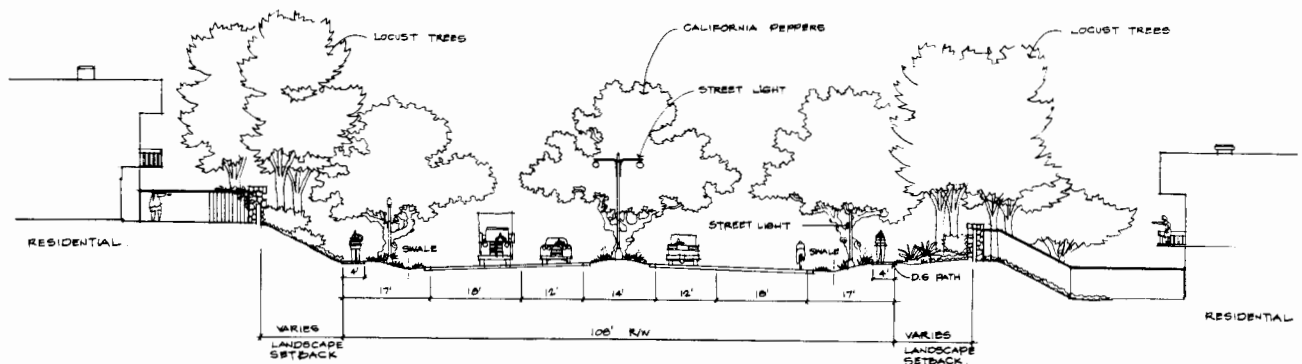
(from Via Princessa to Magic Mountain Parkway)

Main Street's right-of-way is 140 feet, with a majority of this right-of-way being dedicated to parkway rather than roadway. Turning onto Main Street from Via Princessa will be a dramatic visual event. The broad roadway subtly climbs and curves, the view washed with the soft green majesty of California Pepper trees. Peppers will be planted in the center median, as well as two-deep along the 33 foot parkways on both sides of the roadway. To complete the natural imagery, concrete curbs will be substituted with natural drainage swales and native grasses. Native shrubs and grasses will also be blended into the parkways and center median to accentuate the groundplane. A five foot, natural surface decomposed granite (d.g.) pathway will be situated between the double row of Peppers on both sides of Main Street to complete the streetscene. Decomposed granite is a clean, pedestrian inviting surface which feels (and sounds) good to walk on either wet or dry. The Street and walkways will be illuminated by ornamental lighting within the median and the parkways.



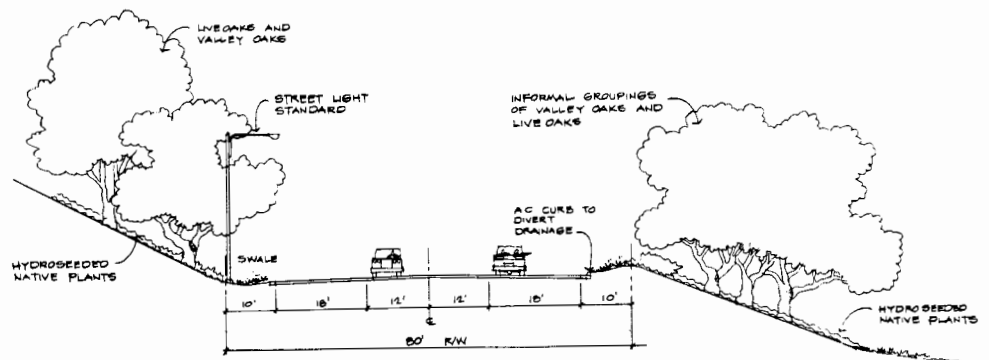
5.4.4 Magic Mountain Parkway Treatment (from Main Street to the Intersection with Rio Vista Road)

The California Pepper/natural swale imagery of Main Street will again be presented along this portion of Magic Mountain Parkway. This continuation of the landscape theme will subtly alert the driver progressing from Main Street of an through-street option other than the pedestrian oriented Main Street through the business district. The Peppers will be utilized within the center median and the first row (closest to street) of trees within the parkway. The second row, which will be nearer the residential homes, will be comprised of locust trees. Ornamental lighting to match Main Street will be located within the median and the parkways to illuminate the groundplane. A four foot wide decomposed granite walkway will be placed within the double tree rows in the parkway. Native shrubs and grasses will finish the groundplane.



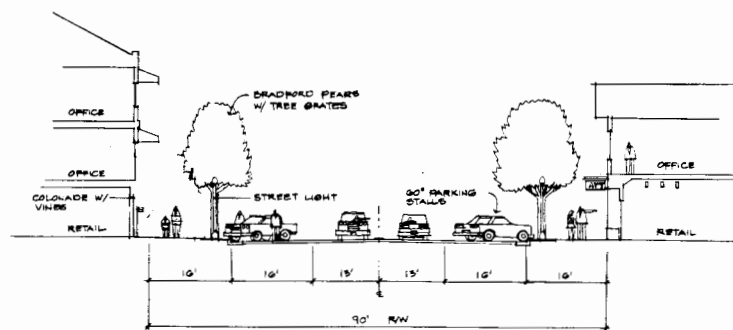
5.4.5 Magic Mountain Parkway, Rio Vista Road and Via Porta Bella Treatments (From project exterior to the intersection of Magic Mountain Parkway and Rio Vista Road)

These roads are important access roads to the site from points east, west and south. The imagery of these roads will reflect the informal Native Landscape Treatments through which they will pass. As with other major character streets in the community, concrete curbing will be replaced with natural drainage swales. Curbing will only be used in areas to divert water into natural drainage courses. Native grasses will be hydroseeded into areas disturbed for the road placement, and for the remaining portion of right-of-way beyond the curb edge.



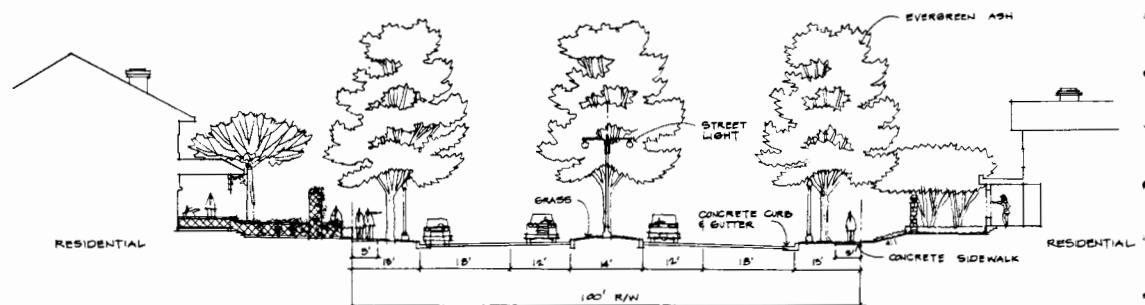
5.4.6 Main Street and Rio Vista Road Treatment (Through the Town Center)

Within the Town Center area, Main Street and Rio Vista will be constructed to a pedestrian, more merchant scale. This scale will reflect the "Main Street" character which will be occurring along these stretches of the streets. Zero setback buildings will frame the road along its right-of-way edge. Bradford Pear trees will be regularly spaced within the 16 foot sidewalk areas in grated tree planter pockets. Sidewalks in the Town Center area should again be reflective of an earlier period, with 2.5' X 2.5' or similar scoring.



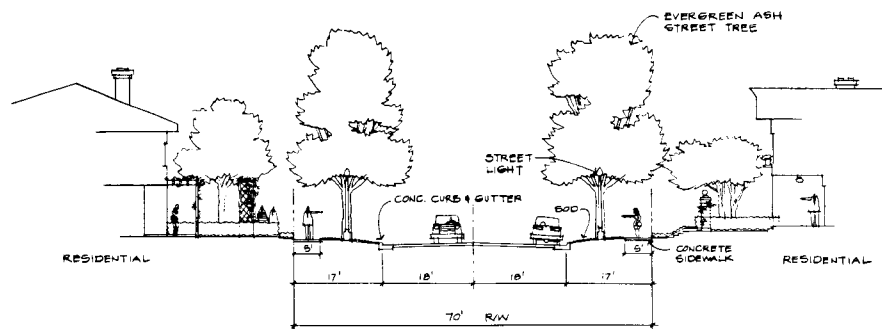
5.4.7 Greene Street Landscape Treatment

The paired single family homes along Greene Street will face onto this street, yet be alley served. No garages or curb cuts will detract from this stately drive. The edges of Greene Street will be strongly reinforced with double rows of Ash canopy parkway trees to provide strong, stately entrance to the residential neighborhoods. Greene Street's intersection with Main Street will be marked with a special landmark landscape and imagery, to further reinforce the entrance to the residential neighborhoods of the South District. Two thematically landscaped roundabouts will be located on formal axis with each other, central to the neighborhoods of the South District. These central green circles will provide an aesthetic, unifying tone to the neighborhoods, as well as reinforcing a sense of community. Adjacent to the property line on both sides of the road will be five foot concrete sidewalks, scored in a 2.5' x 2.5' pattern. Decorative, pedestrian scale street standards will be located along the exterior parkways and in the center median in alignment with the trees.



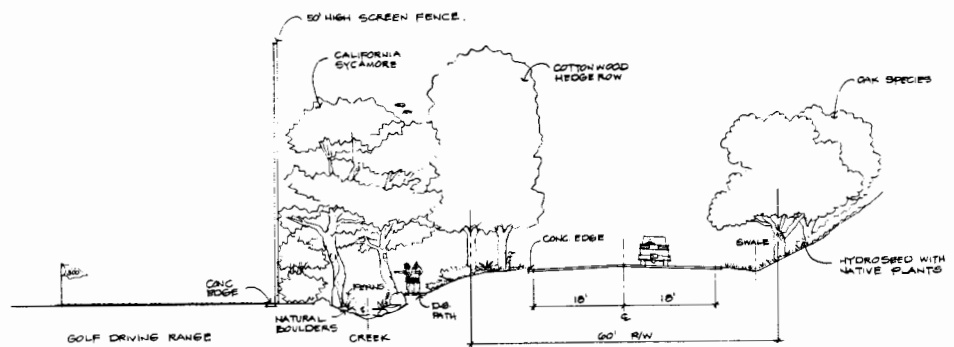
5.4.8 Oro Fino and Oak Canyon Road Treatments

Oro Fino and Oak Canyon Roads will be entrances to the neighborhoods of the South District for those progressing from Via Princessa or Greene Street. These roads are of a smaller scale, reflective of the residential communities they serve. The community's central roundabouts will once again provide an aesthetic view, as they are located within the intersection of the roads and Greene Street. As in Greene Street, Ash trees will define the parkways, with pedestrian scale decorative lighting in alignment with the trees. The green parkway will be followed by a five foot sidewalk, with 2.5"X 2.5" score patterns. Oro Fino and Oak Canyon Roads have been designed with extra wide travel lanes (18 feet) to allow for parallel parking adjacent to curb.



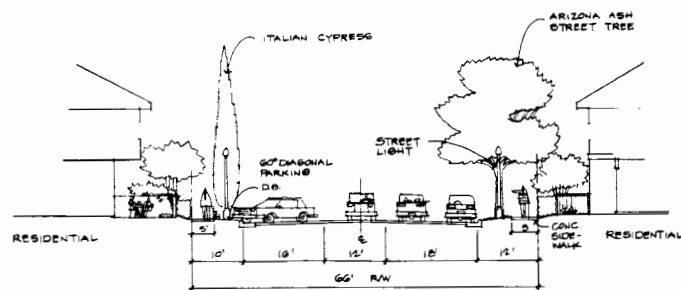
5.4.9 Club Drive Treatment

Club Drive leads from Main Street to the residential and recreational area within the lower "Bowl." The landscape along Club Drive will be a Cottonwood hedgerow on the side of the golf driving range, and a hydroseed of native shrubs and plants on the hill side. Swales will once again replace concrete curbs to reinforce the natural imagery.



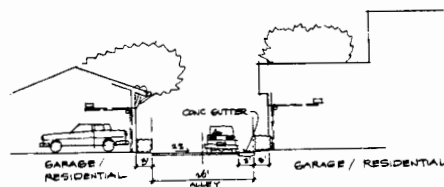
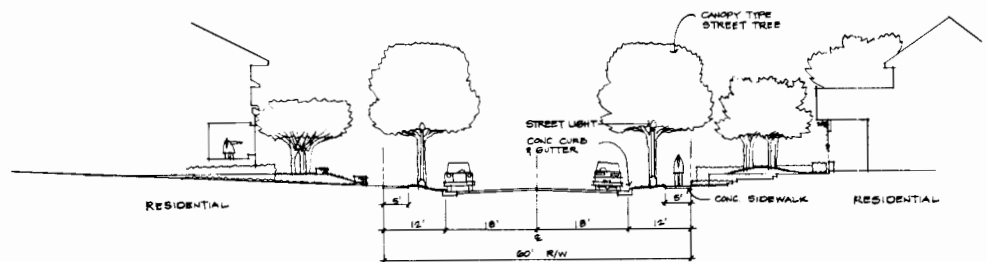
5.4.10 Southern and Pacific Street Treatments

Southern and Pacific Streets are named in tribute to the Southern Pacific Railroad which has strongly influenced the development of the Santa Clarita Valley. These streets will parallel the multi-family community which extends south westerly through the Town Center District. Angled parking is allowed along these streets adjacent to the multi-family housing areas to alleviate the need for on-site parking, and to reinforce the pedestrian scale of the street. On the angled parking side of the street, Italian Cypress are proposed to formally define the edge of curbing. Regularly spaced in alignment with the Cypress Trees will be traditional pedestrian scale light standards. The sidewalk will continue to the edge of right-of-way. On the opposing side of the street, parallel parking will be permitted. Arizona Ash trees will be planted within the 12 foot parkway, and pedestrian scaled street lamps will again be utilized to light the sidewalks and street.



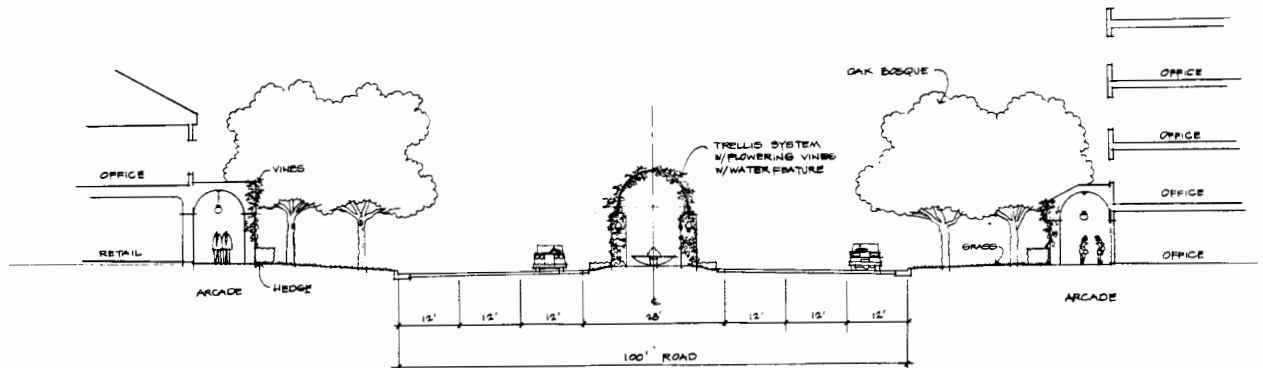
5.4.11 Local Roads/Alleys Landscape Treatment

Typical local roads will have total right-of-way widths of 60 feet, which will include 36 feet of travel way curb-to-curb, and 12 foot parkways. Adjacent to curb, the parkways will be landscaped with canopy street trees centered in an eight foot landscape area, followed by a five foot concrete sidewalk scored in a 2.5' x 2.5' pattern. Pedestrian scale street lamps will be aligned with the trees centered in the eight foot landscape area. Turf will finish the ground plane landscape. Although these landscape areas are within the public right-of-way, the turf will be maintained by the property owner whose home is adjacent the parkway. Parallel parking is permitted on local streets. The 26' alley right-of-way is also illustrated below.



5.4.12 Soledad Rail Station Entry Drive Treatment

The entrance to the Soledad Rail Station serves two purposes, 1) to provide right-in/right-out movement to the Soledad District, and 2) to provide a strong aesthetic and physical connection to Soledad Canyon directly to the community's rail station. This grand entrance will reflect both the symbolic and real importance of the rail station to the community. A broad 28 foot median will be landscaped with a rose trellis, evoking a historical imagery. An oak bosque landscape is recommended for the area immediately adjacent the curb edge, followed by a connecting sidewalk.



Porta Bella Specific Plan

Trees

<i>Botanical Name</i>	<i>Common Name</i>	<i>Treatment Area</i>
<i>Betula verrucosa</i>	European White Birch	Riparian
<i>Cedrus deodara</i>	Deodar Cedar	Street/focal tree
<i>Gleditsia triacanthos</i> "Skyline"	Honey Locust	Street tree
<i>Grevillea robusta</i>	Silk Oak	Hedgerow
<i>Liquidambar styraciflua</i>	Sweet Gum	Street tree
<i>Liriodendron tulipefera</i>	Tulip Tree	Street tree
<i>Magnolia grandiflora</i>	Southern Magnolia	Street tree
<i>Olea europaea</i>	Olive	Street tree
<i>Pinus halepensis</i>	Aleppo Pine	Hillside Grassland
<i>Pistacia chinensis</i>	Chinese Pistache	Street tree
<i>Platanus occidentalis</i>	American Sycamore	Riparian
<i>Platanus Racemosa</i>	California Sycamore	Riparian
<i>Prunus ilicifolia</i>	Holly Leaf Cherry	Riparian
<i>Populus fremonti</i>	Fremont Cottonwood	Riparian, Hillside Grassland
<i>Pyrus calleryana</i>	Bradford Pear	Street tree
<i>Quercus agrifolia</i>	California Live Oak	Riparian, Oak Grassland, Hillside Grassland & Ridge
<i>Quercus lobata</i>	Valley Oak	same as above
<i>Quercus suber</i>	Cork Oak	Street tree
<i>Quercus virginiana</i>	Virginia Oak	Oak Grassland, Hillside Grassland & Ridge
<i>Robinia pseudoacacia</i>	Black Locust	Street tree
<i>Schinus molle</i>	California Pepper	Street tree
<i>Salix species</i>	Willow	Riparian
<i>Ulmus sempervirens</i>	Chinese Elm	Street tree

Shrubs & Vines

<i>Pennisetum setaceum</i>	Fountain Grass	All Treatments
<i>Phormium tenax</i> "atropurpurea"	Purple New Zealand Flax	Streetscape
<i>Diets vegeta</i>	Fortnight Lily	Streetscape
<i>Heuchera sanguinea</i>	Coral Bells	Streetscape
<i>Agapanthus africanus</i>	Lily-of-the-Nile	Streetscape
<i>Hemerocallis species</i>	Day Lilies	Streetscape
<i>Arctostaphylos species</i>	Manzanitas	Hillside Grassland, streetscape
<i>Echium fastuosum</i>	Crown-of-Jewels	Streetscape
<i>Rosa banksiae</i>	Land Bank's Rose	Streetscape
<i>Rosa "mermaid"</i>	Mermaid Rose	Streetscape
<i>Buxus sempervirens</i>	Boxwood	Streetscape
<i>Eriogonum species</i>	Buckwheat	Hillside Grassland, Ridge
<i>Heteromeles arbutifolia</i>	California Toyon	Riparian, Oak Grassland
<i>Rhus integrifolia</i>	Lemonade Bush	Oak Grassland
<i>Rhus ovata</i>	Sugar Bush	Oak Grassland, Hillside Grassland
<i>Rhamnus californica</i>	California Coffeeberry	Oak, Hillside Grassland
<i>Raphiolepis indica species</i>	India Hawthorn	Streetscape
<i>Ligustrum texanum</i>	Texas Privet	Streetscape
<i>Cistus species</i>	Rockrose	Streetscape
<i>Pittosporum species</i>	Pittosporum	Streetscape
<i>Plumbago auriculata</i>	Plumbago	Streetscape
<i>Nerium oleander</i> "dwarf"	Dwarf Oleander	Streetscape
<i>Ficus pumila</i>	Creeping Fig	Streetscape vine

Porta Bella Specific Plan

Parthenocissus tricuspidata
Jasminum polyanthum

Boston Ivy
Jasmine

Streetscape vine
Streetscape vine

Ground Cover

Hypericum calycinum
Hedera helix
Perennial ryegrass

St. Johnswort
English Ivy

Streetscape
Streetscape
Oak, Hillside Grasslands & Ridge.

Fuel Modification Treatment

Trees

Quercus virginiana
Quercus agrifolia
Maintain oaks in pruned and open area condition

Virginia Oak
California Live Oak

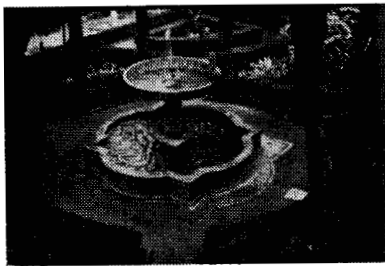
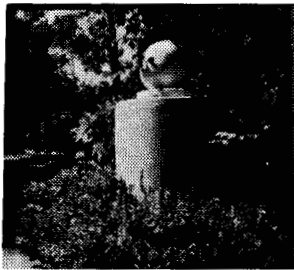
Shrubs & Groundcover

Salvia sonomensis
Acacia redolens
Baccharis pilularis "Twin Peaks"
Cistus crispi
Lantana montevidensis
Heteromeles arbutifolia
Rhus integrifolia
Ceanothus prostratus
Atriplex semibaccata

Creeping Sage
Prostrate Acacia
Dwarf Coyote Brush
Descando Rockrose
Trailing Lantana
Toyon
Lemonade Berry
Squaw Cappel
Creeping Saltbush

MASTER PLANT PALETTE

5.5 Focal Landscapes & Identities



One of the objectives in the design of the residential areas was to unify the neighborhoods. The land use distribution and relationships which have been established within this Plan are therefore more interactive, and have less compartmentalization than many contemporary neighborhoods. It is for this ideal that individual community monumentation will be substituted with other forms of identity, such as special landscape or street forms within the residential areas. In addition, features like the central community roundabouts in the South District are to be landscaped in a public, open manner. For example, identity for these areas may be achieved by a central sculpture or fountain, with ornamental theme trees encircling the exterior edge of the area. Landscape within the community focal areas should be developed with community imagery and ideals in mind, rather than monument walls with signage.

Within the commercial areas, monumentation and identity markers will be permitted subject to the sign regulations found within Section 8.0 of this Plan.

Special landscape elements within the various treatment areas shall also be included to highlight seasonal color and theme. These planting areas may include items such as seasonal color flowers, vegetables, and fruits. Examples of seasonal planting themes are roses, perennials, pumpkins, raspberries, strawberries, and fruit trees.

5.6 Fuel Modification Areas

Development areas which abut a Natural Open Space district must be naturally buffered from fire danger with special plant materials which are less susceptible to fire. The width of this “fuel modification zone” shall be no less than 150 feet. The selection of plants for this area is sensitive, in that the plants should look indigenous to the area, yet the plants must be less susceptible to fire.

5.7 General Landscape Standards

The following standards shall apply to all development within the Specific Plan area, unless noted otherwise within this document.

5.7.1 Irrigation

Generally, there should be a permanent automatic irrigation system installed for all commonly maintained landscaped areas, excluding the Natural Landscape areas. Temporary irrigation systems may be used at the developer’s discretion for areas which will eventually be non-irrigated or for areas which will be irrigated by others in the future.

5.7.2 Installation and Maintenance

Required landscaping shall be maintained in a neat, clean and healthy condition. This includes pruning, mowing of lawns, weeding, removal of litter, fertilizing, replacement of plant materials where necessary, and the regular irrigation of plant material.

The minimum tree size for any installation shall be the 15 gallon size. Larger sizes are recommended for focal landscape areas. Plantings within Natural Open Space areas and within private yards have no minimum size.

Generally, trees larger than 5-gallon size should be double staked until they are strong enough to withstand the normal winds (Eucalyptus trees are exempt from this requirement on the recommendation of a Landscape Architect). "Annual" plantings should be replaced upon decline.

5.7.3 Landscape Planting

- (1) Minimize lawn areas to reduce water application
- (2) Wherever possible use field grown trees that have been grown in similar soils and climatic conditions
- (3) Plant materials should shade the western sides of buildings, especially windows, to reduce heat gain inside homes.

5.7.4 Parking Area Landscape

Parking lots shall be screened from view from adjacent roads by either low walls, shrubs, berms or a combination thereof. The height of the screening shall be determined by the specific site conditions, but generally should be 42".

At least one tree per six parking stalls shall be provided. All trees installed to meet this requirement shall be 15-gallon minimum size. Trees may be placed in either regular or formalized groupings, whichever is most appropriate for the project.

All landscape planter beds in interior parking areas shall be not less than five feet in width, not including curbing.

5.7.5 Loading and Storage Screening

All loading, storage, and refuse collection areas shall be screened from streets and adjacent non-similar use properties with walls, fencing or landscaping, or a combination thereof. Shrub materials utilized exclusively for screening shall be a minimum 5-gallon size at the time of installation. Tree used for screening purposes shall be a minimum 15-gallon size.

5.7.6 Sidewalks

Concrete sidewalks in all public rights-of-way shall be scored in a 2.5' x 2.5' pattern for five and ten foot sidewalks, and a 2' by 2' pattern for four foot sidewalks. This scoring will be reflective of the sidewalk character of earlier neighborhoods.

5.7.7 Front Yard Landscaping

Individual front yard landscaping may be installed by the developer for residential districts in which the average lot size is less than 5,000 square feet. The required landscape shall be installed within 30 days of occupancy and shall be equipped with an underground irrigation system.

5.7.8 Buffer Areas

In areas where buffering between adjacencies is desired, the use of landscape in conjunction with or in lieu of decorative walls is encouraged. The use of monotonous, non-articulated walls will not be permitted in public view areas.

5.8 Maintenance

There will be three levels of maintenance responsibilities: community-wide, neighborhood/district, and private. Maintenance for community-wide landscape areas will be shared by a Master Homeowner's Association, and a landscape and lighting district administered by the City. Landscape areas within an individual neighborhood will be maintained by a homeowner's association set up for that individual neighborhood. Maintenance of yards within the single family ownership areas shall be the responsibility of the resident. The following is a summary of maintenance responsibilities for each group.

Natural Open Space Areas, including fuel modification areas outside of private ownership will require relatively little maintenance, and will be maintained by the Master Homeowner's Association.

Except for the manufactured slopes clearly within a specific project area, manufactured slope areas, and those slopes which are visually important to the whole community, will also be maintained by the Master Homeowner's Association.

Landscape areas within the public right-of-way and expanded parkways will be maintained by a Master Landscape and Lighting District.

Public Parks, including Active Parks and Shared Park facilities and trails will be maintained by the City of Santa Clarita. Private recreation facilities will be maintained by private associations.

Property owners shall be responsible for the maintenance of yards for single family lots.

5.9 Grading

A large contributor to the interest of the project site is its topographical character. The high point on the site is nearly 700 feet above the river plain which generally surrounds the project area. Barrancas, canyons and undulations are found on the project area, many of which have been disturbed by previous grading.

Grading Concept Approach

The grading concept was created by studying the project area's landform, its relationship to the surrounding area, selecting the best connection points to exterior streets and determining areas of environmental significance that must or should be preserved.

5.10 General Grading Regulations

Unless otherwise indicated on the approved grading plan, drainage and terracing shall conform to the provisions of this section for cut and fill slopes steeper than 3 horizontal to 1 vertical (3:1).

5.10.1 Terraces

Terraces shall be established at not more than 30-foot vertical intervals on cut and fill slopes to control surface drainage and debris except that where only one terrace is required, it shall be at mid-height. For cut and fill slopes greater than 60 feet and up to 120 feet in vertical height, one terrace at approximately mid-height may be installed if it is of adequate width to capture and divert runoff. Terrace widths and spacing shall be designed by the civil engineer and approved by the building official. Suitable access shall be provided to permit proper cleaning and maintenance.

5.10.2 Cuts and Fills

Unless otherwise recommended in the approved soils engineering and/or engineering geology report, cuts and fills shall conform to the provisions of this section.

The slope of surfaces shall not be steeper than is safe for the intended use and shall be no steeper than 2 horizontal to 1 vertical unless the owner furnishes a soils engineering or an engineering geology report, or both, stating that the site has been investigated and giving an opinion that a cut or fill at a steeper slope will be stable and not create a hazard.

5.10.3 General Design Standards

(1) The overall shape, height, and grade of any cut or fill slope should be in harmony with the existing contours and the scale of the adjoining natural terrain.

(2) Where toe cut or fill slopes intersect, the ends should be rounded and blended with a minimum radius of twenty five feet.

(3) Where any cut or fill slope meets the natural grade, the end of each slope should be rounded and blended with the natural contours so as to present a natural slope appearance.

(4) Where any cut or fill slope exceeds 200 feet in horizontal length, the slope should be curved in an undulating fashion that reflects the natural terrain.

(5) Landscape, including trees, shrubs and ground cover, shall be installed and/or hydroseeded for all cut or fill slopes in excess of four feet in height.

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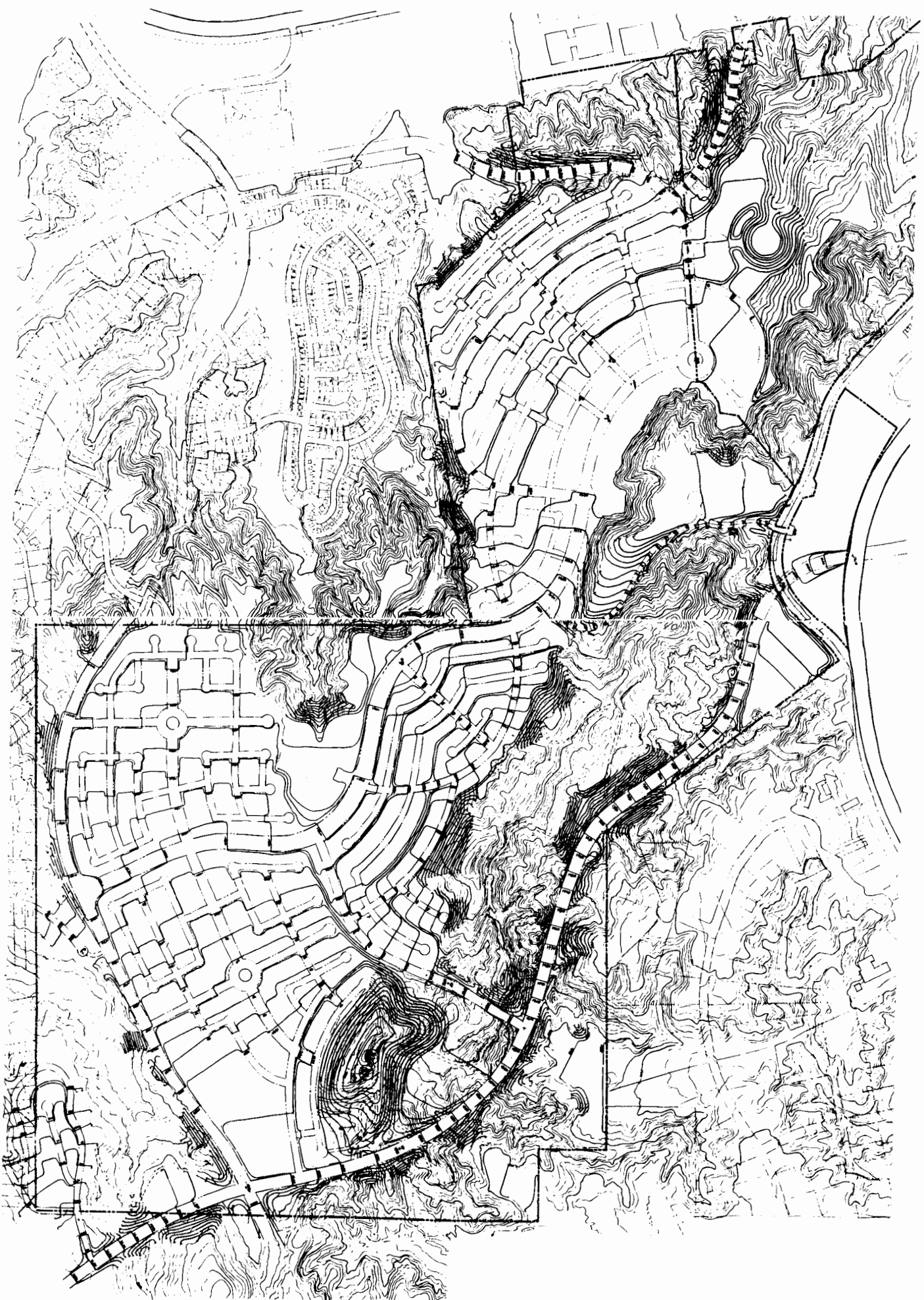
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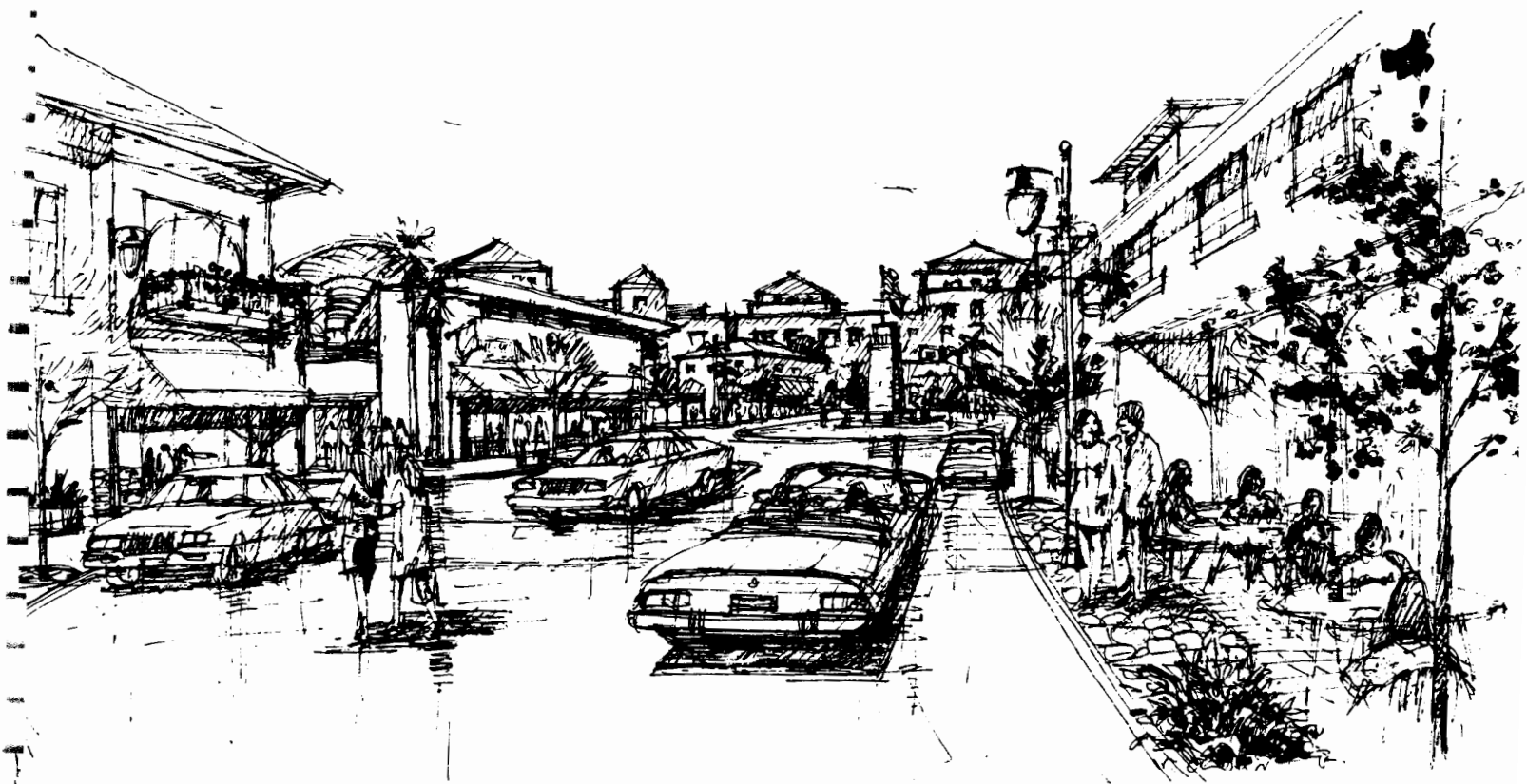


CONCEPTUAL GRADING PLAN

- 1.0 *The Specific Plan as Planning Tool***
- 2.0 *Community Context & Integration***
- 3.0 *Land Use Master Plan***
- 4.0 *Transportation & Circulation***
- 5.0 *Master Landscape Plan***

- 6.0 *Community Design & Land Use***

- 7.0 *Parking Regulations***
- 8.0 *Sign Regulations***
- 9.0 *Community Lighting Regulations***
- 10.0 *Infrastructure & Public Utilities***
- 11.0 *Design Review***
- 12.0 *Implementation***



6.0 COMMUNITY DESIGN & LAND USE STANDARDS

The criteria described in the following paragraphs and exhibits will guide development within Porta Bella. These criteria are intended to ensure a high quality of development within the community, without discouraging individually innovative or imaginative development.

It is recommended that as a part of reviewing the subsequent development applications, the following criteria be evaluated to ensure the ideas which are put forth within the Porta Bella Specific Plan are also included in these subsequent developments.

- The proposed subdivision or commercial development will contribute to the overall objective of maintaining and/or enhancing the community as a socially interactive entity.
- The proposed subdivision or commercial development will interact in a pedestrian manner to its adjacent areas; existing and proposed.
- The proposed subdivision or commercial development will interact and contribute to the Open Space/Parkland system of the community.
- The proposed subdivision or commercial development will provide an interactive and convenient mobility system providing equal access for vehicular and non-vehicular traffic.
- The proposed subdivision or commercial development will create an effective and functional landscape design which contributes to the environmental unity of the entire community.
- The proposed development will utilize and/or allow energy and resource saving techniques, such as natural drainage, permeable surfaces or landscape for climate control.

6.1 Process and Procedure

In any case where the processes or procedures for an interpretation or implementation question are not spelled out or are not agreed to, the order of seeking direction is: Community Development Director, Planning Commission, then City Council. Any process of procedural determination may be appealed in the same order.

Whenever the regulations contained herein conflict with the City of Santa Clarita Zoning Ordinance, the regulations contained herein shall take precedence. In the absence of specific provisions within this code, the requirements of the City's Zoning Ordinance will prevail. Refer to Section 10.0 for further detail regarding Design Review.

6.2 Design Guidelines

The intent of this section is to establish a set of guidelines and an architectural vocabulary which will insure that the built form of Porta Bella will be experienced as a unified whole while encouraging creativity within the individual buildings. It is not intended to dictate a specific architectural style. Also, it should not be a substitute for the design process of the individual architects who will ultimately create the buildings in Porta Bella.

The architectural character of the community may be loosely defined as "New Heritage." Like the community form, this character takes the best elements from contemporary and traditional sources, and blends them in a distinctive manner. This character is inspired by a number of distinct architectural styles such as Monterey, Mission, Italianate and traditional American architec-

ture, including Prairie, Craftsman and California Bungalow. Many of these styles share a common relationship in that most were popular in the early days of California.

“High Contrast” architecture should be considered only if it can be found by the Planning Commission that its particular application would be a time enduring benefit to the community.

6.3 Commercial & Non-Residential Design Guidelines

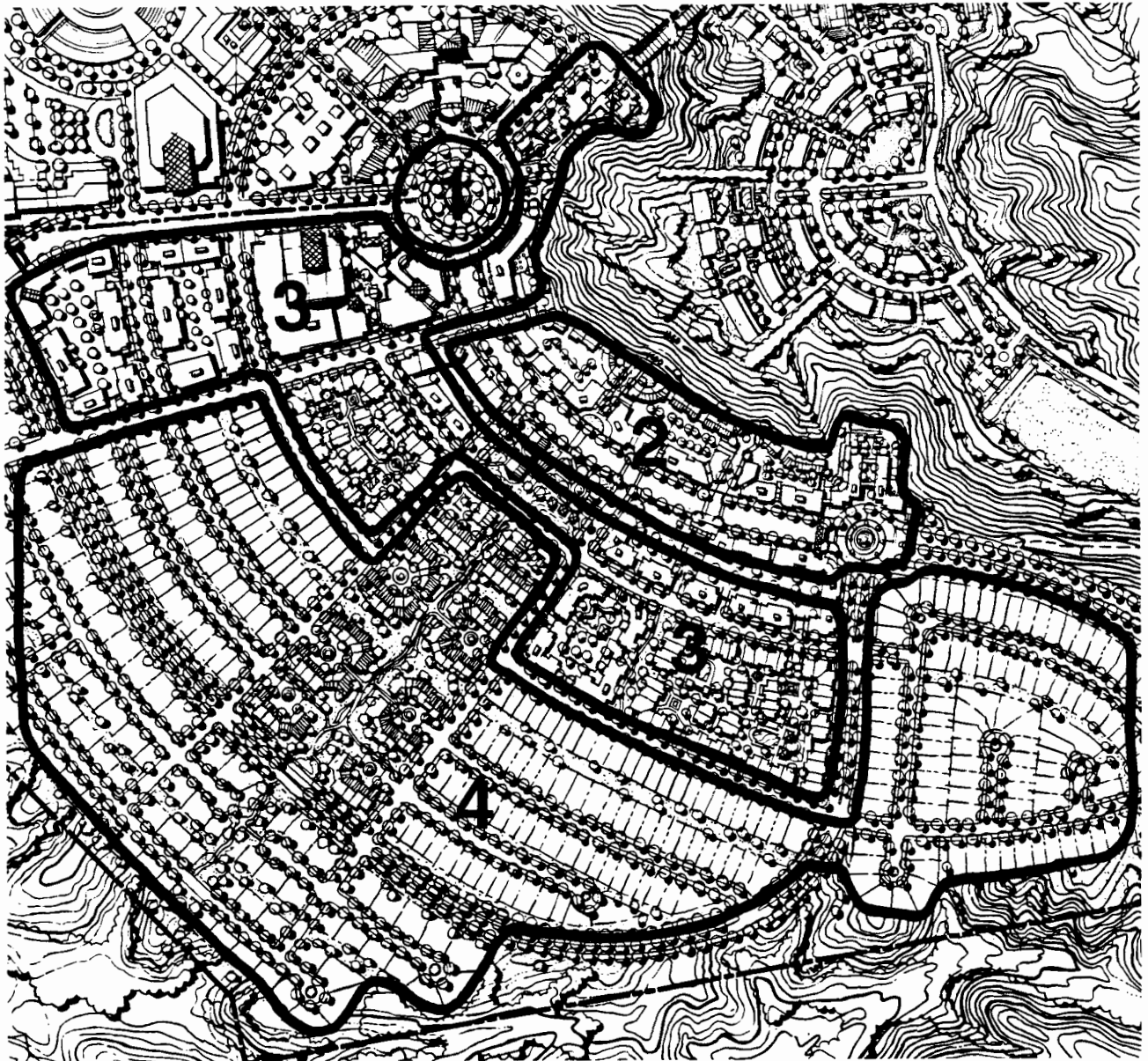
As previously described, Porta Bella is comprised of four “Districts,” being the Town Center, Soledad, Central and South. Three of these four districts have commercial uses incorporated within them. The following is a discussion of the character for each of the commercial areas within their districts, in the following order:

6.3.1 Town Center District

6.3.2 Soledad District

6.3.3 South District

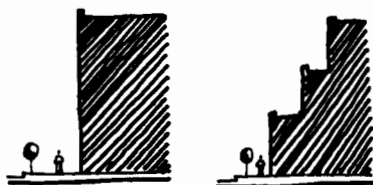
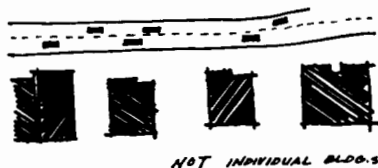
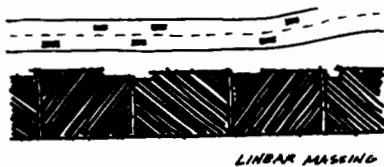
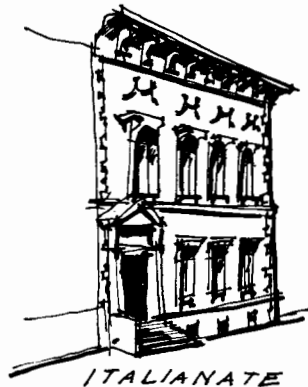




KEY

1. TOWN CENTER CIRCLE
2. MAIN STREET
3. PERIPHERAL AREAS
4. RESIDENTIAL AREAS

TOWN CENTER DISTRICT



6.3.1 Town Center District Commercial

The scale of structures within this area is intended to be of a finer grain than contemporary retail areas in an effort to promote an entertaining, pedestrian district. The Town Center area should exemplify the traditional hometown character of Porta Bella.

Commercial areas within the Town Center are best understood if broken down into several principal components; A. Main Street, B. Peripheral Areas, and C. the Town Center Circle. The architecture should respond to the physical, programmatic, and image requirements within each individual component.

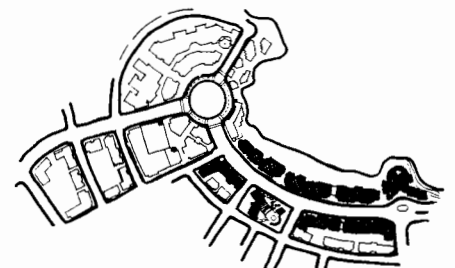
A. Main Street

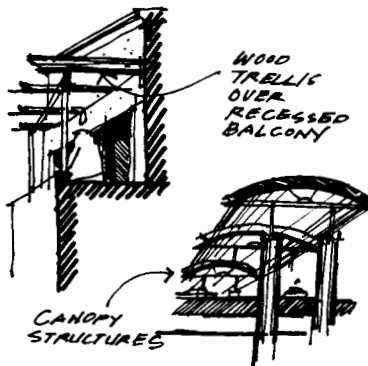
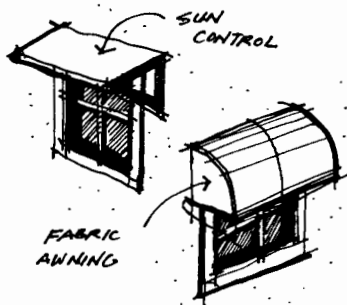
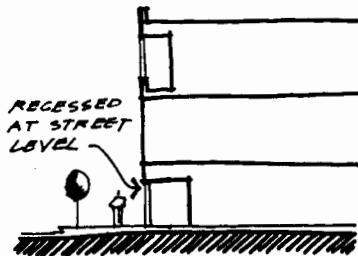
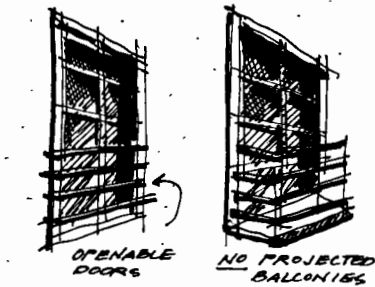
Historical Reference

Examples of this type of project range from Pioneer Square in Seattle to the traffic circle in the City of Orange. Buildings in this context form the historical precedent of American urban architecture. Architecturally, these buildings have been built in styles ranging from Federal to Italianate to French Colonial, all having elements which are appropriate to this project.

Building Mass & Scale

The plan should emphasize its linear relation to the street edge. The linear plan may be created by a single horizontal element or through a series of connected vertical elements. Building massing should be strongly vertical to frame the street. Buildings should not step back from street at upper floors.





Balconies

Balconies are not encouraged at the street side of two-story buildings. They are acceptable at the street side on third floor spaces, but should be limited, to maintain building vertical massing.

Balconies should be recessed rather than projecting. Doors protected by a railing at the wall surface may be used at either second or third floors.

Colonnades

True colonnades are not appropriate along Main Street and are not recommended. Recesses at the street level that do not erode the street edge are acceptable.

Awnings/Canopies/Trellises

These devices are an integral part of this architectural vocabulary. These elements should be carefully integrated into the building design to avoid the appearance of being tacked-on or afterthoughts. These elements may occasionally span the width of the sidewalk at areas such as entrances to the buildings. Wood trellises should be in the "craftsman" tradition (i.e. Greene & Greene, Pasadena). Rough sawn, re-sawn, or heavy timber will not be allowed.

Signage

See signage Section 8.0 for general sign guidelines. Signage for street level tenants must be integrated into the basic building design. Ignoring this criteria will result in signage that appears tacked-on and detrimental to the quality of design. Signage should be viewed as a design opportunity as it is an integral part of the image of this street. No signage will be allowed for upper floor tenants, except painted or etched window signs.

Stairs

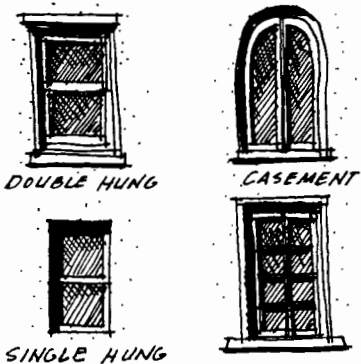
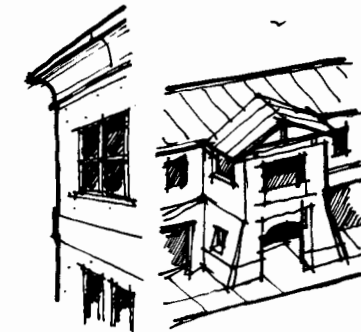
Stairs to all upper story uses will be internal to the building.

Roofs

Flat roofs with walls terminating at cornices or molding should be the principle form along this street. Other roof forms such as a flat pitched, hipped and gable roofs may be used as accents, or to terminate specific architectural elements.

Windows

Use casement, single or double hung windows individually or grouped. Continuous bands of fenestration will not be allowed. Use lintels and sills as architectural accents. Recess windows from exterior wall plane to create shadow lines. Window frames and mullions should have a deep profile i.e. wood windows or clad windows. Residential grade aluminum windows will not be allowed.



DOUBLE HUNG

CASEMENT

SINGLE HUNG



PROJECTIONS ENGAGE SIDEWALK

Storefronts/Doors

Create a rhythm with the storefronts which will allow for flexibility in lease break-ups. Recess storefronts from wall plane to provide shadow lines. Use storefront material with a deep profile such as milled wood. Use materials which will add textures and accent to the building at non-glazed storefront bulkheads. The building entries to access upper floors should be easily identifiable and should not be confused with street level retail entries. The entry element may project into or span the sidewalk to engage the pedestrian.



RECESSED STOREFRONTS WITH
RICH BULKHEAD MATERIALS

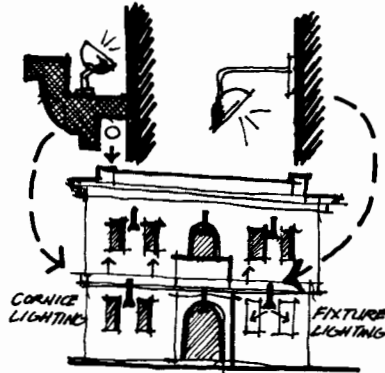
Materials

The building materials should project the image of substance and durability. Materials which are rich texturally should be used at the street level where the pedestrian is in closest contact with the building. The use of local materials is encouraged, such as Bouquet Canyon Stone. Appropriate Materials include but are not limited to:

- Stone/stone veneer
- Masonry - split-faced, polished
- Cement plaster
- Tile - glazed, quarry
- Concrete/pre-cast concrete
- Wood for trellises, accents
- Wood doors/storefronts/windows
- Asphalt roof shingles
- Flat concrete roof tile
- Copper roofs
- Metal roofs
- Fabric awnings - canvas, synthetic

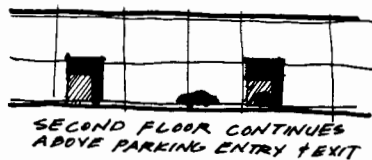
Colors

This guideline will not attempt to establish a specific color palette but rather determine the range of colors which are appropriate. Using building products made from local materials will establish a color range of their own. Muted colors (i.e. the darker end of the value scale) are appropriate - rust, ochre, forest green, grey, etc. Pale colors are appropriate - tan beige, off-white. Colors at the brighter end of the value scale are not appropriate except as accents in limited areas i.e. bright greens, yellows, purples, etc. Use of changes in materials as accents in lieu of painted accents is encouraged.



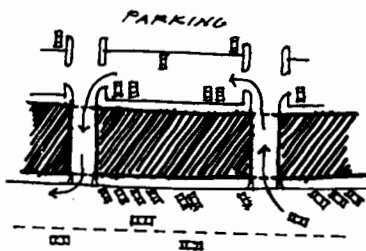
Building Lighting

Building facades should be lit to enhance activity and security at street level. Buildings should not rely entirely on spill-over from street lights or from storefronts for illumination. In general, the building lighting should be designed so that the light source is not visible or the exposed fixture is an integral part of the design. Building lighting should be directed to avoid glare or blinding of pedestrians or in upper level residence. The lighting of upper floors and architectural feature is encouraged.



Parking

Parking for buildings on Main Street will be 60° parking in the street, surface parking behind building and structured parking below buildings in some cases. Provide access to surface and structured off street parking from Main Street, but minimize breaks in buildings for parking access. Where possible, maintain continuity of upper level at parking entry through building. If building is above surface parking screen parking from street and avoid placing building on pilotis. Provide natural light to parking areas where possible.



Loading

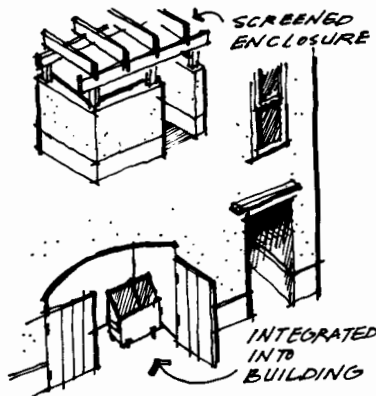
Provide accessibility for loading and deliveries from interior portion of site where possible. Limit amount of loading for the street.

Mechanical Equipment

Screen all equipment from visibility from street and adjacent buildings.

Antennas and Satellite Dishes

Screen dish antennas from visibility from street and adjacent buildings.



Gutters/Downspouts

Provide gutters at areas of pedestrian access such as building entrances. Downspouts and gutters shall be hidden unless they are an integral part of the architectural design.

Skylights

Do not use skylights on sloping roofs visible from the street. Use of skylights is encouraged to provide upper floor tenants a maximum of natural light.

Trash Enclosures

Trash receptacles should not be visible from the street and shall be located in enclosure or integrated into building. If enclosure is visible from above, provide screening over the top that is architecturally compatible with the building.

Mailboxes

First floor commercial tenants shall provide for suite-to-suite delivery to comply with U.S. Postal regulations. Upper floor residential tenants shall have ganged mailboxes in the building lobby. Upper floor office tenants shall have a mailroom at the first level of the building.



B. Peripheral Areas

Only the sections listed below vary from the guidelines for “Main Street.” Refer to those guidelines for sections not shown here.



Historical Reference

American suburban architecture which re-interpreted European influences, such as Italian Renaissance, Georgian, and Beaux Arts, eventually developed into more purely American styles such as Prairie style. Elements found in these examples are the basis of the architectural vocabulary for this portion of the Town Center.

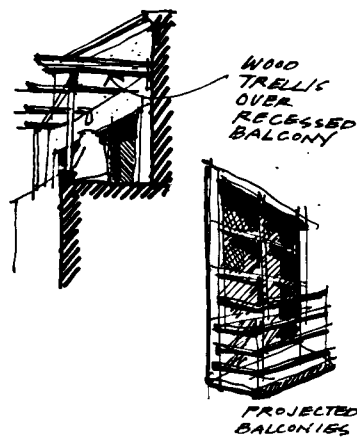


Building Mass and Scale

The plan may be linear or massed based on the individual site. The elevations may have stronger horizontal emphasis, and the buildings may step back from the street at the upper level.

Balconies

Use of balconies at office building should be minimized. Balconies at upper floors of residential are encouraged, and they may be recessed or projecting.

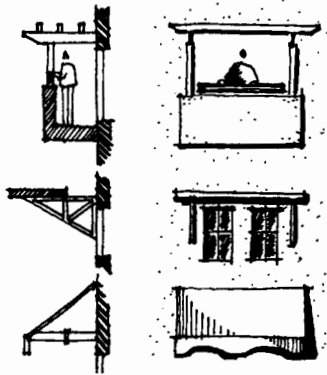


Colonnades

Recessing all first floor storefronts from the building plane to create a protected walkway is acceptable.

Awnings/Canopies/Trellises

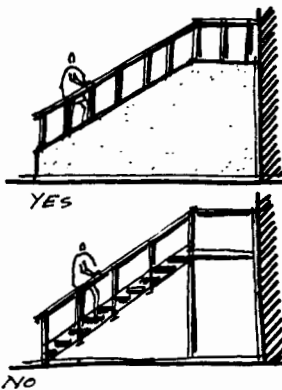
Use of these elements should be carefully integrated into the building design to avoid the appearance of being tacked-on or afterthoughts. These elements should not encroach into the side-



walk except at building entrances. Wood trellises should be in the “craftsman” tradition (i.e. Greene & Greene). Rough sawn, re-sawn, or heavy timber trellises will not be allowed.

Signage

See signage Section 8.0 for General Sign Guidelines. Signage for street level tenants must be integrated in to the basic building design. No signage will be allowed for commercial uses above the first floor, except for painted window signage

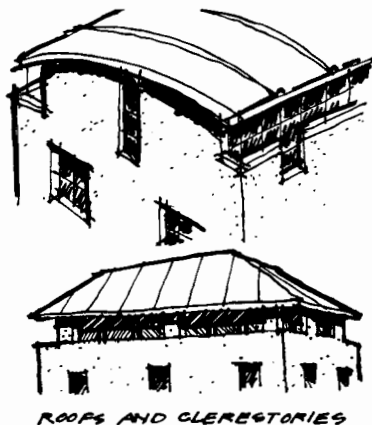


Stairs

Stairs to all upper story uses will be internal to the buildings. If first floor residences are not at street grade, stoops from the street may be used to access individual units.

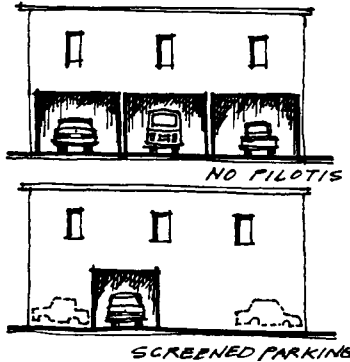
Roofs

Hipped and gable roofs with a flat pitch (30° max) will be the principal roof forms. Flat roofs may be introduced to accent a specific architectural elements.



Windows

Use casement, single or double hung window individually grouped. Continuous bands of fenestration such as clerestories may be used at upper floor of office buildings. Use lintels and sills as architectural accents. Recess windows from exterior wall plane to create shadow lines. Window frames and mullions should have a deep profile i.e. wood windows or clad widows. Residential grade aluminum windows will not be allowed.



Parking

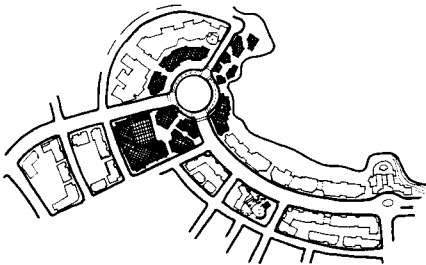
If building is above surface parking, screen parking from street and avoid placing building on pilotis. Where parking faces the interior of the property, “tuck under” parking is acceptable. Provide natural light to parking areas where possible.

Loading

Provide accessibility for loading and deliveries from interior portion of site.

Mailboxes

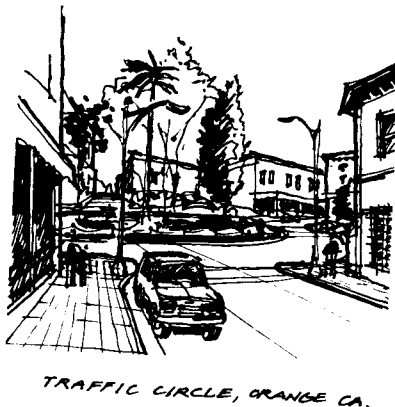
First floor commercial tenants shall provide for suite-to-suite delivery to comply with U.S. Postal regulations. Upper floor residential tenants shall have ganged mailboxes in the building lobby. Upper floor office tenants shall have a mailroom at the first level of the building.



C. Town Center Circle

Historical Reference

Plaza's, piazzas, and town squares have been civic focal points since cities have existed. They range from landscaped traffic circles, such as the traffic circle in the City of Orange, to intimate neighborhood gathering places, as found in most Italian cities. The Town Center Circle should be the physical, cultural, and civic center for all of Santa Clarita



The Town Center is where the architecture for Main Street and the peripheral residential and offices will interact. Since this area will be a mix of uses, scales and sizes, its architecture should reflect this.



The guidelines for the buildings in the Town Center Circle are to be a combination of the previous two sections. The two story office over commercial, and the theater/retail complex should reflect the guidelines for Main Street. The retail area and restaurant row are more appropriately scaled to the guidelines for the peripheral uses/offices.



PIAZZA SANTO SPIRITO, FIRENZE

A unique architectural opportunity within this area is the movie theater. Movies create a fantasy world and the buildings which house these have a strong tradition of being an integral part of that experience. Unfortunately, “contemporary” planning practices have often discouraged this. While conforming to the general guidelines of the sections above, this building should be exuberant in its expression. Its signage, marquees, architecture and lighting should promote a return to its historical roots, as a true place of entertainment.

Only the following sections contain changes specific to the Town Center Circle:

Balconies

Large second-story balconies outside retail areas and at upper floors of restaurants facing the Town Center Circle are encouraged.

Colonnades

Limited use of colonnades to create a layering outside the retail uses or an area for outside seating is acceptable. Areas above the colonnades may be used as second-story balconies.



BALCONY AND COLONNADE



ARCHITECTURAL ROOF ELEMENTS

Signage

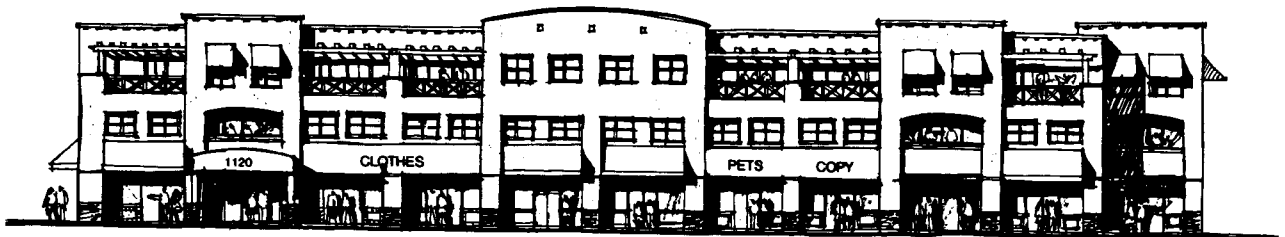
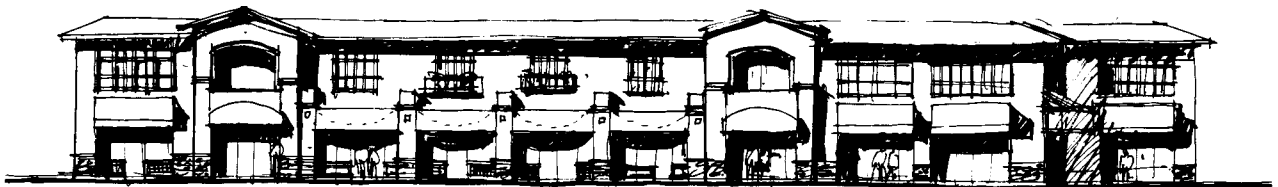
Upper level retail, restaurant and commercial uses may have signage to conform to the General Sign Guidelines, Section 8.0.

Stairs

Stairs to second story retail or restaurant uses may be exterior. Exterior stairs may have open balusters and railing but should be solid below the tread level.

Roofs

Roof forms within the Town Center Circle may vary from those noted in the two previous sections. Use of domes, mansards, vaults, etc. may be used to accent specific architectural forms.



6.3.2 Soledad District Commercial

The Soledad Mixed Use area is intended to blend uses such as convenience retail, service, office and transportation uses which support the rail station function of the area, and should not compete with the specialty or entertainment uses permitted within the Town Center District. The area will, however, be aesthetically complimentary to the Town Center, reaffirming their physical linkage.

The rail station should be the most identifiable and prominent structure within that area. All structures within this area should relate in architecture and orientation to this central rail station.

The purpose of creating separate Office and Business Park areas is to enable clusters of larger scale buildings to be constructed within local proximity to the Mixed Use areas, particularly the Multimodal transportation facility. This relationship between these uses and the transportation facility is intended to act as an employment base for people utilizing the rail or buses.

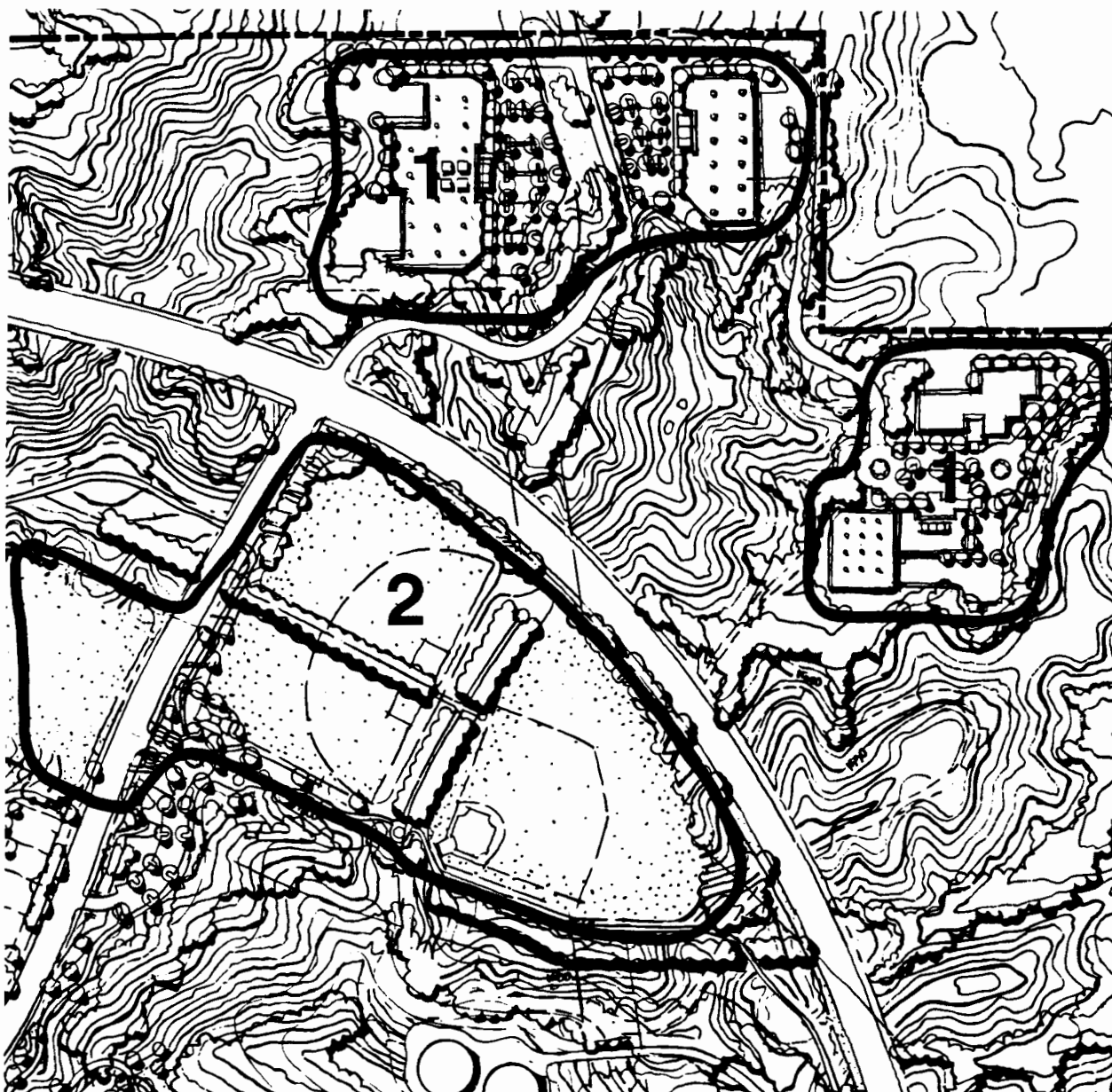
Historical Reference

As in the Peripheral Area of the Town Center, the Soledad District reflects to the American suburban architecture which re-interpreted European influences, such as Italian Renaissance, Georgian, and Beaux Arts, eventually developed into more purely American styles such as Prairie style. Elements found in these examples are the basis of the architectural vocabulary for this portion of the Town Center.



KEY

1. RAIL STATION AND COMMERCIAL
2. OFFICE / BUSINESS PARK
3. MULTI FAMILY RESIDENTIAL
4. SPORTS CLUB / GOLF DRIVING RANGE
5. PEOPLE MOVER / ESCALATOR



KEY

1. BUSINESS PARK
2. COMMUNITY PARK

SOLEDAD DISTRICT

Building Mass and Scale

The plan may be linear or massed based on the individual site. The elevations may have stronger horizontal emphasis, and the buildings may intermittently step back from the pedestrian level at the upper level. Office buildings which will occupy both sides of Santa Clarita Parkway may be more vertical, creating a “frame” around the Parkway. The Business Park area located further along Santa Clarita Parkway should be designed with a structural relationship to the Parkway, rather than a parking lot foreground.

Balconies

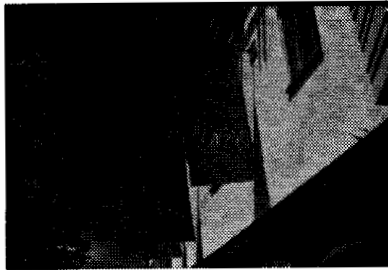
Recessed or slightly projecting balconies at upper floors are permitted.

Colonnades

Recessing all first floor store fronts from the building plane to create a protected walkway is acceptable.

Awnings/Canopies/Trellises

These devices are an integral part of this architectural vocabulary. These elements should be carefully integrated into the building design to avoid the appearance of being tacked-on or afterthoughts. These elements may occasionally span the width of the sidewalk at areas such as entrances to the buildings. Wood trellises should be in the “craftsman” tradition (i.e. Greene & Greene). Rough sawn, re-sawn, or heavy timber will not be allowed.



Signage

See signage Section 8.0 for General Sign Guidelines. Signage for tenants must be integrated into the basic building design. Ignoring this criteria will result in signage that appears tacked-on and detrimental to the quality of design. Signage should be viewed as a design opportunity as it is an integral part of the image of this street.

Stairs

Stairs to all upper story uses will be internal to the building.

Roofs

Hipped and gable roofs with a flat pitch (30° max) will be the principal roof forms. Flat roofs may be introduced to accent a specific architectural elements.

Windows

Use casement, single or double hung window individually or grouped. Continuous bands of fenestration such as clerestories may be used at upper floor of office buildings. Use lintels and sills as architectural accents. Recess windows from exterior wall plane to create shadow lines. Window frames and mullions should have a deep profile i.e. wood windows or clad widows. Residential grade aluminum windows will not be allowed.

Frontages/Doors

Create a rhythm with the frontages which will allow for flexibility in lease break-ups. Recess frontage from wall plane to provide shadow lines. Use storefront material with a deep profile such as

milled wood. use materials which will add textures and accent to the building at non-glazed storefront bulkheads. The building entries to access upper floors should be easily identifiable and should not be confused with street level entries. The entry element may project into or span the sidewalk to engage the pedestrian.

Materials

The building materials should project the image of substance and durability. Materials which are rich texturally should be used at the street level where the pedestrian is in closest contact with the building. The use of local materials is encouraged, such as Bouquet Canyon Stone. Appropriate Materials include but are not limited to:

- Stone/stone veneer
- Masonry - split-faced, polished
- Cement plaster
- Tile - glazed, quarry
- Concrete/pre-cast concrete
- Wood for trellises, accents
- Wood doors/storefronts/windows
- Asphalt roof shingles
- Flat concrete roof tile
- Copper roofs
- Metal roofs
- Fabric awnings - canvas, synthetic

Colors

This guideline will not attempt to establish a specific color palette but rather determine the range of colors which are appropriate. Using building products made from local materials will establish a color range of their own. Muted colors (i.e. the darker end of the value scale) are appropriate - rust, ochre, forest green, grey, etc. Pale colors are appropriate - tan beige, off-white. Colors at the

brighter end of the value scale are not appropriate except as accents in limited areas i.e. bright greens, yellows, purples, etc. Use of changes in materials as accents in lieu of painted accents is encouraged.

Building Lighting

Building facades should be lit to enhance activity, aesthetics and security. Buildings should not rely entirely on spill-over from street lights or from frontages for illumination. In general, the building lighting should be designed so that the light source is not visible or the exposed fixture is an integral part of the design. Building lighting should be directed to avoid glare or blinding of pedestrians or in upper level residence. The lighting of upper floors and architectural features is encouraged.

Parking

Surface and structured parking below buildings in some cases. Minimize breaks in buildings for parking access. Where possible, maintain continuity of upper level at parking entry through building. If building is above surface parking screen parking from street and avoid placing building on pilotis. Provide natural light to parking areas where possible.

Loading

Provide accessibility for loading and deliveries from interior portion of site where possible. Limit amount of loading for the maneuvering lanes.

Mechanical Equipment

Screen all equipment from visibility from street and adjacent buildings. Equipment in the Soledad District shall also be screened from view from the escalator and view areas within the Town Center District. Equipment rooms are highly encouraged. No exposed roof mounted equipment will be allowed in the Soledad District.

Antennas and Satellite Dishes

Screen dish antennas from visibility from streets, adjacent buildings and Town Center District.

Gutters/Downspouts

Provide gutters at areas of pedestrian access such as building entrances. Downspouts and gutters shall be hidden unless they are an integral part of the architectural design.

Skylights

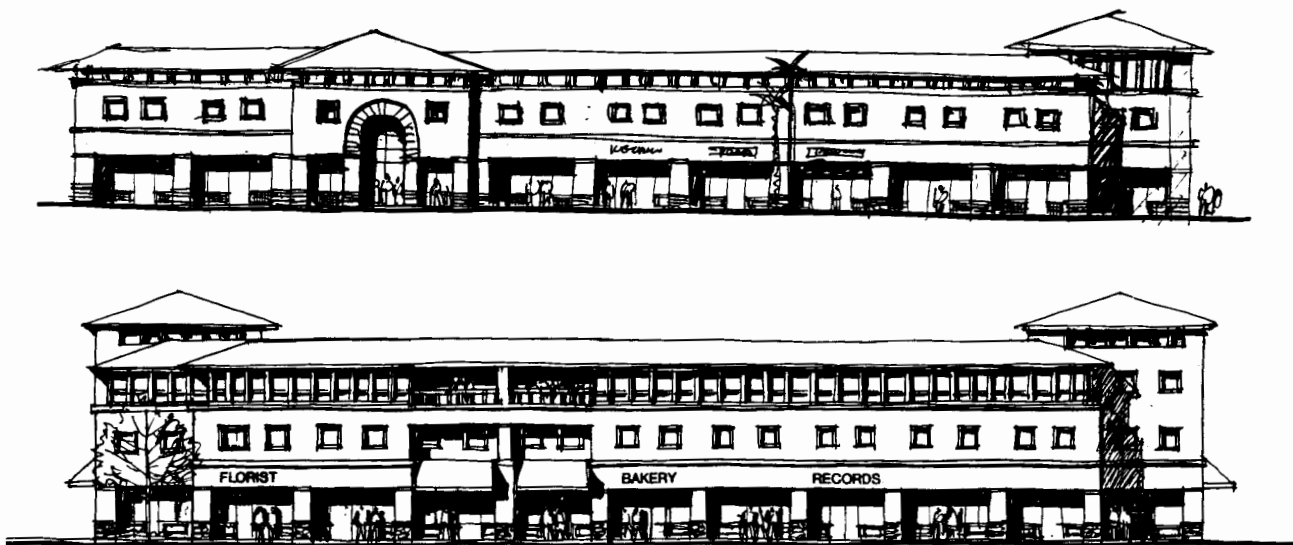
Do not use skylights on sloping roofs visible from the street. Use of skylights is encouraged to provide upper floor tenants a maximum of natural light.

Trash Enclosures

Trash receptacles shall not be visible from the street and shall be located in enclosure or integrated into building. If enclosure is visible from above, provide screening over the top that is architecturally compatible with the building.

Mailboxes

First floor commercial tenants shall provide for suite-to-suite delivery to comply with US Postal regulations. Upper floor residential tenants shall have ganged mailboxes in the building lobby. Upper floor office tenants shall have a mailroom at the first level of the building.



6.3.3 Central District Non-Residential

The location of the elementary school land use in this district is due to its central location within the community, its proximity to the adjacent active park, and its locations adjacent the Riparian habitation area, which may be utilized for educational purposes. The School is also serviced by a community-wide trail system for pedestrians, with grade separation crossing the Main Street for safety.

The architectural character of the school should reflect and exemplify the notions of the New Heritage character. The following design guidelines should be considered in the development of the school.

The Central District reflects to the American suburban architecture which re-interpreted European influences, such as Italian Renaissance, Georgian, and Beaux Arts, eventually developed into more purely American styles such as Prairie style. Elements found in these examples are the basis of the architectural vocabulary for this portion of Porta Bella.

Building Mass and Scale

The plan may be grouped or campus-like to fit the site. The elevations should have low horizontal emphasis, with decorative vertical elements signifying the schools importance.

Balconies

Recessed or slightly projecting balconies at second floors are permitted, if applicable.

Doors

Recess frontage from wall plane to provide shadow lines. Use materials which will add textures and accent to the building. The entry element may project into or span the sidewalk to engage the pedestrian.

Materials

The building materials should project the image of substance and durability. Materials which are rich texturally should be used where the pedestrian is in closest contact with the building. The use of local materials is encouraged, such as Bouquet Canyon Stone. Appropriate Materials include but are not limited to:

- Stone/stone veneer
- Masonry - split-faced, polished
- Cement plaster
- Tile - glazed, quarry
- Concrete/pre-cast concrete
- Wood for trellises, accents
- Wood doors/storefronts/windows
- Asphalt roof shingles
- Flat concrete roof tile
- Copper roofs
- Metal roofs
- Fabric awnings - canvas, synthetic

Colors

This guideline will not attempt to establish a specific color palette but rather determine the range of colors which are appropriate. Using building products made from local materials will establish a color range of their own. Muted colors (i.e. the darker end of the value scale) are appropriate - rust, ochre, forest green, grey, etc.

Pale colors are appropriate - tan beige, off-white. Colors at the brighter end of the value scale are not appropriate except as accents in limited areas i.e. bright greens, yellows, purples, etc. Use of changes in materials as accents in lieu of painted accents is encouraged.

Building Lighting

Building facades should be lit to enhance activity, aesthetics and security. Buildings should not rely entirely on spill-over from street lights for illumination. In general, the building lighting should be designed so that the light source is not visible or the exposed fixture is an integral part of the design. Building lighting should be directed to avoid glare or blinding of pedestrians or to nearby residences. The lighting of upper floors and architectural features is encouraged.

Loading

Provide accessibility for loading and deliveries from interior portion of site where possible.

Mechanical Equipment

Screen all equipment from visibility from street and adjacent uses. Equipment rooms are highly encouraged. No exposed roof mounted equipment should be allowed.

Gutters/Downspouts

Provide gutters at areas of pedestrian access such as building entrances. Downspouts and gutters should be hidden unless they are an integral part of the architectural design.

Skylights

Do not use skylights on sloping roofs visible from the street. Use of skylights is encouraged to provide a maximum of natural light.

Trash Enclosures

Trash receptacles should not be visible from the street and shall be located in enclosure or integrated into building. If enclosure is visible from above, provide screening over the top that is architecturally compatible with the building.

6.3.5 South District Commercial & Institutional

The Neighborhood Commercial use within the South District is intended service the surrounding community, and cross-town motorists traveling on Via Princessa or Santa Clarita Parkway. Uses within this area should generally provide consumer necessities, and should not compete with the retail, specialty retail and entertainment oriented Town Center Mixed-Use area.

The institutional use located along Santa Clarita Parkway is intended to be of public benefit, whether it be a house of worship, fire station, day care center, parochial school or similar. Its architecture should reflect the visually prominent location it occupies by being an exemplary model of the New Heritage character.

Historical Reference

The South District reflects to the American suburban architecture which re-interpreted European influences, such as Italian Renaissance, Georgian, and Beaux Arts, eventually developed into more purely American styles such as Prairie style. Elements found in these examples are the basis of the architectural vocabulary for this portion of the Town Center.

Building Mass and Scale

The plan may be linear or massed based on the individual site. The elevations may have stronger horizontal emphasis. Structures in this area will be primarily single storey.

Colonnades

Recessing store fronts from the building plane to create a protected walkway is acceptable.

Awnings/Canopies/Trellises

These devices are an integral part of this architectural vocabulary. These elements should be carefully integrated into the building design to avoid the appearance of being tacked-on or afterthoughts. These elements may occasionally span the width of the sidewalk at areas such as entrances to the buildings. Wood trellises should be in the “craftsman” tradition (i.e. Greene & Greene). Rough sawn, re-sawn, or heavy timber will not be allowed.

Signage

See signage Section 8.0 for General Sign Guidelines. Signage for tenants must be integrated into the basic building design. Ignoring this criteria will result in signage that appears tacked-on and detrimental to the quality of design. Signage should be viewed as a design opportunity as it is an integral part of the image of this street.

Roofs

Hipped and gable roofs with a flat pitch (30° max) will be the principal roof forms. Flat roofs may be introduced to accent a specific architectural elements.

Windows

Use casement, single or double hung window individually or grouped. Use lintels and sills as architectural accents. Recess windows from exterior wall plane to create shadow lines. Window frames and mullions should have a deep profile i.e. wood windows or clad widows. Residential grade aluminum windows will not be allowed.

Storefronts/Doors

Create a rhythm with the frontages which will allow for flexibility in lease break-ups. Recess frontage from wall plane to provide shadow lines. Use storefront material with a deep profile such as milled wood. Use materials which will add textures and accent to the building at non-glazed storefront bulkheads. The building entries to access upper floors should be easily identifiable and should not be confused with street level entries. The entry element may project into or span the sidewalk to engage the pedestrian.

Materials

The building materials should project the image of substance and durability. Materials which are rich texturally should be used at the street level where the pedestrian is in closest contact with the building. The use of local materials is encouraged, such as Bouquet Canyon Stone. Appropriate Materials include but are not limited to:

- Stone/stone veneer
- Masonry - split-faced, polished
- Cement plaster
- Tile - glazed, quarry
- Concrete/pre-cast concrete
- Wood for trellises, accents
- Wood doors/storefronts/windows
- Asphalt roof shingles
- Flat concrete roof tile
- Copper roofs
- Metal roofs
- Fabric awnings - canvas, synthetic

Colors

This guideline will not attempt to establish a specific color palette but rather determine the range of colors which are appropriate. Using building products made from local materials will establish a color range of their own. Muted colors (i.e. the darker end of the value scale) are appropriate - rust, ochre, forest green, grey, etc. Pale colors are appropriate - tan beige, off-white. Colors at the brighter end of the value scale are not appropriate except as accents in limited areas i.e. bright greens, yellows, purples, etc. Use of changes in materials as accents in lieu of painted accents is encouraged.

Building Lighting

Building facades should be lit to enhance activity, aesthetics and security. Buildings should not rely entirely on spill-over from street lights or from frontages for illumination. In general, the building lighting should be designed so that the light source is not visible or the exposed fixture is an integral part of the design. Building lighting should be directed to avoid glare or blinding of pedestrians adjacent residential areas.

Loading

Provide accessibility for loading and deliveries from interior portion of site where possible. Limit amount of loading for the maneuvering lanes.

Mechanical Equipment

Screen all equipment from visibility from street and adjacent buildings. Equipment rooms are highly encouraged.

Antennas and Satellite Dishes

Screen dish antennas from visibility from streets, and adjacent buildings.

Gutters/Downspouts

Provide gutters at areas of pedestrian access such as building entrances. Downspouts and gutters shall be hidden unless they are an integral part of the architectural design.

Skylights

Do not use skylights on sloping roofs visible from the street. Use of skylights is encouraged to provide tenants a maximum of natural light.

Trash Enclosures

Trash receptacles shall not be visible from the public view, and shall be located in enclosure or integrated into building. If enclosure is visible from above, provide screening over the top that is architecturally compatible with the building.

Mailboxes

Commercial tenants shall provide for suite-to-suite delivery to comply with US Postal regulations. Tenants may have ganged mailboxes.

6.3.6 General Commercial Standards

The following general provisions are intended to apply to all commercial districts within Porta Bella. However, these standards shall take precedence only when a specific area regulation does not specify greater or more restrictive criteria.

Temporary Uses

The following temporary uses may be permitted upon review and conditional approval by the Director of Community Development:

- (1) Temporary on-site construction offices/facilities
- (2) On-site leasing and sales offices
- (3) On-site real estate signs and future development signs in conformance with Section 8.0, Sign Regulations.

The above uses shall be approved for a specified period of time, not to exceed one year intervals, and a performance bond ay be required to remove any structures at the end of the approved period of use.

Operation in General

All commercial uses which are required to be operated in a completely enclosed building shall be operated in such a way as to produce no objectionable noise or odors outside its own building's walls.

Landscaping

Commercial development shall have all required landscaping installed at the time of occupancy, and said landscaping shall be provided with irrigation systems which comply with standards described in Section 5.0, Landscape regulations.

All landscaping and irrigation systems shall be maintained in good condition by the property owner unless other provisions have been made. All developments shall be responsible for landscaping and maintenance of adjacent parkways, unless other provisions have been made.

Fences and Walls

Where required, an opaque fence or wall shall be a minimum of six feet as measured from the highest grade elevation on either side of the fence or wall. The following exceptions shall apply:

- (1) In required front setback areas, the maximum height of a fence or wall shall be forty-two inches.
- (2) Along property lines to non-service or storage areas, buffer fences and walls should be used sparingly, leaving comfortable openings to enhance pedestrian mobility.

Nothing within this Section is intended to minimize the Planning Commission from requiring the design of a specific fence or wall to accommodate a particular situation or condition.

Self-service Devices

Self-serve devices, including, but not limited to, newspaper racks and ice machines, may be permitted subject to approval by the Director of Community Development prior to installation. In approving the location of said devices, the Director shall ensure that the device:

- (1) Complies with all other City Codes and Ordinances
- (2) Will not interfere with pedestrian and vehicular circulation patterns
- (3) Will not encroach into the public right-of-way
- (4) Will not encroach into required parking, setback, or landscape areas.

Circulation Patterns

Circulation patterns within commercial and industrial districts shall be designed to minimize the number of curb cuts required. Curb cuts should be located as far as possible from intersections and whenever possible be aligned with others on the opposite side of the street.

Recycling Centers

Recycling drop-off containers and services shall be conditionally permitted as an accessory use in all non-residential land use areas. Hazardous materials drop-off and recycling centers for household items such as paints, oils, insecticides/herbicides, and similar items are also conditionally permitted accessory uses in these areas. The incorporation of these facilities within the community is encouraged, and review should be limited to the function of the center in its proposed location.

Gas Stations

The provisions of the Section shall apply to all new service stations and other places where automotive fuels are dispensed.

(1) Site

All new sites for Gas Stations shall have a minimum net lot area of 15,000 square feet and a frontage of 125 feet on any street having a driveway. No driveways may be located closer than 75 feet to any intersection.

(2) Pumps

All gasoline pumps and pump islands upon which they are placed shall be setback a minimum of thirty feet from any property line

(3) Canopies

Canopies shall setback a minimum of twenty feet from any property line.

(4) Activities

The following activities may be permitted: Dispensing of gasoline, diesel fuels, oil, grease, tires, batteries and replacement parts and installation of the items enumerated.

Heavy engine or transmission repair or painting shall not be permitted in a service station. Convenience stores, mini-marts, car washes whether automatic or manual shall be permitted in conjunction with a services station subject to approval of a Conditional Use Permit. All such uses shall be conducted within a completely enclosed building.

(5) Repair and Servicing

All repair equipment including hydraulic hoists, portable jacks, pits, alignment equipment, and tire equipment and all servicing other than dispensing of fuel and oil shall be entirely enclosed within a building. Bay doors for service areas shall be located away from view from public areas.

(6) Parking

Vehicles shall not be parked on the premises other than in designated parking spaces. No overnight parking shall be permitted except for vehicles under repair when maintained within a fully enclosed building. When a mini-mart or convenience store is operated in conjunction with the gasoline station, a minimum of five parking spaces shall be provided and conveniently located to serve store customers. All car wash installations shall provide adequate stacking for a minimum of four vehicles per lane at the entry of the facility. Parking areas for air and water service, drying and vacuuming shall be clearly provided.

(7) Walls

A decorative masonry wall six feet in height shall be constructed and maintained along all side and rear interior property lines abutting residential property. Where such walls abut or are adjacent to commercial/office uses, they shall be five feet in height. A minimum five foot landscape planter shall be provided adjacent to the wall. Walls may be reduced in height or waived where the gasoline station and abutting commercial or industrial uses share a common driveway, or where it is determined unnecessary by the Director of Community Development.

(8) Signs

All price signs shall be limited to monument or wall style and shall satisfy the minimum size and number required by State law. All such signs shall be incorporated as an integral part of the business' permanent signs in such a manner as not to detract from the appearance of the primary sign. Price signs shall not be affixed to light standards or other non-sign structures. Advertising displays and devices other than approved signs shall be prohibited.

(9) Towing Operations

Towing operations, clearly incidental to, and in conjunction with a permitted Gas Station may be permitted provided trucks are parked within approved parking spaces. Such spaces shall be located to the rear of the property, shall be screened from public view and shall be clearly identified on submitted development plans. No on-site storage of towed vehicles, other than those towed for minor repair may occur.

(10) Design

Except as otherwise provided in this Section, Gas Stations shall comply with the design concepts and standards in this Chapter and within the individual district Chapters as required for all their commercial businesses. A preferred site design for gas stations on corner lots is to locate the structure diagonally near the corner, with fuel pumps located to the interior of the project. This design forces entrances away from the corner intersection, and creates a stronger streetscape with the building as an anchor.

(11) Restrooms

Men's and women's restrooms shall be provided and made available to customers. Separate facilities shall be provided for each sex and maintained open to the public during business hours and kept in sanitary and working condition.

(12) Handicap Accessibility

All buildings, services locations and restrooms shall be handicap accessible and usable as established by State and locally adopted handicap standards.

(13) Loading Facilities

Loading facilities to serve convenience marts and fuel tanks shall be located such that they do not block or restrict circulation drive-ways on-site.

(14) Sale of Alcoholic Beverages

Establishments engaged in the concurrent sale of motor vehicle fuel with alcoholic beverages shall abide by the following requirements:

- (a) No advertisement of alcoholic beverages shall be displayed at motor fuel islands and no self-illuminated advertising for beer or wine shall be located on buildings or windows.
- (b) No sale of alcoholic beverages shall be made from a drive-in window.
- (c) No display or sale of beer or wine shall be made from an ice tub or similar.
- (d) Automobile Storage - Automobiles parked in any required parking space shall be drivable, clearly operable in general and have a current licence and registration.

6.5 Residential Design Guidelines

The residential architectural character of the community may also be defined as “New Heritage.” This character takes the best elements from contemporary and traditional sources, and blends them in a distinctive manner. This character is inspired by a number of distinct architectural styles such as Monterey, Mission, Italianate and traditional American architecture, including Prairie, Craftsman and California Bungalow. Many of these styles share a common relationship in that most were popular in the early days of California.

Within Porta Bella, the selection of one consistent design style is not as important as is the selection of a design palette that will result in a distinct and unique character. Good examples of cities which have elements of this eclectic character are Pasadena, Redlands, Carmel, San Juan Capistrano, Monterey and Santa Barbara. Within these cities are varying degrees of the above mentioned architecture.

Building Mass & Scale

Structures within the community should be developed with spatial relationships in mind. Several points should be considered by the developer prior to submittal. They are:

- The proposed development will have an aesthetic relationship to the street level, especially the pedestrian and automobile perspective. Appropriate setbacks and use relationships (i.e. front porch vs. garage door) should especially be considered.
- The proposed development will have an aesthetic relationship to the surrounding properties.
- Obstructions to views will be minimized.

Roof Pitches & Materials

Roof pitches may be varied within the community according to the architectural style chosen. Due to the inherent fire danger of the area, special care shall be taken in selecting roof materials. Roofing materials shall be fire resistant pursuant the criteria of the Building Department.

Except on flat surface roofs, tar paper roofing shall not be allowed. Asphalt shingle roofing shall only be allowed if it can be demonstrated that the particular brand to be utilized can be installed with a three dimensional quality, such as simulated wood-shake or thick-border, random tab varieties.



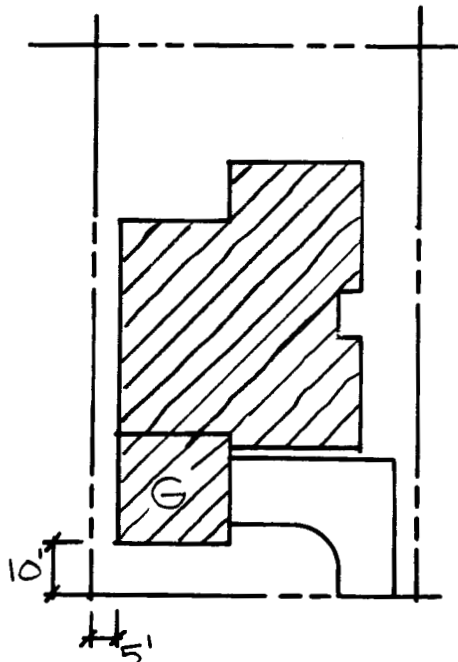
Windows & Doors

Particular attention must be given to the shading of window with a western exposure. Interior and exterior shading devices are encouraged. Passive energy design is recommended

Silver and gold window or door frames are prohibited, as is reflective glass. Aluminum frame window shall be treated with matching or complimentary building trim color.



Front doors are very important in setting the tone of an architectural style. Care should be taken in the selection of front doors that they reflect the architectural style of the home, and are solid and decorative in appearance.



Garage Doors and Driveways

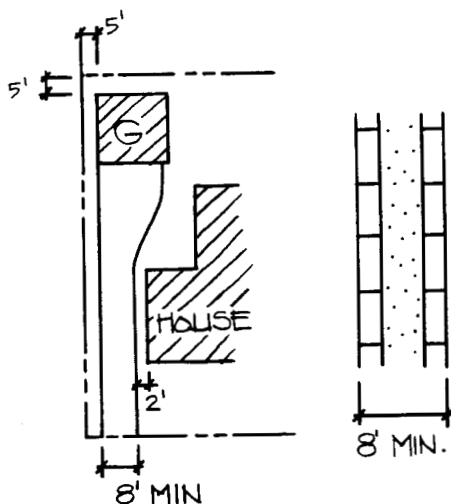
Often in neighborhood design, the garage door is perhaps the single most visible architectural element on a home, as it is usually located facing the street. Within this community, setbacks have been established which encourage the home to project in front of the garage door, therefore reducing the door's dominance of the streetscene.

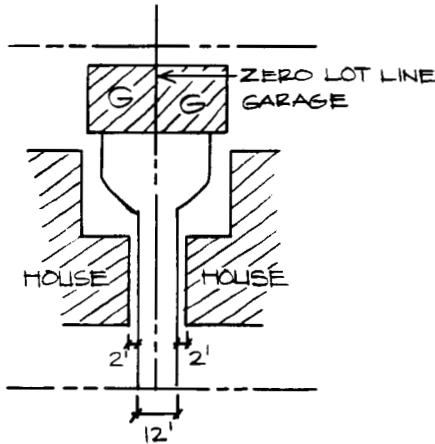
Garages to the rear of the structure are also highly encouraged within Porta Bella by utilization of alleys in the Paired Single Family areas, and the allowance of single-lane sideyard driveways in all other single family areas with lots above 4,000 square feet. This design offers a unique design opportunity by creating a "monitorable" play area to the rear, rather than the street front, of the home. Shared zero lot-line driveways are permitted to access rear yard garages.

Driveways within this community are also encouraged to be designed in the traditional concrete "strip" design. This design lessens the expanse of concrete in front of the unit, which acts to:

- Create a "softer" streetscape
- Create a more traditional character
- Conserve water by lessening the non-permeable surface area
- Conserve water by allowing it to soak into the ground, instead of running off into the street when car washing, etc.
- Lessen the amount of heat generation caused by urban surfaces.

Side yard driveways, which are intended to service rear yard detached garages, should be single lane in width, especially at the curb. This driveway may expand to two lanes behind the leading edge of the wall of which it is adjacent.



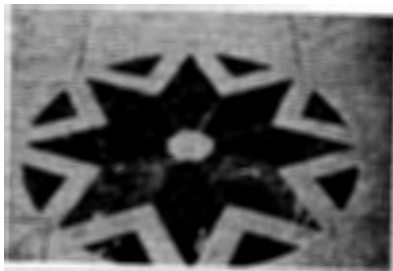


Exterior Stairs

Exterior stairs should architecturally compliment and enhance the structure it serves.

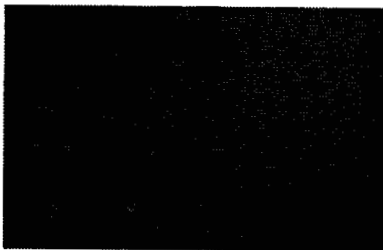
Project Fences, Hedges and Walls

Private walls and fences may be constructed to provide security, privacy and landscape definition. Visual barriers shall be limited to a maximum height of six feet. Height of visual barriers within residential front setback areas shall not exceed forty-two inches, except that visual barriers may exceed forty two inches if they are set back from the front property line a distance equal to the dwelling, garage, or carport setback, but not less than ten feet. These restrictions are not applicable regarding the planting of trees in parkway, median, slope or front yard areas.



Project fences, hedges and walls shall not interfere with driver sight distances or angles, and may be staggered due to terrain.

Walls and fences should compliment the architectural style of which it is related. No chain link fencing shall be permitted in the front yard areas beyond the adjacent wall plane of the house.



Paving

Textures, patterns and colors are encouraged in the design of paved areas, especially in public areas of Multi-family areas such as motor courtyards. Large monotonous areas of single-color non-textured paving are discouraged.

Exterior Colors

This guideline will not attempt to establish a specific color palette but rather determine the range of colors which are appropriate. Using building products made from local materials will establish a color range of their own. Muted colors (i.e. darker end of the value scale) are appropriate - rust, ochre, forest green, grey, etc. Pale colors are appropriate - tan, beige, off-white. Colors at the brighter end of the value scale are not appropriate. Use of changes in materials as accents in lieu of painted accents is encouraged.

Mechanical Equipment

All air conditioning/heating equipment, soft water tanks, gas meters, and electric meters must be screened from public view. Sound attenuation is encouraged.

Antennas & Satellite Dishes

All antennas are restricted to the attic or interior of the residences. All satellite dishes shall be located on the ground level of the rear yard, and shall not be visible to public view.

Flashing Sheet Metal, Vents

All flashing, sheet metal, vent stacks and pipes shall be painted to match the adjacent building surface.

Skylights

Skylights are encouraged to provide natural lighting within a home. Skylights should be designed as an integral part of the roof. Their form, location, and color should relate to the building.

Solar panels

Solar panels and their accessories should be integrated into the roof design, flush with the roof slope. Frames and accessories must be colored to compliment the roof. Natural aluminum frames are prohibited.

Trash Enclosures

Trash bins shall be fully enclosed within a six foot structure with non-gated openings for human access. The structure shall architecturally compliment the structure which it accommodates, and shall be softened with landscaping.

Mailboxes

Where common mailbox services are provided, their structure's architectural style must match that of the surrounding structures. Common mailbox areas must be safe, and adequately illuminated at night. Mailbox locations must be approved by the U.S. Postal Service.

Trellises

Open trellis and beam construction shall be permitted to attach the garage or carport to the dwelling where detached and may also extend from the dwelling to within 5' of the property line in the side, rear yards or front yards, and will not be included in the calculation of the building coverage. Trellises shall not exceed 50% of the exterior space.

Water Preservation

Within the Specific Plan area, all residential developments, including hotels/motels, shall utilize water saving fixtures including, but not limited to:

- Low-flow showerheads with a tested rate of 3 gpm or less. Showers installations must incorporate a convenient “stop water” mechanism to stop water flow mid-shower.
- Low flush volume toilets with a tested rate of 3 gallons or less per flush.
- Encourage centrally located water heater near the water fixtures to minimize the wasteful “heat-up” time for water.

Temporary Uses Permitted

- (1) Model homes, temporary construction offices, temporary real estate offices and signs;
- (2) Continued use of an existing building during construction of a new building on the same building lot.

Projections Into Yards

- (1) Roof projections may extend into a required side yard a maximum of fifty percent of the yard's width, not to exceed two feet, and may extend into a required front or rear yard a maximum of four feet;
- (2) Greenhouse and bay windows, planting boxes, and fire place structures may project a maximum of two feet into required yard setbacks.
- (3) Staircases, balconies, covered porches and other similar structural features may not project more than fifty percent into any required front, rear, or side yard setback, but in no case shall distance exceed four feet except a covered front porch may extend up to eight feet.

Rear Yard Setbacks

Rear Yard Setbacks as established within the individual land use standards, shall be relatively flat. Slopes shall not be included within the calculation of this setback.

6.5 Land Use Standards

The following paragraphs detail the permitted and conditional land uses and development standards within the individual areas. If a use is not listed as permitted or conditional, it shall be considered to be prohibited. At the discretion of the Community Development Director, uses which are prohibited may be reviewed by the Planning Commission for a Conditional Use status.

6.5.1 Town Center Mixed Use

The Town Center Mixed Use Area is intended to blend uses such as entertainment, retail, service, office, cultural, institutional/religious and residential uses.

(A) Permitted Uses

The following uses shall be permitted in the Town Center Mixed Use areas. No single use may occupy a structure with a footprint of greater than 30,000 square feet without a conditional use permit. A condition of the conditional use permit shall be that the architectural style of the use appears to be series of individual uses, rather than one dominant use. Uses which are not listed as Permitted or Conditional shall be considered Prohibited. Prohibited uses may be reviewed by the Planning Commission for Conditional Use status if so determined by the Community Development Director.

1. Retail Uses

- a. Apparel stores
- b. Appliance/electronics stores
- c. Arts and craft galleries or studios
- d. Bicycle sales and rental shops
- e. Food stores, including markets, bakeries, and health food establishments, candy stores.
- f. Florists
- g. General merchandise stores
- h. Hardware stores
- i. Health and exercise clubs
- j. Hobby supply stores
- k. Jewelry stores
- l. Media shops; including bookstores, newsstands, and video tape/laser disc outlets.
- m. Music stores; including sales of instruments, records, tapes

and discs

- n. Parking structure
- o. Pet shops; retail sales and grooming only. No boarding of animals
- p. Restaurants and eating places, but excluding drive-ins and drive throughs. Outdoor eating areas, including sidewalk seating which does not interfere with the pedestrian circulation is permitted. The outdoor serving of alcoholic beverages is permitted in private, clearly defined areas.
- q. Schools and for dance and music
- r. Skating rinks, dance clubs and similar
- s. Specialty food & tobacco items, such as ice cream, wine, candy and tobacco stores.
- t. Sporting goods stores
- u. Theaters, Cinemas and Comedy clubs. Theaters, Peep-shows, Stage shows, paraphernalia shops and similar which specialize in adult oriented materials are prohibited within this area.
- v. Toy shops
- w. Other uses consistent with the intent, scale and character of this area.

(2) Office and Service Uses.

- a. Accountants, advertising agencies, appraisers, attorneys, business and management consultants, economists, public relations consultants, and other professional offices.
- b. Administrative or executive offices of any type of business.
- c. Architects, landscape architects, planners, engineers and surveyors, geologists, industrial designers, graphic designers, and interior designers.
- d. Barber shops and beauty salons
- e. Employment agencies, travel agencies, and airline ticket agencies.

- f. Financial institutions, including banks, savings and loan associations, finance companies and credit unions. This does not including institutions primarily involved in check cashing.
- g. Government offices and service facilities; excluding storage of equipment, repair or warehouses.
- h. Insurance brokers and services, investment brokers, real estate brokers and offices, and title and escrow companies.
- i. Medical offices, including chiropractors, clinics, dentists, general practitioners, orthodontists, and similar.
- j. Oculists, opticians, and optometrists
- k. Office supply and stationery stores
- l. Personal service establishments; including dry cleaning and tailors.
- m. Photography studio
- n. Prescription pharmacies
- o. Quick copy and printing establishments
- p. Schools and studios for arts, crafts, photography, music, dance and similar humanities.
- q. Small appliance repair, watch and jewelry repair, shoe repair and similar.
- r. Other uses consistent with the intent of this area.

(3) *Residential Uses*

- a. Multi-family residence when located above and/or behind first floor Retail and/or Service-Office uses.
- b. Convalescent, retirement, rest homes, sanitariums or similar congregate care facilities when located above and/or behind first floor retail and/or service-office uses.

B. Uses Subject to a Conditional Use Permit

Certain uses, while similar to the Permitted uses, may adversely affect surround properties and require additional consideration. Such uses shall require a conditional Use Permit. These uses include the following:

1. Bars and Cocktail lounges not in conjunction with a restaurant.
2. House of worship
3. Day Care for children and adults
4. Gas Stations, subject to the provisions of this sections regarding Gas Stations
5. Hotels & Motels
6. Outdoor sales of agricultural products
7. Private clubs and lodges
8. Push cart vendors such as flower carts, shoeshine, and properly licensed food vendors.
9. Recycling centers
10. Structures exceeding the maximum height or square footage permitted by this Section.

C. Site Development Standards

(1) Parcel Size

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(2) Setbacks

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

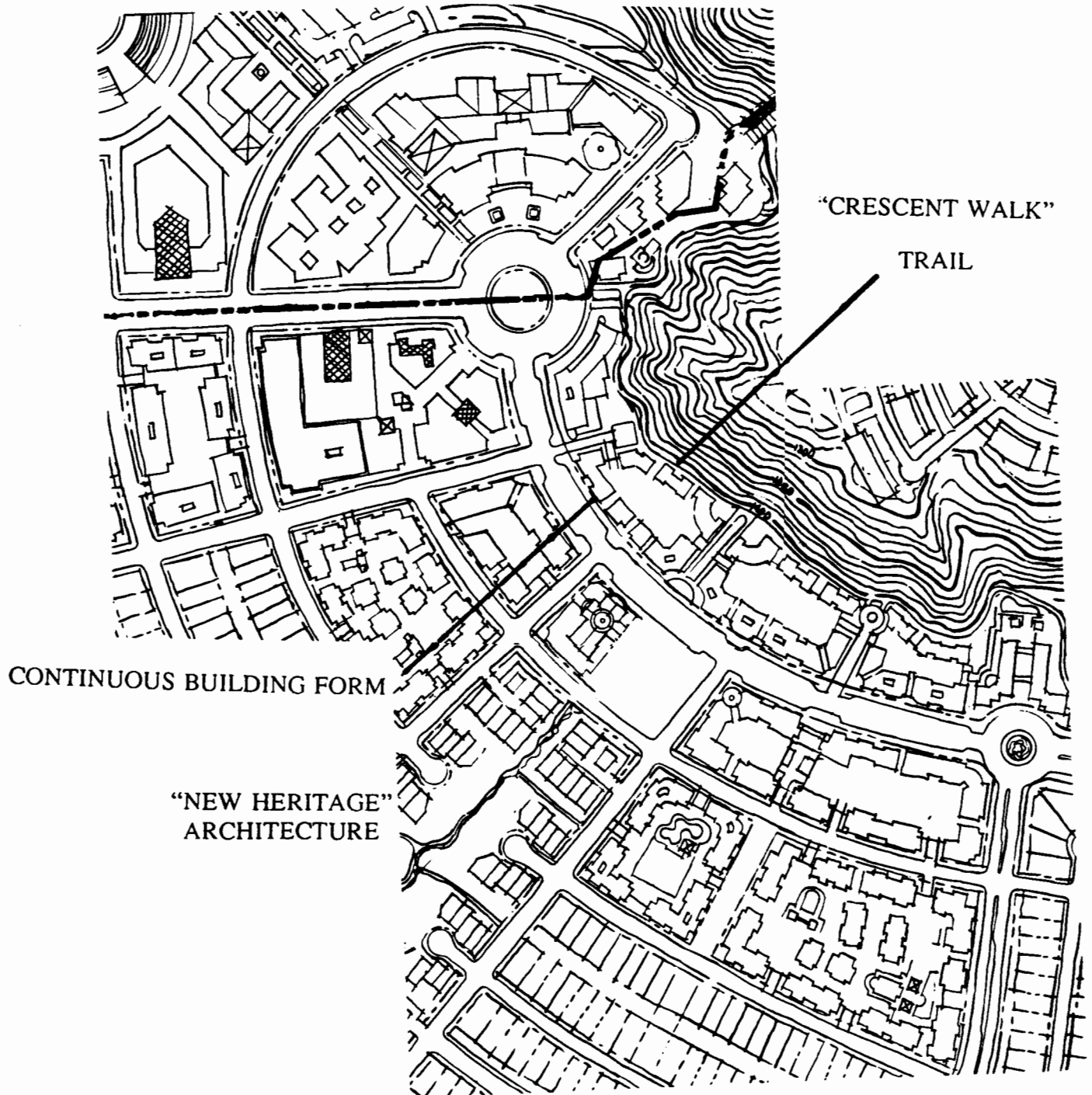
(3) Frontage

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(4) Building Height

The maximum building height shall be forty feet, except for architectural features such as towers, cupolas, and other elements that contribute to the overall character of the community, and have a footprint are of less than 300 square feet.

STRUCTURED OR INTERIOR LOT PARKING
ZERO FOOT SETBACK FROM PROPERTY LINE



6.5.2 Soledad Mixed-Use (SC)

The maximum permitted footprint of a structure within this zone shall be 50,000 feet without a conditional use permit. A criteria in granting this conditional use permit shall be that the proposed structure does not overpower nor detract from the Santa Clarita Central Rail Station in prominence, character, scale, or massing.

(A) Permitted Uses

The following uses shall be permitted in the Rail Station Mixed Use area. Uses which are not listed as Permitted or Conditional shall be considered Prohibited. Prohibited uses may be reviewed by the Planning Commission for Conditional Use status if so determined by the Community Development Director.

1. Retail Uses

- a. Apparel stores
- b. Appliance/electronics stores
- c. Bicycle sales and rental shops
- d. Fast food establishments
- e. Food stores, including markets, bakeries, and health food establishments, candy stores.
- f. Florists
- g. Furniture stores
- h. General merchandise stores
- i. Hardware stores
- j. Health and exercise clubs
- k. Hobby supply stores
- l. Home improvement stores
- m. Media shops; including bookstores, newsstands, and video tape/laser disc outlets.
- n. Music stores; including sales of instruments, records, tapes

and discs.

- o. Outdoor sale of agricultural products
- p. Plant nurseries & sales
- q. Restaurants and eating places, but excluding drive-ins and drive throughs. Outdoor eating areas, including sidewalk seating which does not interfere with the pedestrian circulation is permitted. The outdoor serving of alcoholic beverages is permitted in private, clearly defined areas.
- r. Specialty food items, such as candy and tobacco stores.
- s. Sporting goods stores.
- t. Other uses consistent with the intent, scale and character of this area.

2. *Office and Service Uses.*

- a. Accountants, advertising agencies, appraisers, attorneys, business and management consultants, economists, public relations consultants, and other professional offices.
- b. Administrative or executive offices of any type of business.
- c. Architects, landscape architects, planners, engineers and surveyors, geologists, industrial designers, graphic designers, and interior designers.
- d. Automobile repair shops, within completely enclosed structures. Garage bays must be oriented away from public view.
- e. Automobile service shops which specialize in quick turn around repairs, such as oil and lubrication, muffler, brake, tire, and similar shops.
- f. Automobile parts stores
- g. Barber shops and beauty salons
- h. Employment agencies, travel agencies, and airline ticket agencies.

- i. Financial institutions, including banks, savings and loan associations, finance companies and credit unions. This does not including institutions primarily involved in check cashing.
- j. Insurance brokers and services, investment brokers, real estate brokers and offices, and title and escrow companies.
- k. Office supply and stationery stores
- l. Personal service establishments; including dry cleaning and tailors.
- m. Prescription pharmacies
- n. Quick copy and printing establishments
- o. Small appliance repair, watch and jewelry repair, shoe repair and similar.
- p. Transportaton station, multi-modal transportation facility
- q. Other uses consistent with the intent of this area.

B. Uses Subject to a Conditional Use Permit

Certain uses, while similar to the Permitted uses, may adversely affect surround properties and require additional consideration. Such uses shall require a conditional Use Permit. These uses include the following:

1. Automobile rental agencies
2. Gas Stations, subject to the provisions of this sections regarding Gas Stations
3. Child day-care centers
4. Commercial bus carrier station
5. Outdoor sale of agricultural products
6. Recycling centers
7. Structures exceeding the maximum height or square footage permitted by this Section.

C. Site Development Standards

(1) Parcel Size

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(2) Setbacks

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(3) Frontage

No minimums, subject to Design Review as Specified in Section 11.0, Design Review.

(4) Building Height

The maximum building height shall be thirty five feet, except for architectural features such as towers, cupolas, and other elements that contribute to the overall character of the community, and have a footprint are of less than 300 square feet. The maximum height for the train station shall be 45 feet.

(5) Landscape Improvements

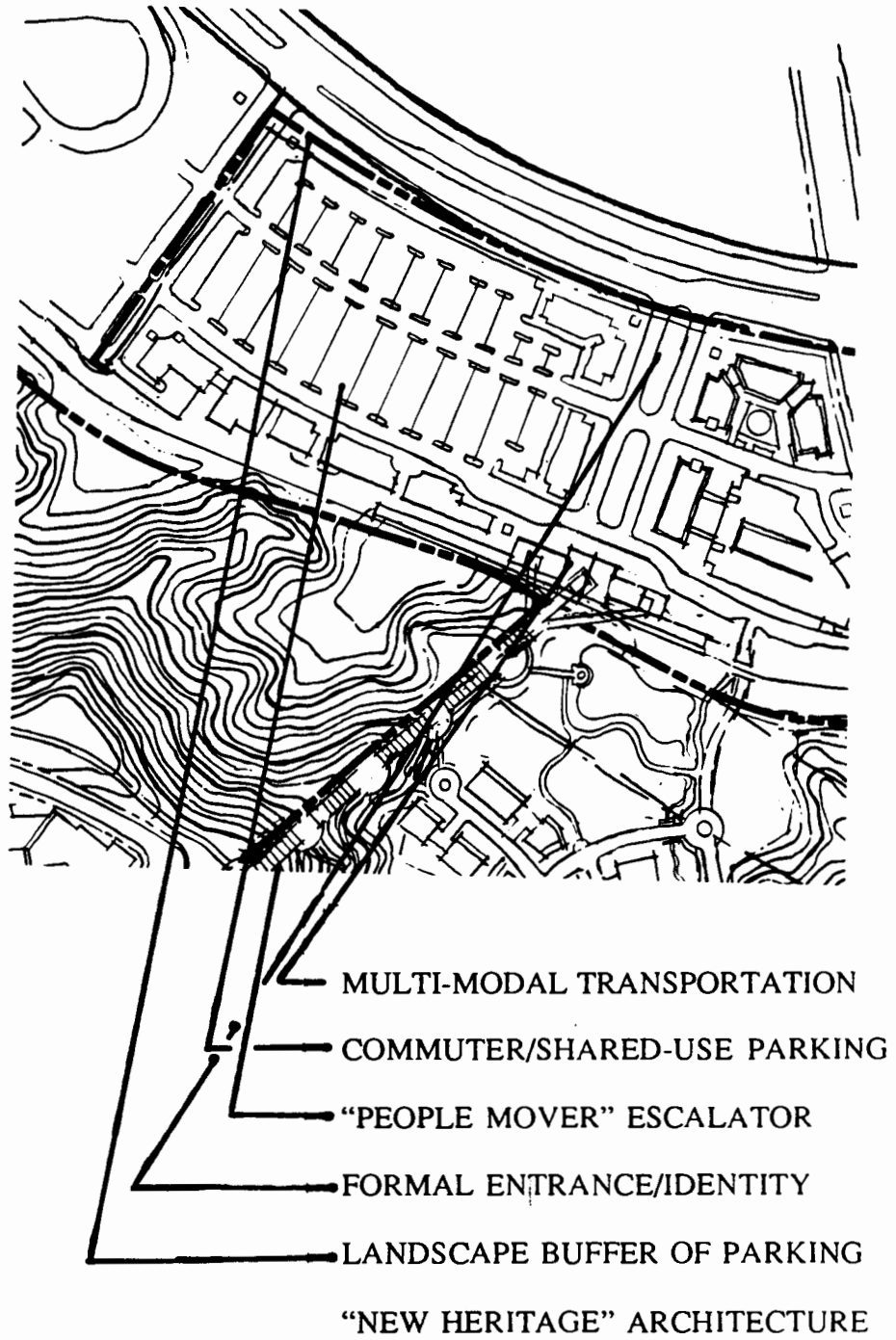
The provisions of Section 5.0, Landscape Regulations, shall be used to determine the landscape requirements for development in the Rail Station Mixed Use Area.

(6) Parking

The provisions of Section 7.0, Parking Regulations, shall be used to determine the parking for development in the Soledad Mixed Use area. Parking within this area and with convenient access to the escalator system may be utilized for shared parking uses in conjunction with the Town Center District.

(7) Signage

The provisions of Section 8.0, Signage Regulations, shall be used to determine the signage for development in the Soledad District Mixed-Use.



6.5.3 Neighborhood Commercial

The maximum footprint of a structure in this area shall be 60,000 square feet, unless a conditional use permit is granted which allows a greater footprint.

(A) Permitted Uses

The following uses shall be permitted in the Commercial Neighborhood area. Uses which are not listed as Permitted or Conditional shall be considered Prohibited. Prohibited uses may be reviewed by the Planning Commission for Conditional Use status if so determined by the Community Development Director.

1. Retail Uses

- a. Fast food establishments
- b. Food stores, including markets, bakeries, and health food establishments, candy stores.
- c. Florists
- d. General merchandise stores
- e. Hardware stores
- f. Health and exercise clubs
- g. Hobby supply stores
- h. Media shops; including bookstores, newsstands, and video tape/laser disc outlets.
- i. Music stores; including sales of instruments, records, tapes and discs.
- j. Pharmacies
- k. Restaurants and eating places, but excluding drive-ins and drive throughs. Outdoor eating areas, including sidewalk seating which does not interfere with the pedestrian circulation is permitted. The outdoor serving of alcoholic beverages is permitted in private, clearly defined areas.

1. Other uses consistent with the intent, scale and character of this area.

2. ***Office and Service Oriented Uses***

- a. Automobile parts stores
- b. Barber shops and beauty salons
- c. Employment agencies, travel agencies, and airline ticket agencies.
- d. Financial institutions, including banks, savings and loan associations, finance companies and credit unions. This does not including institutions primarily involved in check cashing.
- e. Personal service establishments; including dry cleaning and tailors.
- f. Quick copy and printing establishments
- g. Small appliance repair, watch and jewelry repair, shoe repair and similar.
- h. Other uses consistent with the intent of this area.

- B. Uses Subject to a Conditional Use Permit***

Certain uses, while similar to the Permitted uses, may adversely affect surrounding properties and require additional consideration. Such uses shall require a conditional Use Permit. These uses include the following:

1. Gas Stations, subject to the provisions of this sections regarding Gas Stations
2. Recycling centers
3. Structures exceeding the maximum height or square footage permitted by this Section.

C. Site Development Standards

(1) Parcel Size

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(2) Setbacks

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(3) Frontage

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(4) Building Height

The maximum building height shall be thirty-five feet, except for architectural features such as towers, cupolas, and other elements that contribute to the overall character of the community, and have a footprint are of less than 300 square feet.

(5) Landscape Improvements

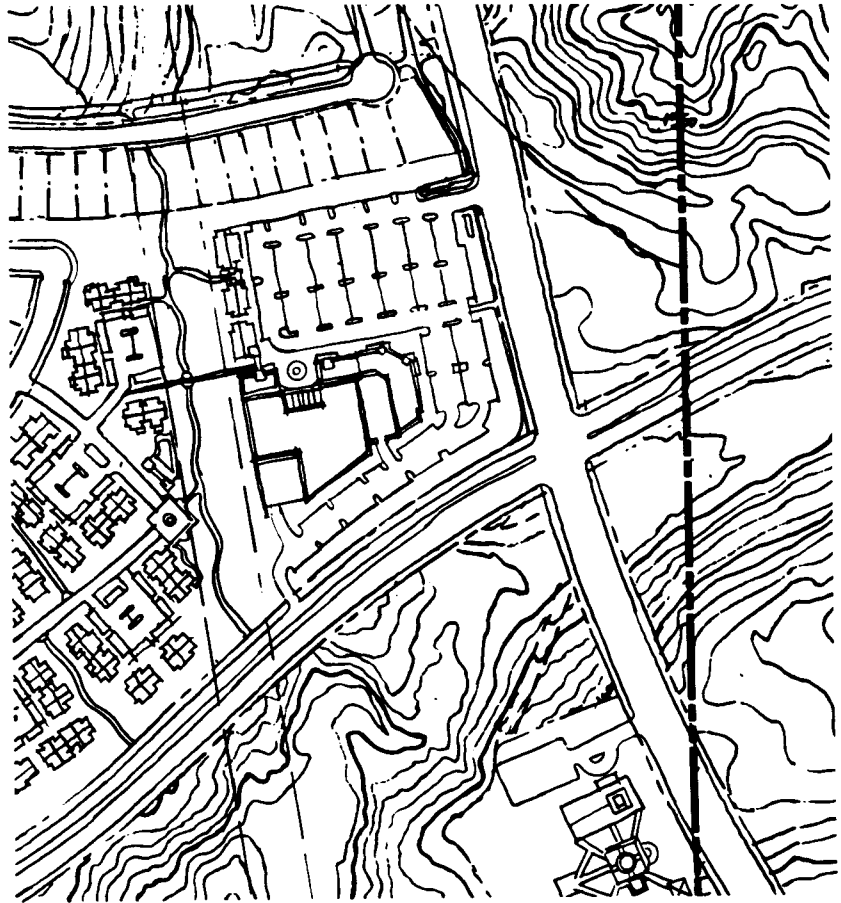
The provisions of Section 5.0, Landscape Regulations, shall be used to determine the landscape requirements for development in the Neighborhood Commercial area.

(6) Parking

The provisions of Section 7.0, Parking Regulations, shall be used to determine the parking for development in the Neighborhood Commercial area.

(7) Signage

The provisions of Section 8.0, Signage Regulations, shall be used to determine the signage for development in the Neighborhood Commercial area.



PEDESTRIAN CONNECTION TO RESIDENTIAL
AESTHETIC RELATION TO SURROUNDING AREA
“NEW HERITAGE” ARCHITECTURE

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6.5.4 Office Park

The Office Park area is intended to provide office facilities within close proximity to the Town Center and Soledad Mixed-Use centers.

(A) Permitted Uses

The following uses shall be permitted in the Office Park area. Uses which are not listed as Permitted or Conditional shall be considered Prohibited. Prohibited uses may be reviewed by the Planning Commission for Conditional Use status if so determined by the Community Development Director.

1. Retail Uses

- a. Media shops; including bookstores and newsstands
- b. Other uses consistent with the intent, scale and character of this area.

2. Office and Service Uses.

- a. Accountants, advertising agencies, appraisers, attorneys, business and management consultants, economists, public relations consultants, and other professional offices.
- b. Administrative or executive offices of any type of business.
- c. Architects, landscape architects, planners, engineers and surveyors, geologists, industrial designers, graphic designers, and interior designers.
- d. Employment agencies, travel agencies, and airline ticket agencies.
- e. Financial institutions, including banks, savings and loan associations, finance companies and credit unions. This does not including institutions primarily involved in check cashing.

- f. Health clubs
- g. Insurance brokers and services, investment brokers, real estate brokers and offices, and title and escrow companies.
- h. Office supply and stationery stores
- i. Parking structure
- j. Quick copy and printing establishments
- k. Other uses consistent with the intent of this area.

B. Uses Subject to a Conditional Use Permit

Certain uses, while similar to the Permitted uses, may adversely affect surrounding properties and require additional consideration. Such uses shall require a conditional Use Permit. These uses include the following:

- 1 Child day-care centers
- 2. Structures exceeding the maximum height permitted by this Section.

C. Site Development Standards

(1) Parcel Size

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(2) Setbacks

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(3) Frontage

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(4) Building Height

For buildings in Office Park Near Rail Station:

The maximum building height shall be 65 feet, except for architectural features such as towers, cupolas, and other elements that contribute to the overall character of the community, and have a footprint area of less than 300 square feet.

For buildings in Office Park in the Town Center District:

The maximum height building height shall be 45 feet, except for architectural features such as towers, cupolas, and other elements that contribute to the overall character of the community, and have a footprint area of less than 300 square feet.

(5) Landscape Improvements

The provisions of Section 5.0, Landscape Regulations, shall be used to determine the landscape requirements for development in the Office Park Area.

(6) Parking

The provisions of Section 7.0, Parking Regulations, shall be used to determine the parking for development in the Office Park Area.

(7) Signage

The provisions of Section 8.0, Signage Regulations, shall be used to determine the signage for development in the Office Park.



SOLEDAD DISTRICT

TALLER BUILDINGS

STRUCTURED OR INTERIOR LOT PARKING

COMPLIMENT RAIL STATION

"NEW HERITAGE" ARCHITECTURE



TOWN CENTER DISTRICT

ZERO FOOT SETBACK FROM PROPERTY LINE

CONTINUOUS BUILDING FORM

STRUCTURED OR INTERIOR LOT PARKING

"NEW HERITAGE" ARCHITECTURE

6.5.6 Business Park

The Business Park land use area is intended to permit light to medium manufacturing and limited large scale retail.

(A) Permitted Uses

The following uses shall be permitted in the Business Park area. Uses which are not listed as Permitted or Conditional shall be considered Prohibited. Prohibited uses may be reviewed by the Planning Commission for Conditional Use status if so determined by the Community Development Director.

1. Light Industrial

- a. Light wholesale, storage and distribution
- b. Custom manufacturing and assemble
- c. Light manufacturing and assembly
- d. Business support services
- e. Automotive rental agency car storage lots
- f. Wholesale/retail sales of goods produced on-site
- g. Auto and light truck repair - minor
- h. Public facilities and utilities
- i. Warehousing and distribution

2. Retail

- a. Within the 12.00 acre Business Park area adjacent to Santa Clarita Parkway, the regionally oriented retail sales of bulk merchandise, outlet, discount and similar merchandising is permitted.

B. Uses Subject to a Conditional Use Permit

Certain uses, while similar to the Permitted uses, may adversely affect surrounding properties and require additional consideration. Such uses shall require a conditional Use Permit. These uses include the following:

1. Caretaker's residence
2. Animal kennel
3. Structures exceeding the maximum height permitted by this Section.

C. Site Development Standards

(1) Parcel Size

One-half acre minimum.

(2) Setbacks

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(3) Frontage

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(4) Building Height

Thirty-five feet (35) maximum height.

(5) Landscape Improvements

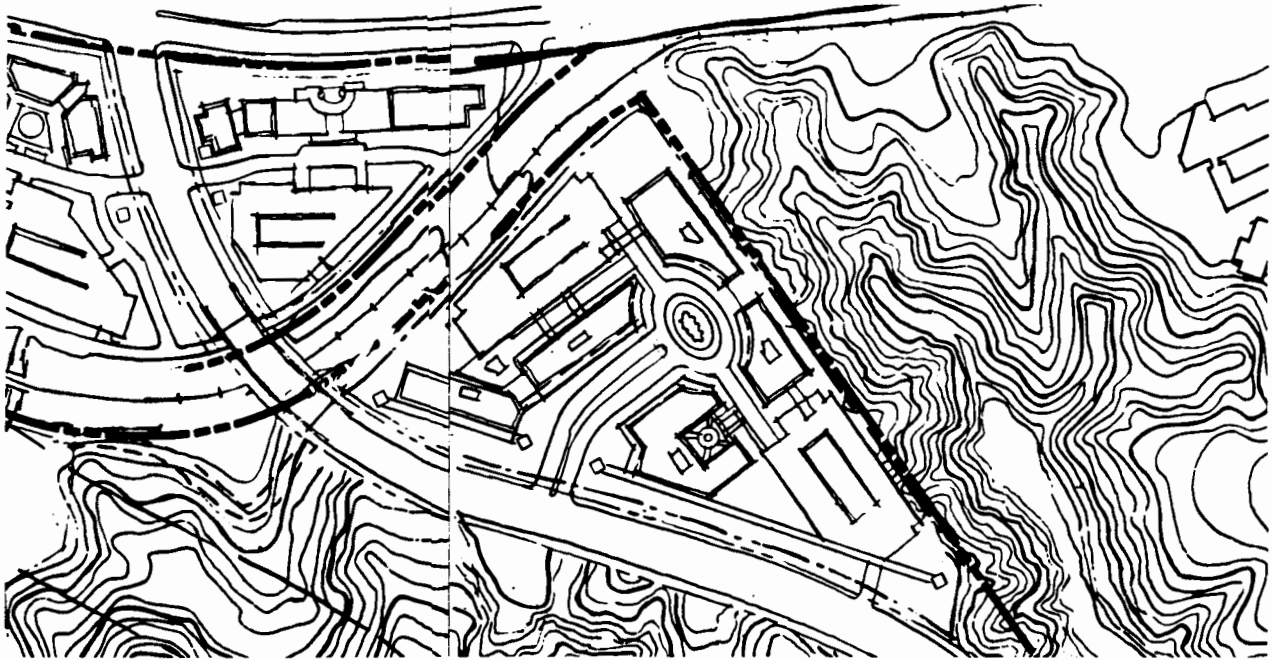
The provisions of Section 5.0, Landscape Regulations, shall be used to determine the landscape requirements for development in the Business Park Area.

(6) Parking

The provisions of Section 7.0, Parking Regulations, shall be used to determine the parking for development in the Business Park Area.

(7) Signage

The provisions of Section 8.0, Signage Regulations, shall be used to determine the signage for development in the Business Park.



STRUCTURAL RELATIONSHIP TO PARKWAY

INTERIOR LOT OR BUFFERED PARKING

NATURAL LANDSCAPE BACKGROUND



6.5.7 Institutional

The Institutional land use area is intended for uses which have a public or community orientation or benefit, such as non-profit clubs, meeting hall, fire station or house of worship, or similar.

(A) Permitted Uses

The following uses shall be permitted in the Institutional area. Uses which are not listed as Permitted or Conditional shall be considered Prohibited. Prohibited uses may be reviewed by the Planning Commission for Conditional Use status if so determined by the Community Development Director.

1. Institutional

- a. Non-profit meeting halls or clubs
- b. Day care center
- c. Private or parochial school
- d. Fire station
- e. House of worship
- f. Other uses consistent with the intended use of this Section.

B. Uses Subject to a Conditional Use Permit

Certain uses, while similar to the Permitted uses, may adversely affect surrounding properties and require additional consideration. Such uses shall require a conditional Use Permit. These uses include the following:

1. Structures exceeding the maximum height permitted by this Section.

C. Site Development Standards

(1) Parcel Size

Four acre minimum.

(2) Setbacks

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(3) Frontage

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(4) Building Height

Thirty-five feet (35) maximum height.

(5) Landscape Improvements

The provisions of Section 5.0, Landscape Regulations, shall be used to determine the landscape requirements for development in the Institutional Area.

(6) Parking

The provisions of Section 7.0, Parking Regulations, shall be used to determine the parking for development in the Institutional Area.

(7) Signage

The provisions of Section 8.0, Signage Regulations, shall be used to determine the signage for development in the Institutional.



“NEW HERITAGE” ARCHITECTURE

LANDMARK LOCATION

AESTHETIC RELATION TO SURROUNDING AREAS

6.5.8 School

The School land use area is intended for the use as an elementary school with playfields. If the School Area opts not to utilize the property for an elementary school, the property may be utilized Institutional or Multi-Family residential purposes (MF-22). The development provisions of those land use areas will then apply.

(A) Permitted Uses

The following uses shall be permitted in the School land use area. Uses which are not listed as Permitted or Conditional shall be considered Prohibited. Prohibited uses may be reviewed by the Planning Commission for Conditional Use status if so determined by the Community Development Director.

- a. Public school and accessory facilities

B. Uses Subject to a Conditional Use Permit

Certain uses, while similar to the Permitted uses, may adversely affect surrounding properties and require additional consideration. Such uses shall require a conditional Use Permit. These uses include the following:

- 1. Structures exceeding the maximum height permitted by this Section.

C. Site Development Standards

(1) Parcel Size

Ten (10) acre minimum

(2) Setbacks

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(3) Frontage

No minimums, subject to Design Review as Specified in Section 10.0, Design Review.

(4) Building Height

Forty feet maximum. Areas of the school which are traditionally taller, such as the gymnasium, natatorium or theater may exceed the height requirement as determined necessary by the City Planner.

(5) Landscape Improvements

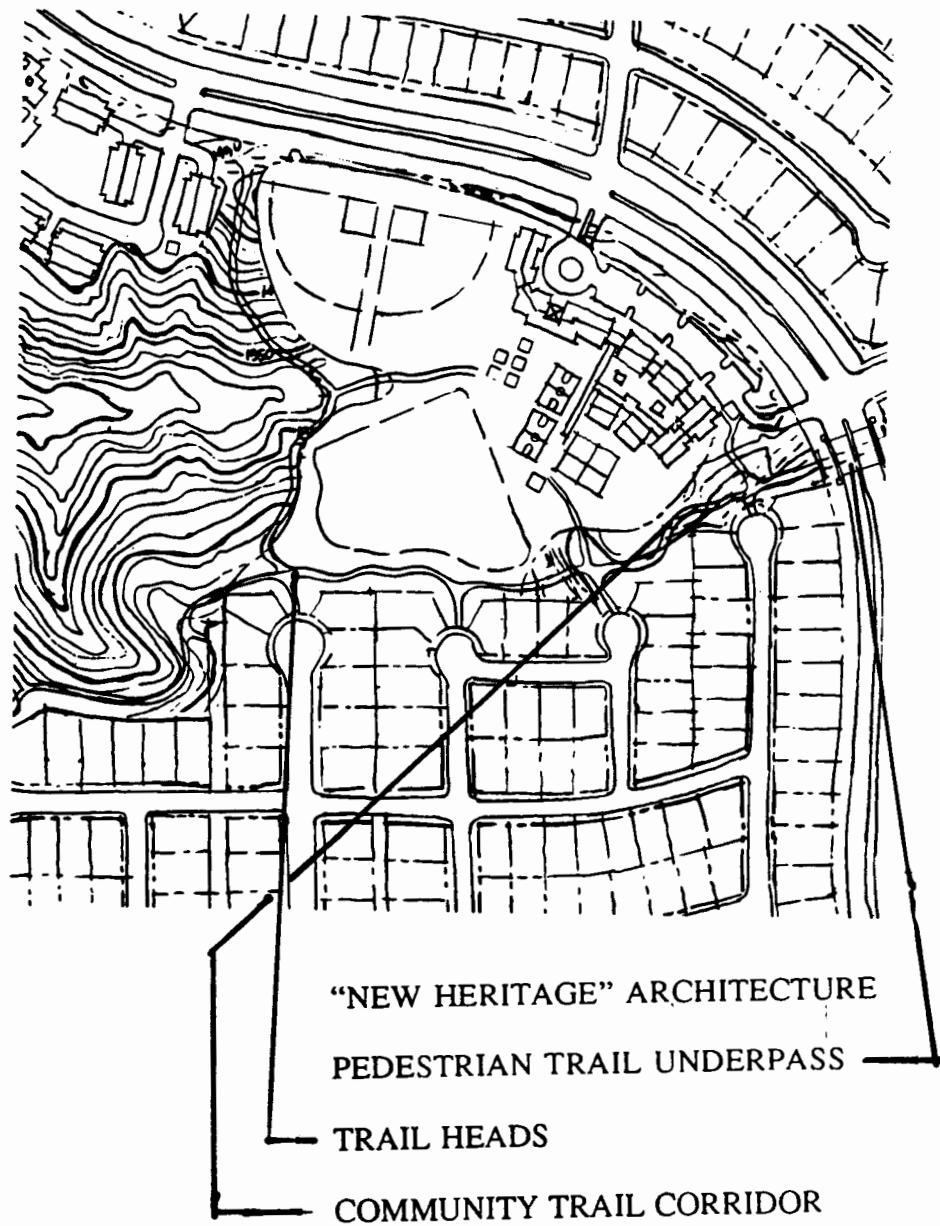
The provisions of Section 5.0, Landscape Regulations, shall be used to determine the landscape requirements for development in the School land use area.

(6) Parking

The provisions of Section 7.0, Parking Regulations, shall be used to determine the parking for development in the School area.

(7) Signage

The provisions of Section 8.0, Signage Regulations, shall be used to determine the signage for development in the School.



6.3 Residential Land Use Standards

The following paragraphs detail the permitted and conditional land uses and development standards within the individual districts. If a use is not listed as permitted or conditional, it shall be considered to be prohibited. At the discretion of the Community Development Director, uses which are prohibited may be reviewed by the Planning Commission for a Conditional Use status.

6.3.1 Single Family 10,000 (SF 10,000)

Homes within this area may be constructed as either custom or tract. The design of this area is intended to reflect the existing scale of the adjacent large lot community. Parcelization of lots in this community include only the generally buildable pad areas, which does not encroach into the canyon area. This derivation from the established pattern was done in an effort to ensure that the canyon would enjoy the protection of the Open Space land use designation, rather than potential disturbance under private ownership.

The following regulations apply:

A. Uses Permitted

- (1) Single family detached home
- (2) Open space, parks & trails
- (3) Neighborhood recreation facilities and buildings

B. Accessory Uses Permitted

- (1) Garages and carports, in compliance with site development standards provided herein.
- (2) Fences, walls & trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use as determined by the Community Development Director.

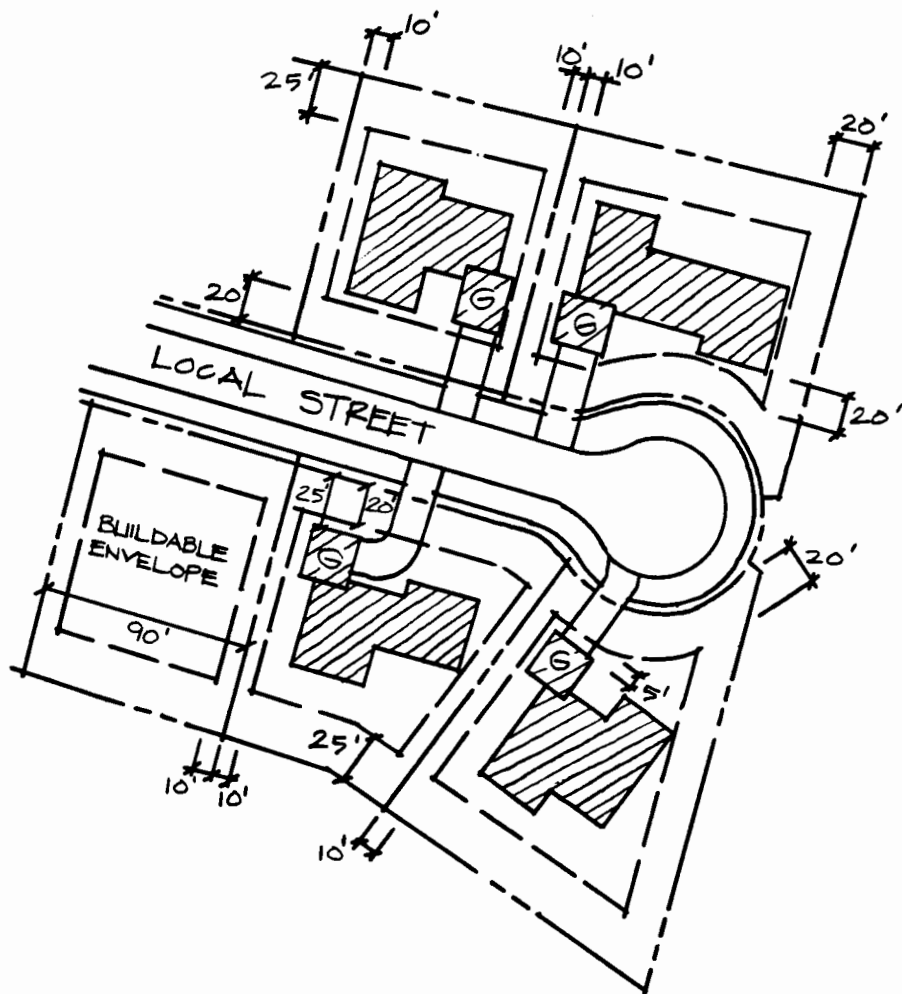
C. Uses Subject to a Conditional Use Permit

- (1) Second dwelling units (Granny Flats)
- (2) Keeping of exotic birds or animals, or more than three weaned dogs and/or cats.
- (3) Agricultural or horticultural uses for commercial purposes.

- (4) The outdoor sale of agricultural products.
- (5) Community recreation facilities and buildings.
- (6) In-home office

D. Site Development Standards

- (1) Minimum lot area: 10,000 square feet, minimum; it is intended that lots in this area range from 10,000 to 14,000 square feet. May exceed this range if slopes are present.
- (2) Minimum lot width: 90 feet minimum average width at mid-point depth.
- (3) Building coverage: 40% maximum.
- (4) Building setbacks: front yard 20 feet minimum; garage 25 feet minimum; interior side yard, 5 feet minimum on one side, 10 feet minimum on the other, corner (frontage) side yard, 20 feet; rear yard, 25 feet minimum. Zero lot lines for detached rear yard garages.
- (5) Building height: 35 foot maximum. No exterior wall plane shall measure more than 25 feet vertical.



6.3.2 Single Family 8,000 (SF 8,000)

Homes within this area will be constructed as either custom or tract homes. Individual lot sizes are intended to be 8,000 square feet.

The following standards shall apply:

A. Uses Permitted

- (1) Single family detached home
- (2) Open space, parks & trails
- (3) Neighborhood recreation facilities and buildings

B. Accessory Uses Permitted

- (1) Garages and carports, in compliance with site development standards provided herein.
- (2) Fences, walls & trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use as determined by the Community Development Director.

C. Uses Subject to a Conditional Use Permit

- (1) Second dwelling units (Granny Flats)
- (2) Keeping of exotic birds or animals, or more than three weaned dogs and/or cats.
- (3) Community recreation facilities and buildings.
- (4) Private or parochial school
- (5) The outdoor sale of agricultural products
- (6) Day care center

[illegible]

- [illegible]



6.3.3 Single Family 6,000 (SF 6,000)

There are three SF 6,000 areas within the community. The following regulations apply:

A. Uses Permitted

- (1) Single family detached home
- (2) Open space, parks & trails
- (3) Neighborhood recreation facilities and buildings

B. Accessory Uses Permitted

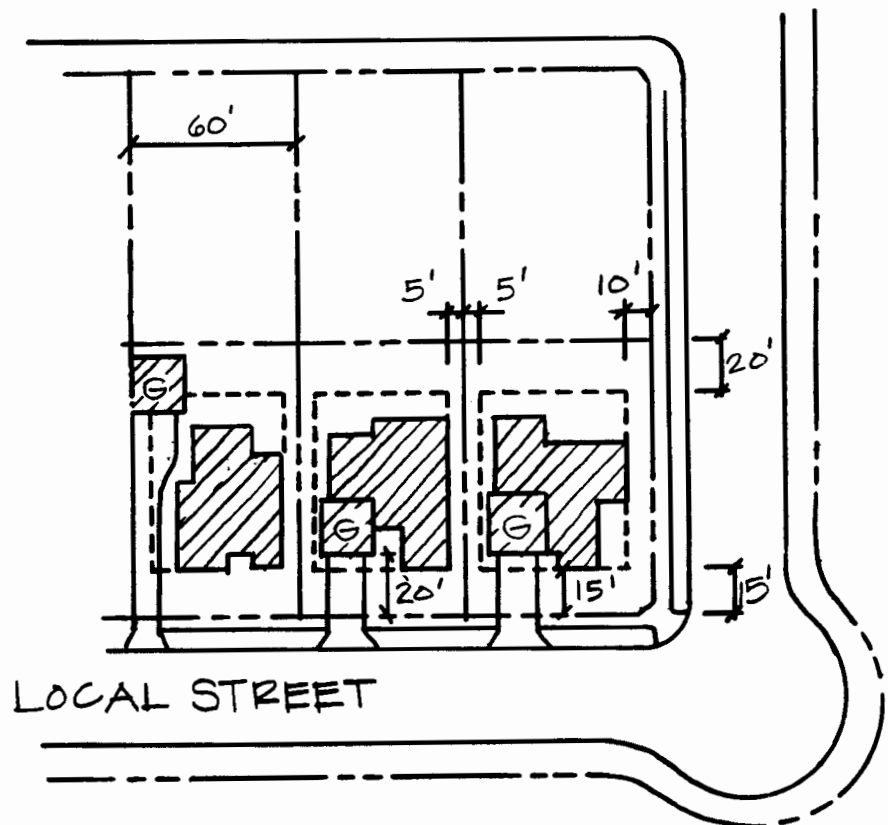
- (1) Garages and carports, in compliance with site development standards provided herein.
- (2) Fences, walls & trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use as determined by the Community Development Director.

C. Uses Subject to a Conditional Use Permit

- (1) Second dwelling units (Granny Flats)
- (2) Keeping of more than three weaned dogs and/or cats.
- (3) Community recreation facilities and buildings.
- (4) Parochial or private schools.
- (5) The outdoor sale of agricultural products.
- (6) In-home office
- (7) Day care center

D. Site Development Standards

- (1) Minimum lot area: 6,000 square feet, minimum; it is intended that residential lots in this area range from 6,000 to 7,000 square feet. May exceed this range if slopes are present.
- (2) Minimum lot width: 50 feet minimum average width at mid-point.
- (3) Building coverage: 50% maximum
- (4) Building setbacks: front yard 15 feet minimum, garage 20 feet minimum; interior side yard, 5 feet minimum, corner (frontage) side yard, 10 feet; rear yard, 20 feet minimum. Zero lot lines for rear yard detached garages. Shared driveways are permitted to access rear yard garages.
- (5) Building height: 25 foot maximum. No exterior wall plane shall measure more than 25 feet vertical.



6.3.4 Single Family 5,000 (SF 5,000)

Two land use areas within the community are designated as SF 5,000. The following regulations apply:

A. Uses Permitted

- (1) Single family detached home
- (2) Zero lot line dwelling
- (3) Open space, parks & trails
- (4) Neighborhood recreation facilities and buildings

B. Accessory Uses Permitted

- (1) Garages and carports, in compliance with site development standards provided herein.
- (2) Fences, walls & trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use as determined by the Community Development Director.

C. Uses Subject to a Conditional Use Permit

- (1) Second dwelling units (Granny Flats)
- (2) Keeping more than three weaned dogs and/or cats.
- (3) Community recreation facilities.
- (4) Parochial or private schools
- (5) The outdoor sale of agricultural products
- (6) In-home offices

6.3.5 Single Family 4,000 (SF 4,000)

There are three areas within the community which are designated as SF 4,000. The following regulations apply:

A. Uses Permitted

- (1) Single Family Detached Home
- (2) Zero lot line dwelling
- (3) Open space, parks & trails
- (4) Neighborhood Recreation Facilities and Buildings

B. Accessory Uses Permitted

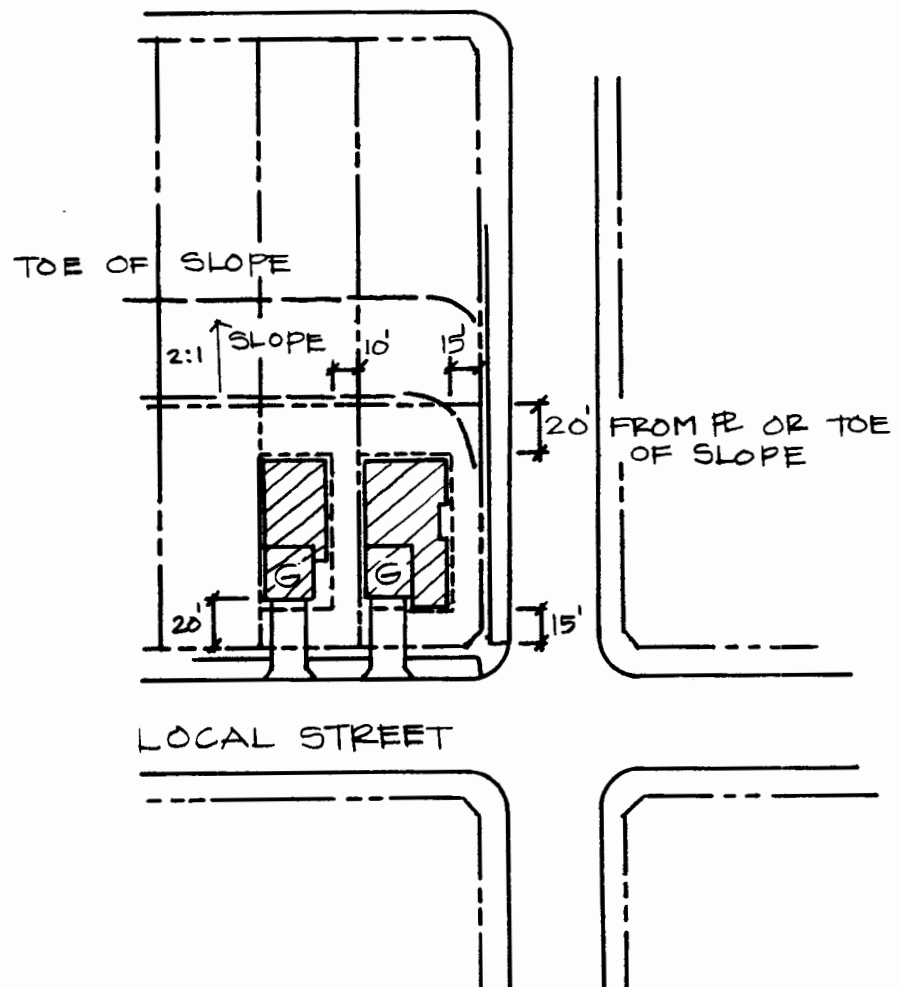
- (1) Garages and carports, in compliance with site development standards provided herein.
- (2) Fences, walls & trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use as determined by the Community Development Director.

C. Uses Subject to a Conditional Use Permit

- (1) Second dwelling units (Granny Flats)
- (2) Keeping more than three weaned dogs and/or cats.
- (3) Community recreation facilities and buildings.
- (4) Parochial or private schools
- (5) The outdoor sale of agricultural products
- (6) In-home office

D. Site Development Standards

- (1) Minimum lot area: 4,000 square feet, minimum, it is intended that residential lots in this area range from 4,000 square feet to 5,000 square feet. May exceed this range if slope is present.
- (2) Minimum lot width: 40 feet minimum average width
- (3) Building coverage: 50% maximum
- (4) Building setbacks: front yard, 15 feet minimum, 20 feet for garage; side yard, zero feet minimum, provided that the combined setbacks from both the side property lines on any building site shall total not less than ten feet; rear yard, 20 feet.
- (5) Building height: 35 foot maximum. No exterior wall plane shall measure more than 25 feet vertical.



6.3.6 Z-Lot Residential (SF-Z)

There is one area within the Specific Plan which utilizes the Z-lot land use area. The following shall apply.

A. Uses Permitted

- (1) Single Family Detached Home
- (2) Zero lot line dwelling
- (3) Open space, parks & trails
- (4) Neighborhood Recreation Facilities and Buildings

B. Accessory Uses Permitted

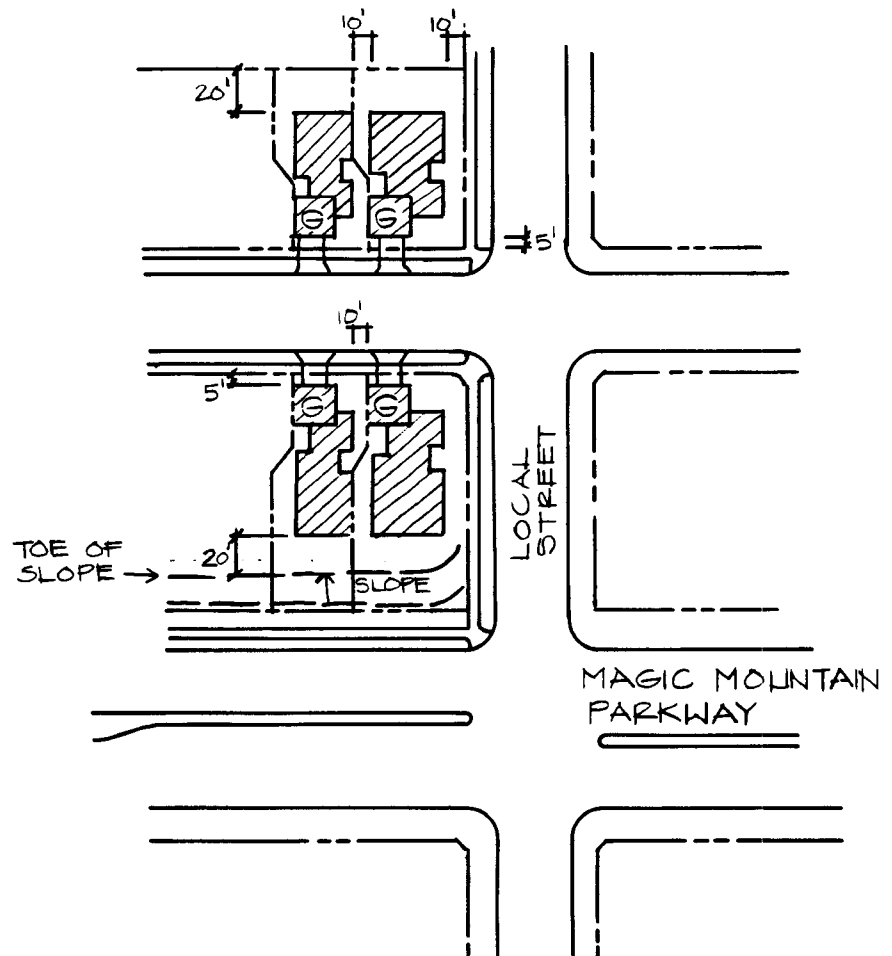
- (1) Garages and carports, in compliance with site development standards provided herein.
- (2) Fences, walls & trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use as determined by the Community Development Director.

C. Uses Subject to a Conditional Use Permit

- (1) Second dwelling units (Granny Flats)
- (2) Keeping more than three weaned dogs and/or cats.
- (3) Community recreation facilities and buildings.
- (4) Parochial or private schools
- (5) In-home office
- (6) The outdoor sale of agricultural products

D. Site Development Standards

- (1) Minimum lot area: 3,000 square feet, it is intended that residential lots in this area vary from 3,000 to 4,000 square feet. May exceed this range if slopes are present.
- (2) Minimum lot dimensions: 35 feet minimum average width.
- (3) Building coverage: 60% maximum
- (4) Building setbacks: front yard, 5 feet minimum if automatic garage door opener is provided, otherwise 20 feet from back of sidewalk to the garage; side yard, zero feet minimum, provided that the combined distance between structures from both the side property lines on any building lot shall total not less than ten feet; rear yard, twenty feet.
- (5) Building height: 35 foot maximum. No exterior wall plane shall measure more than 25 foot vertical.



6.3.7 Paired Single Family Residential (SFP)

The following regulations are applicable for these areas:

A. Uses Permitted

- (1) Single family attached homes (paired)
- (2) Open space, parks & trails
- (3) Neighborhood recreation facilities and buildings
- (4) Zero lot line

B. Accessory Uses Permitted

- (1) Garages and carports, in compliance with site development standards provided herein.
- (2) Fences, walls & trellises
- (3) Swimming pools
- (4) Accessory uses and structures necessary or customarily incidental to a principal use as determined by the City Planner.

C. Uses Subject to a Conditional Use Permit

- (1) Second dwelling units (Granny Flats)
- (2) Keeping more than three weaned dogs and/or cats.
- (3) Community recreation facilities and buildings.
- (4) Parochial or private schools
- (5) In-home office
- (6) The outdoor sale of agricultural products

Category	Item	Value
Agriculture	Wheat	12.5
	Corn	8.7
	Soybeans	15.2
	Rice	3.1
	Cotton	6.8
	Almonds	4.9
	Apples	2.3
	Grapes	1.8
	Oranges	3.5
	Pears	1.2
Manufacturing	Automotive	22.1
	Electronics	18.9
	Chemicals	14.3
	Metals	11.7
	Plastics	9.5
	Textiles	7.2
	Food Processing	6.8
	Pharmaceuticals	5.4
	Printing	3.9
	Other	2.1
Services	Healthcare	16.7
	Education	13.2
	Retail	11.5
	Finance	9.8
	Transportation	8.4
	Real Estate	7.1
	Professional Services	6.3
	Arts and Entertainment	5.6
	Public Administration	4.9
	Other	3.2

- | Category | Item | Value |
|---------------|------------------------|-------|
| Agriculture | Wheat | 12.5 |
| | Corn | 8.7 |
| | Soybeans | 15.2 |
| | Rice | 3.1 |
| | Cotton | 6.8 |
| | Almonds | 4.9 |
| | Apples | 2.3 |
| | Grapes | 1.8 |
| | Oranges | 3.5 |
| | Pears | 1.2 |
| Manufacturing | Automotive | 22.1 |
| | Electronics | 18.9 |
| | Chemicals | 14.3 |
| | Metals | 11.7 |
| | Plastics | 9.5 |
| | Textiles | 7.2 |
| | Food Processing | 6.8 |
| | Pharmaceuticals | 5.4 |
| | Printing | 3.9 |
| | Other | 2.1 |
| Services | Healthcare | 16.7 |
| | Education | 13.2 |
| | Retail | 11.5 |
| | Finance | 9.8 |
| | Transportation | 8.4 |
| | Real Estate | 7.1 |
| | Professional Services | 6.3 |
| | Arts and Entertainment | 5.6 |
| | Public Administration | 4.9 |
| | Other | 3.2 |



6.3.8 Multi-family Residential (MF-12)

The following regulations are applicable for this area.

A. Uses Permitted:

- (1) Multiple-family dwellings
- (2) Private recreation facilities and buildings
- (3) Open space, parks & trails

B. Accessory Uses Permitted:

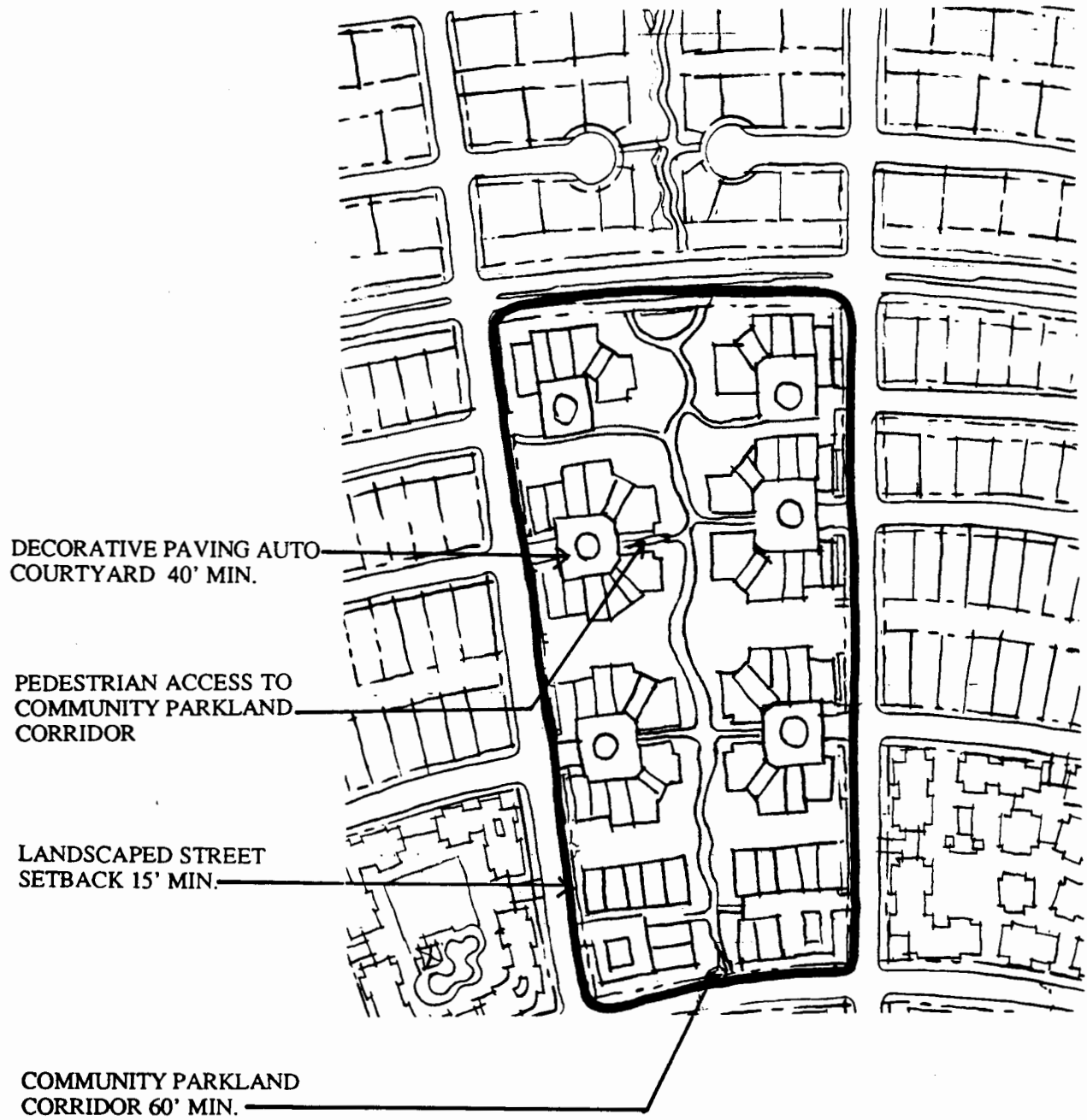
- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Fences, walls and trellises
- (3) Swimming pools
- (4) Neighborhood recreational facilities and structures
- (5) Accessory uses and structures necessary or customarily incidental to a principal use

C. Uses Subject to a Conditional Use Permit:

- (1) Family day care homes
- (2) Keeping more than two weaned dogs or cats
- (3) Parochial or private schools
- (4) Convalescent, retirement, rest homes, sanitariums or similar congregate care facilities
- (5) Private or non-profit clubs
- (6) The outdoor sale of agricultural products

D. Site Development Standards:

- (1) Minimum Site Area: No minimum
- (2) Minimum Site Width: No minimum
- (3) Building Site Coverage: 60% maximum
- (4) Building Setbacks: street frontage, 15 foot minimum
- (5) Building Height: 40 feet, maximum; for towers, cupolas, and other building elements which have a floor area of less than 250 square feet, 65 feet.
- (6) Landscape: Required for common areas and parking lots.
- (7) Public Open Space: A minimum 60 foot community parkland corridor will evenly bisect the MF 12 area. Access to this parkland must be provided from exterior public streets as indicated in Section 5.0, Master Landscape Plan (Special Considerations, MF12).
- (8) Minimum distance of opening through buildings to parkland corridor from streets shall be 20'
- (9) Motor courtyard minimum width of 40'; motor courtyards shall be used as access and plaza use only, no parking.
- (10) Guest Parking spaces as required may be located in angled parking areas on public right-of-way.



6.3.9 Multi-family Residential (MF-18)

The following regulations are applicable for these areas:

A. Uses Permitted:

- (1) Multiple-family dwellings
- (2) Private recreation facilities and buildings
- (3) Open space, parks & trails

B. Accessory Uses Permitted:

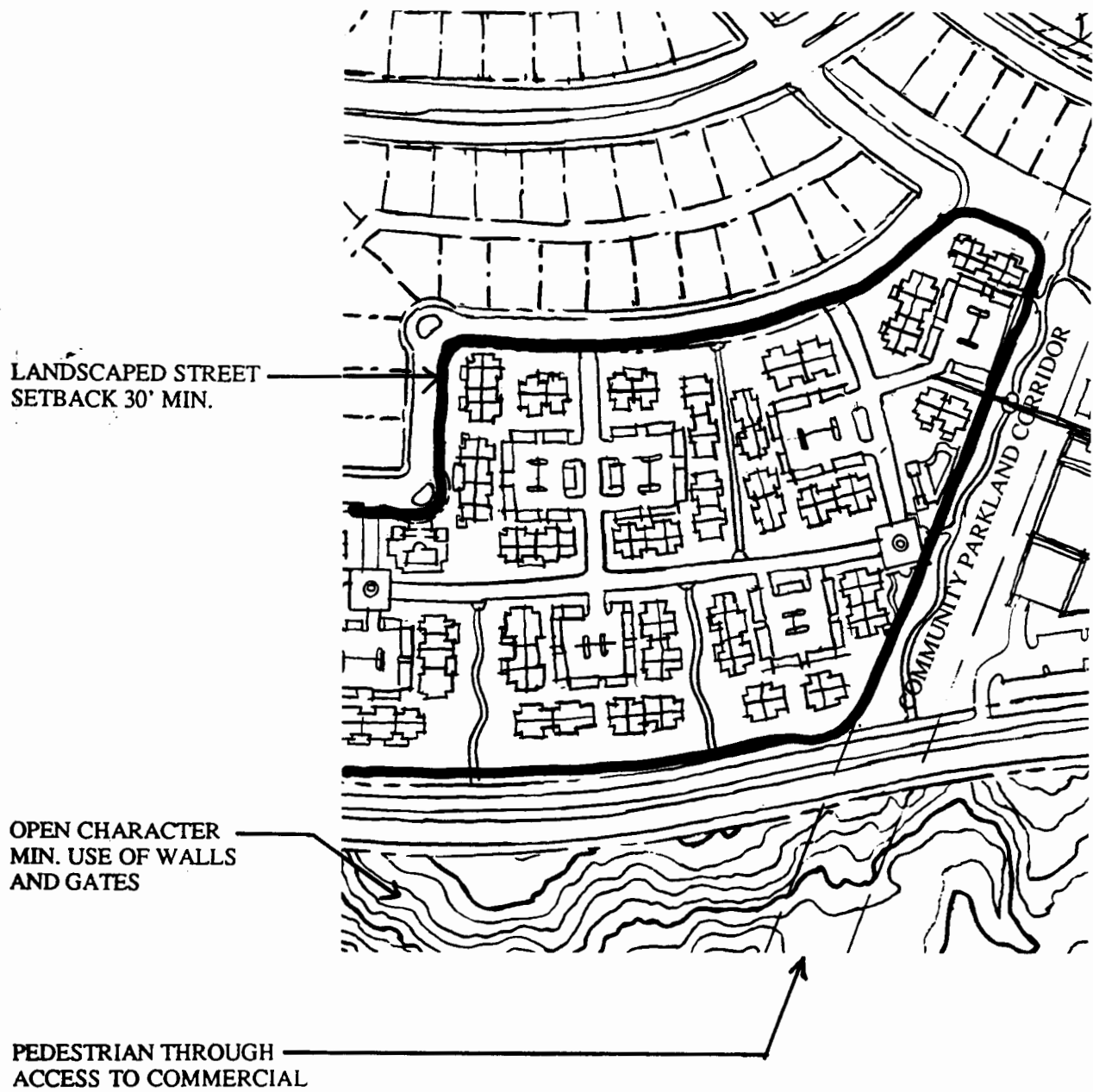
- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Fences, walls and trellises
- (3) Swimming pools
- (4) Neighborhood recreational facilities and structures
- (5) Accessory uses and structures necessary or customarily incidental to a principal use.

C. Uses Subject to a Conditional Use Permit:

- (1) Family day care centers
- (2) Keeping more than two weaned dogs or cats
- (3) Parochial or private schools
- (4) Convalescent, retirement, rest homes, sanitariums or similar congregate care facilities
- (5) Private, non-profit clubs
- (6) In-home office
- (7) The outdoor sale of agricultural products

D. Site Development Standards:

- (1) Minimum Site Area: No minimum
- (2) Minimum Site Width: No minimum
- (3) Building Site Coverage: 70% maximum
- (4) Building Setbacks: street frontage yard, 30 foot minimum; No dwelling or other main building shall be closer than 15 feet to any other dwelling or main building.
- (5) Building Height: 40 feet, maximum; for towers, cupolas, and other building elements which have a floor area of less than 250 square feet, 65 feet.
- (6) Landscape: Required for common areas and parking lots.
- (7) Public Open Space: A 100 foot community parkland corridor will be integrated within the MF 18 Area as illustrated Section 5.0, Master Landscape Plan (Special Considerations, MF 18)



6.3.10 Multi-family Residential (MF 22)

The following regulations are applicable for these areas.

A. Uses Permitted:

- (1) Multiple-family dwellings
- (2) Private recreation facilities and buildings
- (3) Open space, parks & trails

B. Accessory Uses Permitted:

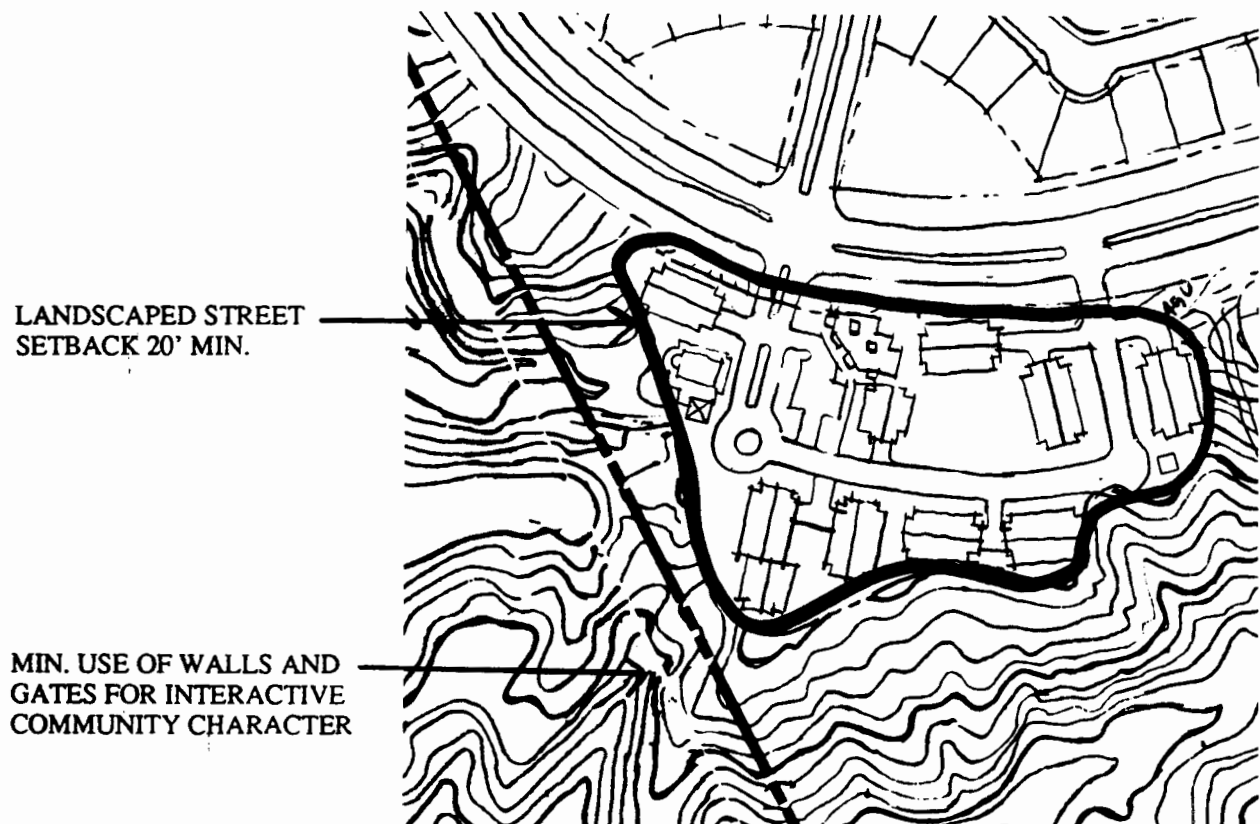
- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Fences, walls and trellises
- (3) Swimming pools
- (4) Neighborhood recreational facilities and structures
- (5) Accessory uses and structures necessary or customarily incidental to a principal use

C. Uses Subject to a Conditional Use Permit:

- (1) Family day care homes
- (2) Keeping more than two weaned dogs or cats
- (3) Parochial or private schools
- (4) Convalescent, retirement, rest homes, sanitariums or similar congregate care facilities
- (5) Private, non-profit clubs

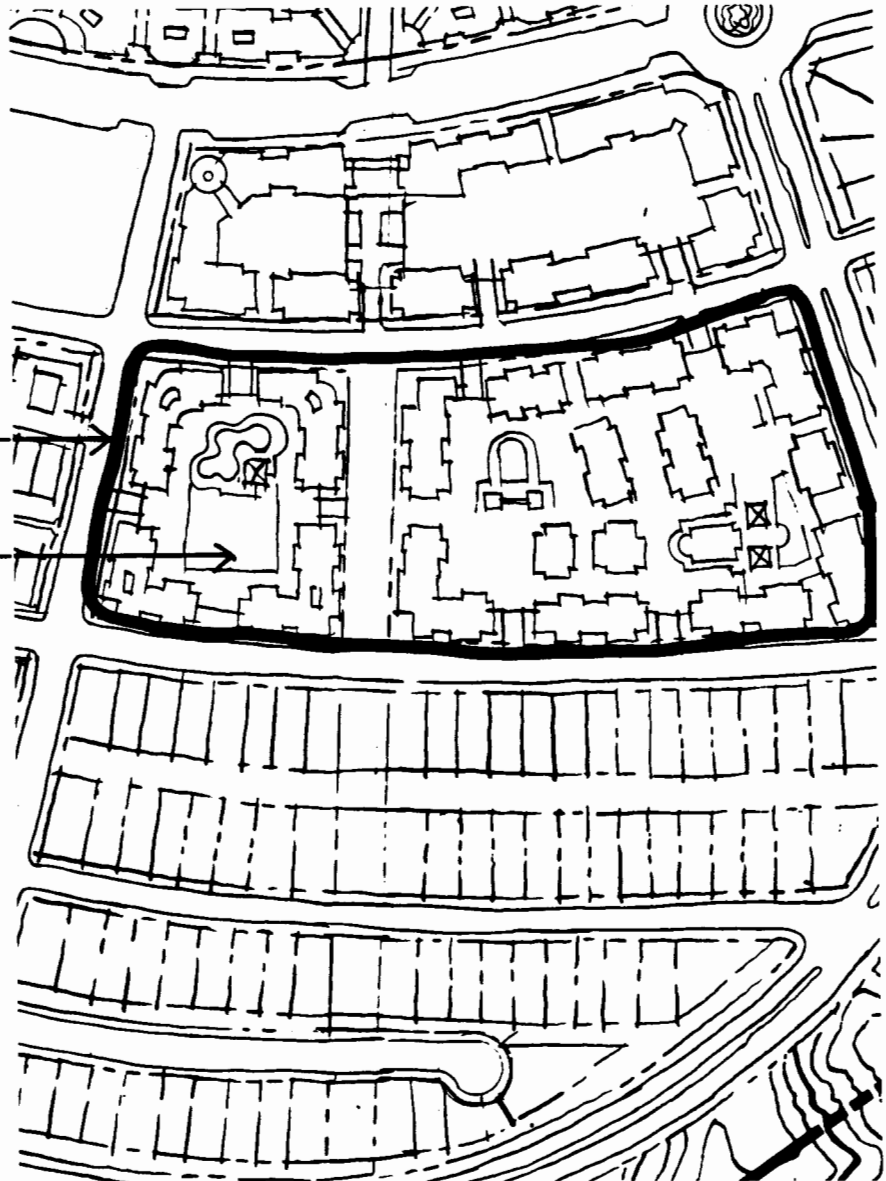
D. Site Development Standards:

- (1) Minimum Site Area: No minimum
- (2) Minimum Site Width: No minimum
- (3) Building Site Coverage: 70% maximum
- (4) Building Setbacks: street frontage yard in Central District, 20 feet, in Town Center District , zero feet; No dwelling or other main building shall be closer than 15 feet to any other dwelling or main building.
- (5) Building Height: 40 feet, maximum; for towers, cupolas, and other building elements which have a floor area of less than 250 square feet, 65 feet.
- (6) Landscape: Required for common areas and parking lots.



ZERO FOOT PROPERTY
SETBACK

UNDERGROUND PARKING



6.3.11 Multi-family Residential (MF 40)

The following regulations are applicable for these areas.

A. Uses Permitted:

- (1) Multiple-family dwellings
- (2) Private recreation facilities and buildings
- (3) Open space, parks & trails

B. Accessory Uses Permitted:

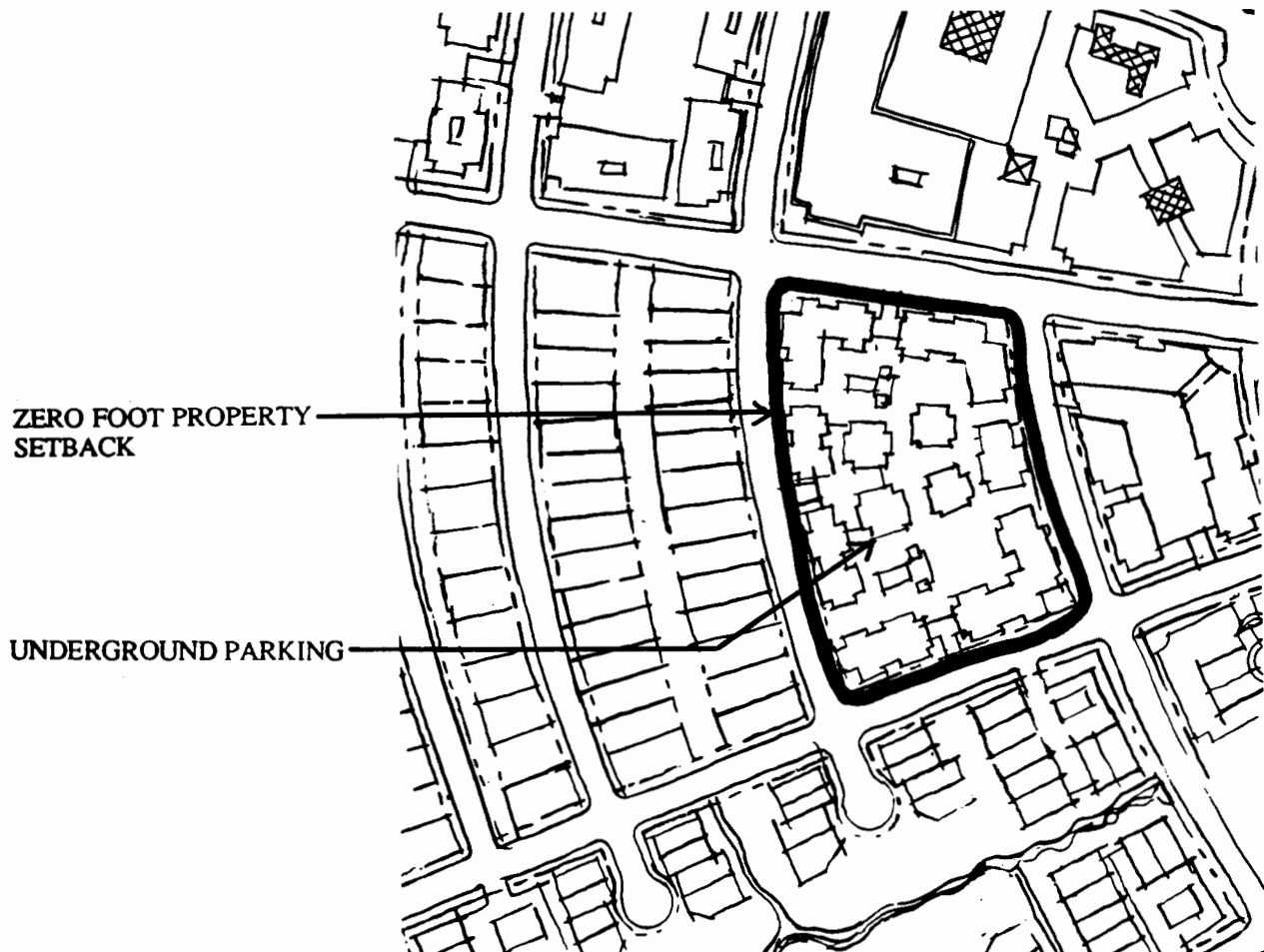
- (1) Garages and carports, in compliance with site development standards provided herein
- (2) Parking structures in compliance with the development standards and guidelines of this Plan
- (3) Fences, walls and trellises
- (4) Swimming pools
- (5) Neighborhood recreational facilities and structures
- (6) Accessory uses and structures necessary or customarily incidental to a principal use

C. Uses Subject to a Conditional Use Permit:

- (1) Family day care homes
- (2) Keeping more than two weaned dogs or cats
- (3) Parochial or private schools
- (4) Convalescent, retirement, rest homes, sanitariums or similar congregate care facilities
- (5) Private, non-profit clubs
- (6) Restaurant/pro-shop/clubhouse in association with recreational facility

D. Site Development Standards:

- (1) Minimum Site Area: No minimum
- (2) Minimum Site Width: No minimum
- (3) Building Site Coverage: 80% maximum
- (4) Building Setbacks: street frontage yard, zero foot minimum in Town Center District, 20 feet minimum in Soledad District; rear yard, 20 feet. No dwelling or other main building shall be closer than 20 feet to any other dwelling or main building.
- (5) Building Height: a 60 feet, maximum; for towers, cupolas, and other building elements which have a floor area of less than 300 square feet, 75 feet.
- (6) Landscape: Required for common areas and parking lots.

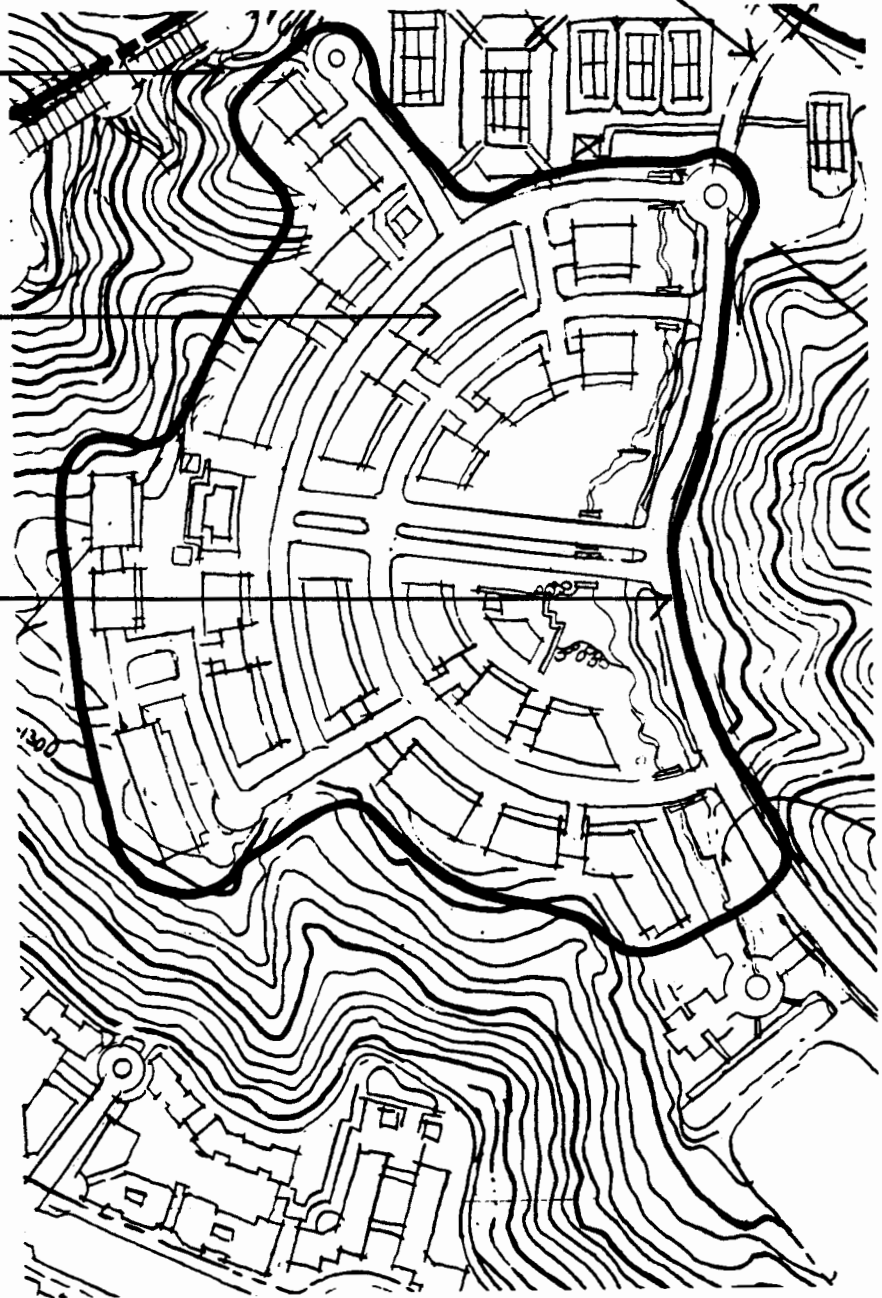


PEDESTRIAN UNDERPASS TO
COMMUTOR STATION AND
COMMERCIAL/OFFICE

ACCESS TO ESCALATOR
SYSTEM

UNDERGROUND PARKING

LANDSCAPED STREET
SETBACK 20' MIN.



6.3.12 Town Center Mixed-use (TC)

The Town Center is a mixed use land use area, which includes a residential component. The residential development standards for these uses are located within the Town Center Development Standards.

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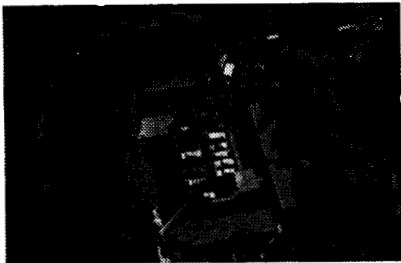
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- 1.0 The Specific Plan as Planning Tool*
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-
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-
- 8.0 Sign Regulations*
- 9.0 Community Lighting Regulations*
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- 11.0 Design Review*
- 12.0 Implementation*

7.0 PARKING REGULATIONS

The purpose of this Section is to provide for sufficient parking and loading spaces for all land uses and to assure the provision and maintenance of safe, adequate and well-designed parking facilities. It is the intent of this section that the number of parking spaces shall be in proportion to the need created by the particular type of use & use area. The standards for parking facilities are intended to reduce street congestion and traffic hazards, promote vehicular and pedestrian safety and the efficient use of land.



Parking and loading areas shall be established in a manner which will promote compatibility between parking facilities and surrounding neighborhoods, protect property values and enhance the environment through good design by providing such amenities as landscaping, walls, fencing and setbacks, improve the appearance of parking lots, yards, uncovered sales areas and buildings, control heat, wind and air pollutants, minimize nuisances, and promote aesthetic values and the general well-being of the residents of the Specific Plan area. Parking shall be provided in accordance with this section when the building or structure is constructed or the use is established.

Parking facilities may be phased to coincide with the demand for parking. Interim parking will be subject to the demand requirement, however, improvements will be installed at the discretion of the planning department. Areas which will eventually require structured parking may utilize interim surface parking within vacant lots, provided an agreement is made among the affected property owners and the city.

7.1 General Provisions

All required parking, loading spaces and driveways shall be maintained in good condition and available for its intended use as long as the use for which it was required continues to operate or exist. Except within in residential garages, no storage shall encroach into required parking or loading space or driveway, and no vehicles shall be continuously parked in these areas, and all vehicles utilizing the parking areas shall be operable and have current registration.

A. Calculations

Where calculations of the number of spaces required results in a fractional number, the next higher whole number shall be used.

In the cases of mixed use in a building or on a lot, the total requirements for off-street parking shall be the sum of the requirements for the various uses computed separately, except for shared parking as described in Section 7.7.

B. Garages

When a garage is specifically required, or provided to meet required parking, entry doors shall be maintained in an operable condition at all times, and no structural alteration or obstruction shall be permitted within the required parking area which would reduce the number of required parking spaces. Use of garages shall be limited to vehicular and general storage purposes only and shall not conflict with any applicable building, housing, or fire codes.

C. Use of Vacant Lots

Vacant lots, privately owned lots, and parking lots of commercial and industrial businesses shall not be used for the advertising and sale of motor vehicles, boats or trailers, or similar property unless they are duly licensed by the City to do so, except that occupied private property may be used for the sale of personal vehicles of the property owner's providing not more than one vehicle may be displayed which is not otherwise prohibited by this Chapter and subject to the provisions of Section 8.0, "Sign Regulations."

No vehicle, boat, trailer or similar shall be parked or stored on any vacant or undeveloped property in any area, unless it is associated with the construction, sales or leasing or a development of Porta Bella.

7.2 Number of Parking Spaces Required

The following minimum numbers of off-street parking spaces shall be provided in accordance with this Section for all new building or uses, and when any building or use is altered, extended, changed, or intensified:

A. Residential Uses

(1) Single-family detached and attached dwellings: Two enclosed spaces per dwelling.

(2) Multi-family developments: One covered space and one half uncovered space per dwelling, plus one half space for each dwelling with more than one bedroom and one guest space for each five dwelling units in the project.

(3) Senior Citizen Housing: Eight-tenths space per dwelling unit.

B. Places of Assembly

Auditoriums, house of worship, theaters, community center, performing arts center and places of assembly: One space per each four seats. Where there are no fixed seats, one space per thirty-five square feet of floor area in places of assembly. Where fixed seats consist of pews or benches, eighteen lineal inches of pew or bench shall be considered one seat.

C. Recreation Facilities

- (1) Game courts, such as tennis and racquetball: two spaces for each court.
- (2) Stadium tennis courts: One space for each three seats, and/or, one space for each 21 square feet of grass slope seating, and/or, one space for each 54 inches of bench for bench seating.
- (3) Golf driving range: one space for each tee, plus one space for each employee.
- (4) Local Public Parks of less than fifteen acres not shared with an adjacent school: two spaces per acre of park with less than ten percent slope. If clearly marked on-street parking is available adjacent to a park-site, those spaces may be utilized to meet the parking requirements.

D. Lodging Facilities

Hotels and motels: One space per room or suite, plus one space per every three employees on the largest workshift.

E. Restaurants

Fast Food: One space per eighty square feet gross area.

Sit Down: One space per one-hundred square feet gross area.

F. Private Schools

(1) Elementary Schools: One space per teacher and staff member, plus one space per two classrooms.

(2) Day Care: One space per each employee plus one space for each ten children the facility is designed to accommodate.

G. Offices

Three spaces for each 1,000 square feet of gross floor area

H. Medical Related

One-space per each 250 square feet of gross floor area.

I. Retail/Commercial

One space per each 250 square feet of gross floor area.

One space per each 500 square feet in the Town Center and Soledad mixed use area.

J. Service

One space per each 350 square feet of gross floor area.

7.3 Size of Parking Spaces and Required Aisle Width

Parking spaces shall have the following minimum clear dimensions and aisle width as indicated below:

(1) For multi-car commercial parking structures or carports the minimum width for standard size parking stalls shall be 9-1/2 feet; for compact cars, the minimum width shall be 8-1/2 feet;

(2) Open or uncovered spaces with side abutting a wall, building, fence, or other obstruction shall be two feet wider than the standard required width.

(3) Parking space length may include a two foot allowance for vehicle overhang of curb or landscape areas.

(4) The required dimensions of a commercial parking structure or carport shall be measured from the interior of the structure or carport.

(5) A maximum of 40 percent of the total number of parking spaces may be compact spaces.

(6) Parking for the handicapped shall be provided in accordance with State and local requirements.

(7) Recreational vehicles, including boats & trailers, shall not be parked on any street for more than 72 continuous hours. Recreational vehicles shall not be allowed to remain in front yard drive-ways for more than 72 continuous hours. Short-term visitors are excepted to this provision.

(8) Permanent RV parking will only be permitted in the side or rear yards where specifically improved for such parking.

7.4 Loading Space Requirements

(1) Loading areas shall be designed to provide for backing and maneuvering on-site and not from a public street, except in the Town Center and Rail Station mixed use districts where such maneuvering may be permitted subject to review by the City Planner;

(2) Loading areas shall not encroach into landscape setbacks;

(3) All loading areas shall be screened from adjacent parcels and streets by a combination of screenwalls, landscaping and/or portions of the building. If the adjoining portion of the parcel is also a loading area, screening may be eliminated subject to review by the City Planner;

(4) Loading area shall be located away from public view, usually to the side or rear of the building;

(5) Loading areas shall be of a minimum 12' x 24' in dimension.

7.5 Drive-Through Establishments

Notwithstanding any other provisions of this chapter, additional vehicle stacking spaces shall be provided for all establishments having vehicle pick-up windows as follows:

(1) A drive-through lane with a minimum stacking for six vehicles shall be provided at twenty feet per vehicle. These cars may be counted towards the parking requirements of the development.

(2) The drive-through lane shall be designed such that it will not interfere with free and orderly circulation of the parking lot;

(3) The drive-through lane shall not encroach upon or block driveways or parking spaces and shall be separated from adjoining driveways, parking spaces, and property lines by a landscaped planter a minimum of five feet in width;

7.6 Shared and Off-Site Parking

The shared parking design criteria, methodologies and procedures put forth in the publication, *Shared Parking: A Study Conducted Under the Direction of the Urban Land Institute*, by Barton Aschman Associates, Inc., and published in 1987 by the Urban Land Institute (International Standard Book #0-87420-9 and Library of Congress Card Catalog Number 83-51648) (“ULI Criteria”) shall be considered acceptable within the non-residential districts.

The incorporation of shared parking into commercial districts shall be made at the time of the approval of the accompanying, discretionary development permit.

In the event of a mix of uses is not readily identifiable by ULI criteria, the Planning Department may utilize reliable parking standards presented by the client or the city staff.

Parking lots for buildings used principally on holidays (e.g. religious buildings) must be graded, compacted and planted, but may be left unpaved.

Some building sites may be set aside for temporary parking on an interim basis by Conditional Use Permit.

7.7 Circulation and Parking Space Layout

All parking areas (except parking structures) shall be designed as follows:

(1) The location and dimensions of aisle areas adjacent to parking spaces shall be arranged in accordance with the minimum parking standards required in this Section.

(2) For all uses other than one-family and two family dwellings located in residential districts, parking shall be arranged so as to permit vehicles to move out of the parking area without backing onto a public street. This provision may be waived in the Town Center area.

(3) Tandem spaces shall be allowed in single family detached and paired dwellings.

(4) No two-way drive aisle shall be less than twenty feet where there is no adjacent parking, and no less than 24' where adjacent parking occurs. No one-way drive aisle shall be less than twelve feet where no parking occurs.

(5) All parking shall be designed to provide complete and through circulation wherever possible. Adequate turning radii and turn-arounds shall be provided for emergency vehicles and trash delivery trucks.

(6) Residential parking spaces should be located within two hundred feet of the use which they are intended to serve. For non-residential uses, this may be extended to four hundred feet. Designated employee parking may exceed this distance.

(7) For any structure which extends above a drive aisle, the minimum vertical clearance shall be fourteen feet.

(8) No “circular” or “club” style driveways shall be allowed within residential front yard setbacks. Pavement should be kept to a minimum.

(9) Hammerheads & turnarounds for loading shall be permitted if found acceptable by the Fire Department.

7.8 Surface of Parking Area

The following standards shall apply to all parking areas required by this Section:

(1) The flooring material for commercial parking structures and carports in all districts shall be concrete, asphalt or acceptable alternative as determined by the City Planner.

(2) All principle parking, loading spaces, and driveways shall be paved and maintained in good repair and safe condition. Porous paving and other permeable surfaces are encouraged, and may be approved by the City Planner. Areas which are primarily utilized for holiday and special event parking shall be graded, compacted and planted, however may be left unpaved.

(3) With the approval of the City Planner, paving of driveways within the Single Family areas may be substituted with a non-dusting gravel or similar.

(4) Except for single family and two family residences, all off-street parking and loading spaces shall be marked by stripes not less than four inches wide painted on the surface area, or by similar means as approved by the City Planner.

7.9 Lighting of the Parking Areas

Adequate parking lot lighting for security purposes shall be required and maintained to effectively illuminate the parking area of all developments, except for single-family and paired single family dwellings. Lighting shall be located and designed so as to minimize the direct glare of light shining onto adjacent property, streets, or into the sky above a horizontal plane passing through the luminary. Further discussion regarding the illumination of developments is included in Section 9.0, Lighting Standards & Guidelines.

7.10 Parking and Storage of Commercial Vehicles

For the purpose of this section, “commercial vehicle” shall mean a vehicle or trailer used or maintained for the transportation of persons for hire, compensation or profit, or used and maintained primarily for the transportation of property, including, but not limited to, tractors, vans, trailers, panel trucks, dump trucks, but excluding vans and pickups with wheel bases less than one-hundred and fifty inches.

(1) Parking or storing of commercial vehicles in residential districts for any length of time is prohibited, except that commercial vehicles may park for the purpose of making pickups and deliveries of materials or persons from or to any building or site.

(2) Commercial vehicles shall not be parked or stored on vacant or undeveloped property in any district unless associated with ongoing construction of the site.

7.11 Landscaping of Parking Areas

Landscaping of parking areas shall be as described in Section 5.0 Master Landscape Plan. Landscape of these areas is important to 1) improve the aesthetic of the lot, 2) create a more pedestrian scale to the lot, 3) reduce the amount of urban heat generation caused by large expanses of unshaded surface, and 4) provide areas which may be utilized for water run-off absorption.

- 1.0 *The Specific Plan as Planning Tool*
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- 7.0 *Parking Regulations*

- 8.0 *Sign Regulations*

- 9.0 *Community Lighting Regulations*
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- 11.0 *Design Review*
- 12.0 *Implementation*

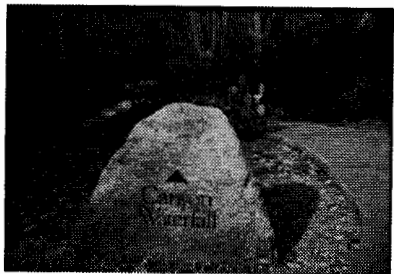
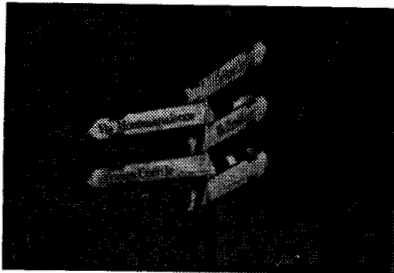


8.0 SIGN REGULATIONS

Signage within Porta Bella will be of the character typically found within the California towns with an earlier heritage. All signs within Porta Bella should reflect the character depicted in the community. Occasionally, the Design Review Board may permit more modern signage if its application would provide an artful contrast to the “New Heritage” architecture. The following are character guidelines which should be integral to the design and location of the sign.

- (A) The sign will fit contextually with its location on the site or building.
- (B) The sign will coexist with the design objectives of the surrounding area. The sign should not overpower.
- (C) The sign will reflect the quality and material character of the related structure.
- (D) The sign will incorporate creativity and artistry in its shape, materials, colors and placement.
- (E) The sign will be comprised primarily of materials which were utilized within the architectural period of the structure to which it will be attached.

All signs within Porta Bella shall be controlled by these sign regulations. The regulations will contribute to the character and value of Porta Bella and have been carefully planned to respond to the image and environmental quality objectives. Specific sign programs shall be submitted to the City Planner as a part of the development review and approval process.



8.1 Sign Types Permitted

Community Identification Sign
Major & Secondary Arterial Office/Commercial Signage
Commercial Village Identification Sign
Commercial Tenant Identification Wall Sign
Commercial Tenant Identification Window Sign
Commercial Tenant Identification Freestanding
Theater Marquee
Pedestrian Kiosks
Restaurant Sidewalk Menu Signs
Residential Identification Wall Sign
Residential Identification Freestanding Sign
Vehicle Directional Signs
Community Marketing Information Sign
Parcel Marketing Information Sign
Property Resale Sign
Special Event, Festival or Holiday Banners
Address Identification
National and State Flags (not for advertising purposes)

8.2 Prohibited Signs

Pole signs
Pylon signs
Roof signs
Gyrating or blinking signs
Inflatable signs
Billboards
Flags and streamers used for advertising
Window painting over 25% of window surface
Window signs over 25% of window surface
Interior illuminated plastic panel signs
Interior illuminated individual letters

8.3 Sign Regulations

All signs, regardless of type or permanency, are affected by nine criteria for design and execution. These guidelines describe the variable for each sign type. These criteria are:

- (1) Sign Type: Name reference
- (2) Function: The purpose of the sign
- (3) Placement: The position of the sign
- (4) Materials: The range of materials and colors from which the sign can be built
- (5) Landscaping: The treatment of landscaping around the sign
- (6) Illumination: The method of lighting or mounting
- (7) Typography: Readability requirements
- (8) Size: The area of the sign
- (9) Quantity: The number of signs allowable

8.4 Community Identification Sign

Function:

Identify the area entrances to Porta Bella , primarily to vehicular traffic. Display name, logo and introduce Porta Bella 's overall sign system's form, material and color palette. Create a visual image unique to Porta Bella. May be in a landmark form other than typical signage, such as landscape or sculpture.

Placement:

Perpendicular, parallel or flanking entrances at the Specific Plan's major entrance areas. Locations of these identity may occur, but not be necessarily limited to, the intersections of 1) Santa Clarita Parkway and Via Porta Bella, 2) Via Princessa and Main Street, 3) Magic Mountain Parkway and San Fernando Road, and 4) Via Princessa and Santa Clarita Parkway.

Materials:

Material palette must be compatible with, reflect and unify Porta Bella 's character.

Landscaping:

Landscaping palette must be consistent with, reflect and unify the Porta Bella community.

Illumination:

Ambient, ground or halo illumination

Typography:

Letter size shall be no greater than 18 inches in height.

Size:

Vertical height of sign background treatment not to exceed 10' 0".
Sign copy area not to exceed 40 square feet.

Quantity:

Five (5) maximum.

8.5 Major & Secondary Arterial Commercial Signage



Function:

Identify entrances into the business sites oriented to the external arterial streets, namely the Neighborhood Commercial Center in the South District (Santa Clarita Parkway & Via Princessa), and the Soledad District Commercial, Office and Business Parks.

Placement:

Perpendicular, parallel or flanking entrances for internal road system.

Materials:

Material palette must be compatible with, reflect and unify the Specific Plan's traditional, character.

Landscaping:

Landscaping palette must be consistent with, reflect and unify the landscape area in which the sign is located.

Illumination:

Ambient, ground or hallow illumination of individual letters/logo

Typography:

Letter size shall be no greater than 24 inches in height.

Size:

Vertical height of sign background treatment shall not exceed 5'0" with sign copy area not to exceed 50 square feet.

Quantity:

Four (4) Maximum. Location of these signs is recommended to occur at 1) Soledad Canyon Road & Soledad District Commercial area, 2) Soledad Canyon Road & Soledad District Office area, 3) Santa Clarita Parkway and Soledad District Business Park area, and 4) Santa Clarita Parkway and Via Princessa for the Neighborhood Commercial area.

8.6 Town Center Identification Sign

Function:

Identify entrances into the Town Center Business District. Display area name, logo and reinforce the Town Center's sign system theme.

Placement:

Perpendicular, parallel or flanking entrance at internal road system. Sign may also extend over street or be located within median or roundabout of streets entering the Town Center.

Materials:

Material palette must be compatible with, reflect and unify the traditional Town Center character.

Landscaping:

Landscaping palette, if applicable, must be consistent with, and reflect the traditional, character of Porta Bella .

Illumination:

Ambient, ground or hallow illumination of individual letters/logo

Typography:

Letter size shall be no greater than 12 inches in height

Size:

Size and shape of this sign to be approved by the City Planner based upon the sign's contribution to the character of the Town Center, and its relationship relative to the streets and view corridors.

Quantity:

Two (2) maximum to be located at the main entrances to the Town Center. Recommended location of these signs is at or near the intersection of Magic Mountain Parkway and Main Street, and at the intersection of Rio Vista and Magic Mountain Parkway.

8.7 Commercial Tenant Identification Wall Signs

Function:

Display tenant's name and/or logo on building wall to specifically identify business location. Projecting blade signs are permitted within this category.



Placement:

On wall preferably near the tenant's entrance. Sign should appropriately identify tenant's scale on the site.

Materials:

Material palette must be consistent with and reflect and unify particular architectural design of the building and urban character.

Landscaping:

Landscape material may be incorporated into wall mounted signage, via flowerboxes and similar devices.

Illumination:

Ambient, ground, directed or hallow illumination of individual letters/logo

Typography:

Town Center District, including the Office Park:

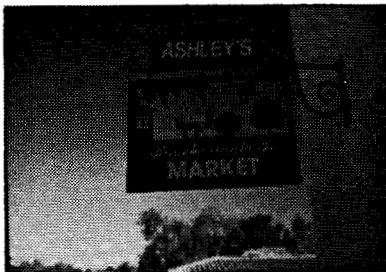
Letter height shall not exceed 12" in height.

Soledad District:

Letter height shall not exceed 36" in height. (Major tenants 60")

Neighborhood Commercial Area:

Letter height shall not exceed 36" in height. (Major tenants 60")



Size:

Total sign area per tenant cannot exceed one (1) square foot per lineal foot of tenant frontage, not to exceed 80% of total storefront. Sign cannot be located above building roof line. Combined sign copy cannot exceed 30 square feet in the Town Center District, 60 square feet in all other districts. All area of the sign including background and the general area of the lettering shall be considered part of the total sign square footage.

Quantity:

One (1) wall sign for each business or tenant. Corner lots tenants: one wall sign for each exterior wall, up to four.

8.8 *Commercial Tenant Identification Window Signs*



Function:

Display tenant's name and/or logo on building window to specifically identify business location. This does not include painting for advertising or special events.

Placement:

On window preferably on or near the tenant's entrance. Sign should appropriately identify tenant's scale on the site. Second and third story signage is permitted for uses which occur above the first floor.

Materials:

"Permanent" Paint or glass etching.

Landscaping:

Landscape material may be incorporated into window signage, via flowerboxes and similar devices.

Illumination:

Ambient or interior illumination.

Typography:

Lettering shall not exceed 12" in height.

Size:

Total sign area 25% of the window in which it is placed.

Quantity:

One (1) window sign for each business or tenant. Corner lots tenants: one window sign for each exterior wall, up to four.

8.9 Commercial Tenant Identification Freestanding



Function:

Display tenant's name on freestanding sign at tenant's entrance. Used only by tenants who have clear and substantial parcel ownership, i.e.: single user parcel. Not allowed in the Town Center District.



Placement:

Perpendicular, parallels or flanking entrance at internal road system.

Materials:

Material palette must be consistent with and reflect the traditional character of Porta Bella .

Landscaping:

Landscaping palette must be consistent with, reflect and unify the landscape area in which the sign is located.

Illumination:

Ambient, ground or hallow illumination of individual letters/logo

Typography:

Letter size shall not exceed 36".

Size:

Vertical height of sign background treatment shall not exceed four (4) feet with sign copy area not to exceed 24 square feet.

Quantity:

One (1) per parcel unless otherwise permitted by the City Planner.

8.10 Theater Marquee Signage

Function:

To display Theater current & coming attractions & events. Should be designed reminiscent of the “movie palace” marquees which typify classic theaters.

Placement:

Relationship to pedestrian access to theater. This sign may extend above the first floor.

Materials:

Materials shall be as necessary to accomplish the classic movie marquee character, such as plastic and metal.

Landscaping:

N/A

Illumination:

Internal Illumination, neon lighting permitted.

Typography:

Letter size shall not exceed 48”.

Size:

No minimum. To be determined through Design Review.

Quantity:

One (1) per theater complex unless otherwise permitted by the City Planner.

8.11 Restaurant Sidewalk Menu Signs

Function:

To inform pedestrian traffic adjacent to a restaurant of the restaurant's menu. This sign is required for all restaurants.

Placement:

Wall mounted. The sign should be located near the primary entrance to the restaurant. In addition to this sign, a simple "A" frame sandwich board sign is permitted for exterior use along sidewalk areas.

Materials:

The sign should be of a simple artistry in design and materials which is complimentary to the character of the restaurant and the neighborhood it is within. Menu frames and chalkboards are recommended.

Landscaping:

N/A

Illumination:

Ambient or indirect lighting.

Typography:

This sign is pedestrian oriented and shall be readable from no further than ten (10) feet.

Size:

Four (4) square feet maximum. Double sided signage is permitted with "A" frame sign.

Quantity:

One (1) per restaurant entrance.

8.12 Residential Identification Wall Sign

Function:

To identify multifamily residential structures. This signage type is to be utilized in lieu of, and not in conjunction with, freestanding residential identification signs. This sign is not intended for advertisement. No telephone numbers or similar shall be permitted on this sign.



Placement:

On wall near primary entrance, or perimeter wall or gate if applicable.



Materials:

Material palette must be consistent with and reflect traditional character of the community. Simplicity and restraint in materials is encouraged.

Landscaping:

N/A

Illumination:

Ambient, ground or hollow illumination of individual letters/logo

Typography:

Letter size shall not exceed 18" in height.

Size:

Twenty-five square foot maximum.

Quantity:

Not to exceed one (1) per each primary entrance or street frontage.

8.13 Residential Identification Freestanding Sign

Function:

To identify multifamily residential structures. This signage type is to be utilized in lieu of, and not in conjunction with, wall mounted residential identification signs. This sign is not intended for advertisement. No telephone numbers or similar shall be permitted on this sign.

Placement:

Perpendicular, parallel or flanking entrance to multifamily community. This sign may be located at a pedestrian or vehicular entrance.

Materials:

Material palette must be consistent with and reflect traditional and character of the community. Simplicity and restraint in materials is encouraged.

Landscaping:

Landscape palette must be consistent with and reflect the traditional character of the community.

Illumination:

Ambient, ground or hollow illumination of individual letters/logo

Typography:

Letter size shall not exceed 18" in height.

Size:

Vertical height of sign background treatment shall not exceed 4'0".
with sign copy area not to exceed 25 square feet.

Quantity:

Not to exceed one (1) per street frontage,

Temporary Signs

8.14 Community Marketing Information Sign



Function:

Identify the Porta Bella area's boundary, community name and community marketing information, ie., Porta Bella Community 960 acre Master Planned Environment.

Placement:

Perpendicular or parallel to Porta Bella Areas external road system. This sign may be double faced.

Materials:

Material palette must be compatible with and reflect the traditional character of the community.

Landscaping:

Landscaping palette must be consistent with, reflect and unify the desired landscape character of the neighborhood the sign is within.

Illumination:

Ambient, ground or hallow illumination of individual letters/logo

Typography:

Letter size shall not exceed 24" in height.

Size:

Vertical height of sign background treatment not to exceed 15'0".

Sign copy area not to exceed 100 square feet.

Quantity:

Five (5) maximum.

8.15 Parcel Marketing Information Sign

Function:

Identify parcel boundary, display parcel name and marketing information, e.g.,

"Oak Estates Single Family Homes from \$150,000."

Placement:

Perpendicular or parallel to the Porta Bella area's internal road system. This sign may be double faced.

Materials:

Material palette must be compatible with and reflect the traditional, character of the community.

Landscaping:

Ambient, ground or hallow illumination of individual letters/logo

Illumination:

Ambient or ground illuminated.

Typography:

Letter size shall not exceed 24" in height.

Size:

Vertical height of sign background treatment not to exceed 10'.

Sign copy area not to exceed 32 square feet.

Quantity:

One (1) per parcel.

8.16 Property Owner's Resale Signage

Function:

Identify individual property for sale. e.g.

"For Sale - A1 Real Estate - Call Sam 555-1212"

Placement:

Located within property which is being sold.

Materials:

Simple materials which are normally associated with real estate sales signage of established firms.

Landscaping:

N/A

Illumination:

None

Typography:

Letter size shall not exceed 8" in height.

Size:

Not to exceed four square feet in background signage, nor four feet in height.

Quantity:

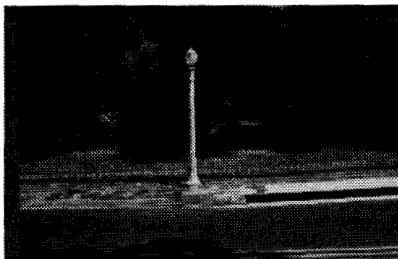
One (1) per parcel.

- 1.0 The Specific Plan as Planning Tool*
- 2.0 Community Context & Integration*
- 3.0 Land Use Master Plan*
- 4.0 Transportation & Circulation*
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- 8.0 Sign Regulations*

- 9.0 Community Lighting Regulations*

- 10.0 Infrastructure & Public Utilities*
- 11.0 Design Review*
- 12.0 Implementation*





9.0 COMMUNITY LIGHTING REGULATIONS

The lighting of a community plays an integral component to the elements of safety and aesthetics. As lighting is a major character feature of an area at night, it follows that care should be taken in the selection of fixtures, their placement, and their luminous intensity.

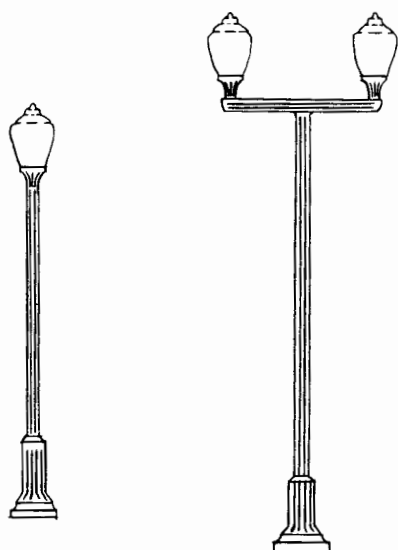
Many different light sources are available such as mercury vapor, high or low pressure sodium, and metal halide. Each light source produced varying degrees of light quality, color rendition, and energy efficiency. Within Porta Bella, the predominant light source shall be a high pressure sodium lamp, for its energy efficiency and "incandescent" light rendition.

The following sections describe lighting considerations and guidelines for each land use area.

9.2 Single Family Residential

Lighting within the single family neighborhoods should be located within the public rights-of-way, and be of low intensity. Lighting standards throughout the residential communities shall be from the same family of fixtures. The character of the lighting fixture shall be appropriate for the architecture.

Lighting fixtures within the single family residential areas may not exceed 15' feet in height for single fixture standards, and 20' feet for double fixture standards located within medians. The spacing and illumination intensity of the fixtures shall be determined upon design of plans associated with street development, including landscape plans.



9.3 Multi-family Residential

Fixtures to the exterior of multi-family residential areas shall be the same as those found within the single family neighborhoods. On-site, fixtures shall be located and designed so as to minimize the direct glare of light shining onto adjacent property, streets, or into the sky above a horizontal plane passing through the luminary. Lighting fixtures within the interior areas of multifamily areas may not exceed 12' feet in height. Care shall be taken in the on-site lighting design to avoid casting light into the dwelling units.

9.4 Commercial, Office Park & Institutional

The purpose of lighting in these land use areas aesthetic, safety and function. Street lamps throughout the complexes must reflect architecturally compatible design, and be of the same family. Maximum height of fixtures within these land use areas is 40 feet. On-site fixtures shall be located and designed as to minimize the direct glare of light shining onto adjacent property, streets, or into the sky above a horizontal plane passing through the luminary.

9.5 Town Center District

The purpose of lighting in these land use areas aesthetic, safety and function. Street lamps throughout the complexes must reflect architecturally compatible design, and be of the same family. Height of fixtures within this land use area ranges from 15 to 25 feet. On-site fixtures shall be located and designed as to minimize the direct glare of light shining onto adjacent property, streets, or into the sky above a horizontal plane passing through the luminary.

9.6 Business Park

Within the Business Park land use area, lighting fixtures must be from the same family. Lighting fixtures may not exceed 30 feet in height for non-loading or storage areas, and not exceed 45 feet for loading and storage areas. On-site fixtures shall be located and designed as to minimize the direct glare of light shining onto adjacent property, streets, or into the sky above a horizontal plane passing through the luminary.

9.7 Parks

Within the park areas, lighting will be provided as determined necessary by the City Parks and Recreation department. All lighting fixtures within park areas shall be located so as to minimize light and glare intrusion into adjacent residential areas. Lighting shields shall be utilized for night lighting fixtures of ball fields in the community park. Pathway lighting of pedestrian scale such as low height standards or bollards is encouraged.

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- 9.0 Community Lighting Regulations*

- 10.0 Infrastructure & Public Utilities**

- 11.0 Design Review*
- 12.0 Implementation*



10.0 INFRASTRUCTURE & PUBLIC UTILITIES

All conventional utilities will be provided in the Specific Plan area including water, sewer, drainage, electrical, gas and franchise utilities such as cable television. The following paragraphs and diagrams describe in a general manner how these systems will be organized and how they will function. More precise plans will be prepared as development progresses.

10.1 Water Service

Water service within the project area will be provided by the Santa Clarita Water Company (SCWC), a City Council approved purveyor, a mutual water company, or by the land owner. As noted within the Santa Clarita General Plan, the SCWC is currently the largest water purveyor in the Santa Clarita Valley, with 18,000 connections, delivering 18,845 acre feet per year.

This Specific Plan proposal includes the construction of two new three million gallon tanks on-site to provide water storage and service to the Specific Plan area. These tanks will be supplied by two off-site lift stations.

Water lines on-site will range from sixteen to eight inches, depending upon their function within the system. A pressure reduction station will enable this higher elevation water system to be connected with the existing lower elevation system along Soledad Canyon Road.

The Water Facilities Plan illustrates the proposed water facilities on-site. Eight inch water lines are not depicted on the Plan, however they will serve the individual developments.

10.2 Sewer Facilities Plan

Los Angeles County Sanitation District 26 serves the area, with existing trunk lines located in Soledad Canyon and San Fernando Roads. The project site will connect with the fifteen inch trunk line in Soledad Canyon at its intersection with the Santa Clarita Parkway. Two additional connections will be made with the existing sewer trunk line located near San Fernando Road. Due to the topography of the site, lift stations will be necessary to service two relatively small service areas located outside of the gravity drainage areas of the sewers system.

Sewage from the site may be treated by both the District 26 and the District 32 plants, which are interconnected. According to the City of Santa Clarita General Plan (1991), the two plants are operating at 85.6 percent of capacity.

The Sewer Facilities Plan illustrates the sewer system for the project area.

10.3 Storm Drain Facilities Plan

The County of Los Angeles Flood Control District maintains and regulates the storm drain facilities for the Santa Clarita Valley. According to the Santa Clarita General Plan, the County has set two general drainage system requirements for development, (1) all development must be free of flood hazards; and (2) development cannot worsen the flood hazard downstream. The storm drain system for the Specific Plan area will meet these requirements.

Within this Specific Plan area, developments are strongly encouraged to incorporate natural drainage techniques. Natural drainage techniques lessen demand on downstream drainage systems, cost less to maintain, retain the aesthetic and enable the ground to absorb and retain water. Some examples of the natural drainage techniques are as follows:

- Incorporate permeable surfaces (such as decomposed granite in place of asphalt, concrete bands in place of broad concrete driveways, etc.) into design where non-permeable surfaces have been traditionally utilized, such as pathways, driveways and parking lots. This technique also diminishes rainwater pollution by eliminating the petroleum based asphalt surface.
- Utilization of drainage ditches and swales instead of curb and gutter on streets.
- Contouring the grade away from streets and into common shallow swales running through common areas behind houses. These shallow swales can overflow into the drainage system, however the overflow will be much less than if allowed to directly empty into the system.
- Utilization of natural “check dams” in the natural drainage and riparian areas, to prevent surges of water downstream. These check dams will slow the rate and volume of water to downstream systems.

To ensure the LACFCD’s requirement that water run-off does not worsen the drainage system downstream, a water detention/percolation basin may be developed, if necessary, on the project site. This basin is currently proposed for the southwestern corner of the project site, and has a maximum capacity of 8 acre feet. The purpose of the detention/percolation basin is to capture peak water runoff and hold it on-site. This rainwater would be generally allowed to seep back into the ground. Gradually, the remaining water would be metered into the Santa downstream storm drain system to flow into the Santa Clara River system.

The Storm Drain Facilities Plan illustrates the storm drain system on-site. Developments which utilize natural drainage techniques which are reviewed and approved for adequacy, may modify the master drainage plan as it effects their portion of the system as necessary.

10.4 Electrical Facilities

Electrical service is provided by the Southern California Edison Company. Electrical facilities will be located within the street rights-of-way, and no overhead utilities will be permitted.

10.5 Natural Gas Facilities

Natural Gas is supplied to the Specific Plan area by the Southern California Gas Company. Natural gas facilities will be located within the street rights-of-way.

10.6 Telephone Facilities

Telephone service is provided by Pacific Bell. Telephone facilities will be located within the street rights-of-way. No overhead telephone facilities will be permitted.

10.7 Cable Television Facilities

Cable television is provided by Cablevision and Kings Videocable Company. Cable television facilities will be located within public rights-of-way.

10.8 Public Facilities

Public facilities such as the Civic center, library, day care center, community service buildings, amphitheater, performing arts center and museum will be located adjacent the project site on the civic center property.

10.9 Schools

Porta Bella is located within the Hart Unified School district for the High School, and the Saugus School district for the elementary school.

10.10 Fire Protection

Fire protection within the Specific Plan area will be provided by the Los Angeles County Fire Department. According to the City of Santa Clarita General Plan, Santa Clarita is currently served by four fire stations with six engines, two paramedic squads and one ladder truck. Approximately 100 fire fighters are assigned, with about 33 on duty at a time. An intermediate size site is available within the project area for a fire station.

10.11 Police Protection

Police protection within the Specific Plan area will be provided by the Los Angeles County Sheriff's Department, which operates under contract with the City of Santa Clarita. According to the City of Santa Clarita General Plan, the city is home to one police station with 107 sworn officers.

10.12 Solid Waste Collection

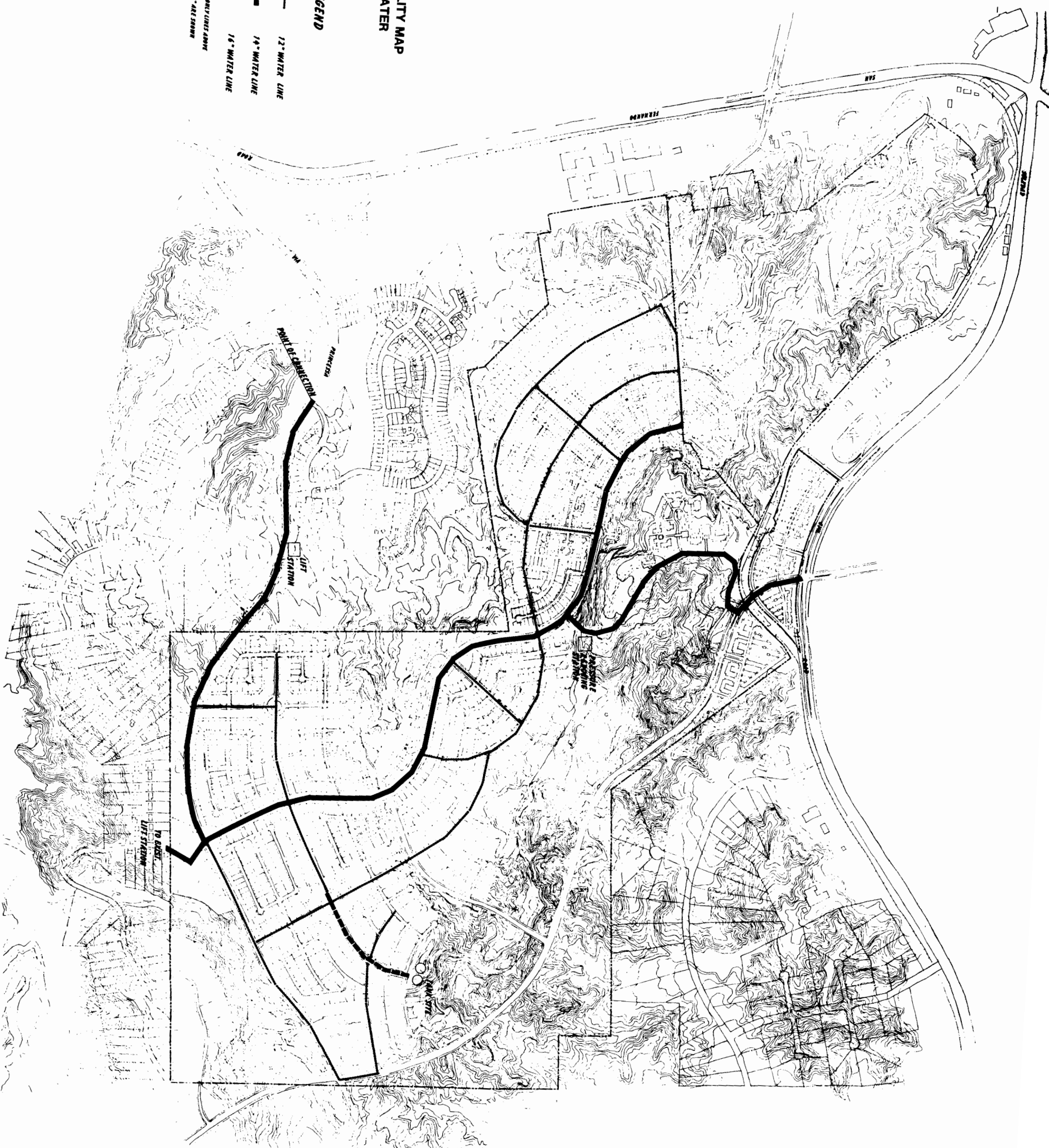
Solid waste collection is provided by a private hauler for refuse and recycling by contract with the City.

UTILITY MAP
WATER

LEGEND

- 12" WATER LINE
- 14" WATER LINE
- 16" WATER LINE

NOTE: ONLY LINES ABOVE
3" ARE SHOWN



LEGEND

PROPOSED STORM DRAIN

EXIST. STORM DRAIN

PROPOSED DETENTION BASIN

DIRECTION OF FLOW

NOTE: ONLY STORM DRAINAGE

PROPOSED STORM DRAIN	EXIST. STORM DRAIN

EXIST. STORM DRAIN

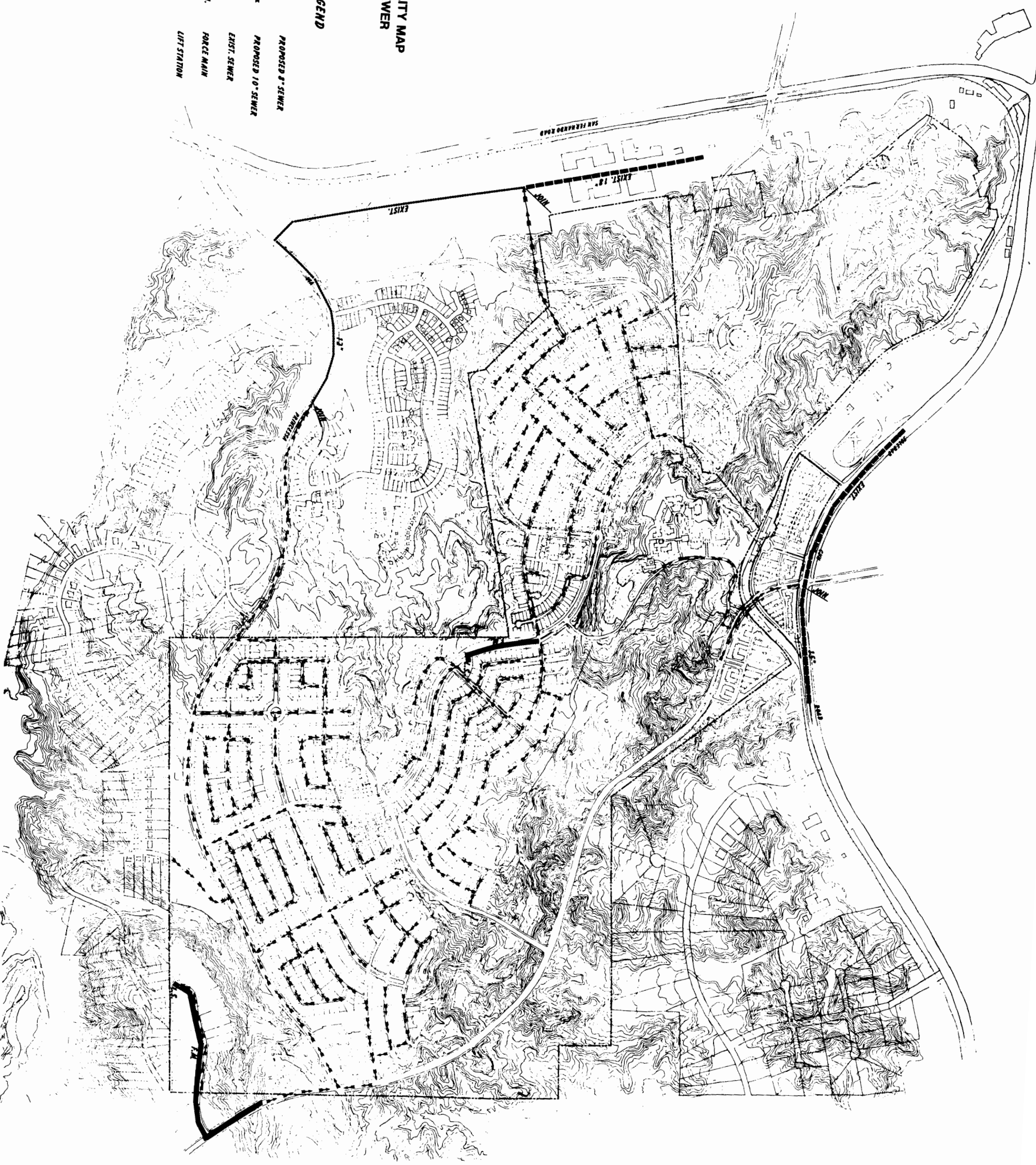
DIRECTION OF FLOW

ABOVE 18" ARE SNOWN

UTILITY MAP
SEWER

LEGEND

- PROPOSED 8" SEWER
- PROPOSED 10" SEWER
- EXIST. SEWER
- FORCE MAIN
- LIFT STATION
- L.S.



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11.0 DESIGN REVIEW

The purpose of this section is to establish a design review process for development proposals in order to ensure that development with the Specific Plan area occurs in a manner which enhances the character and quality of the community and that the scale, spatial relationships and architectural treatment of structures including materials, colors, and design, visually contribute to the overall character of the Specific Plan area. Design review also applies to the ancillary elements of projects such as signs and landscaping in order to ensure that the overall development maintains the same integrity of design as approved for the primary structure(s). All subsequent development must be reviewed and approved by the Master Developer prior to City Review.

11.1 Design Review Required

No building permit shall be issued for, and no person shall commence to use, any structure including signs, until that structure and its accompanying development has received design review approval pursuant to the provisions of this Section.

11.2 Design Review Application

Application for a design review shall be filed with the Planning Department on a form prescribed by the City Planner and shall include, but not be limited to, the following data and maps:

(1) Name and address of the applicant

- (2) A statement that the applicant is the owner or the authorized agent of the owner of the property subject to the Design Review request. This provision shall not apply to proposed public utility right-of-way.
- (3) The address and legal description of the property (Assessor's Parcel Number).
- (4) A list of all owners of property located within three-hundred feet of the exterior boundaries of the subject property; the list shall be keyed to a map showing the location of these properties.
- (5) Site plans, scaled and fully dimensioned, indicating the location of all property lines, type and location of all buildings and structures, entrances, parking, signs, walls, and preliminary grading information.
- (6) Landscape plans that show the location of landscape areas and proposed planting treatments; a list of proposed plant materials shall be provided with the proposed sizes at the time of installation;
- (7) Location of existing improvements on adjacent properties and public right-of-way within one-hundred feet of the site boundaries;
- (8) Elevations and floor plans for all buildings, except where multi-phased development is anticipated and precise elevations and floor plans have not yet been established.
- (9) Material/color board;
- (10) A statistical inventory of the project including size of site, number of parking spaces, and coverage information.
- (11) A statement from the applicant describing the project and its relationship to surrounding development;

(12) The City Planner may require additional information or plans, if necessary, to determine whether a design review request should be granted or denied. The Director may also authorize omission of any plans and drawings required by this action if the finds they are not necessary.

(13) An signed statement of approval from an agent of the Master Developer approving the plans as submitted to the City for review.

11.3 Design Review Hearing

With the exception of a minor design review as provided hearing, the Planning Commission shall hold at least one hearing on each application, for a Design Review. The hearing shall be set and notice given as prescribed in the Zoning ordinance. At this hearing, the Commission shall review the application and drawings submitted and may receive comments from the public concerning the proposed development and the manner it will effect the subject property and surrounding properties.

11.4 Scope of Design Review

In order to achieve the purpose of this Chapter the following design concepts shall be paramount in the consideration of any design review approval:

(1) The scale and spatial relationship of all structures should be appropriate to the site and to surrounding developments.

(2) The design concept should complement the quality of existing development and create a visually pleasing relationship between the proposed and existing projects.

(3) Exterior materials should demonstrate a concern for quality and originality. The use of a particular material should, as a rule, exemplify the special characteristic so the product or be demonstrative of its specific application.

(4) Electrical, mechanical and other equipment should be screened; such screening should be an integral aspect of the project design.

(5) Project should respect neighboring property's privacy, quite, function, or views; service areas including, but not limited to, openings, docks and equipment should be located in such a way as not to create a nuisance for adjoining properties.

(6) Accessory structures, fences and walls should be harmonious with the design of the principal structures and should employ compatible building materials.

(7) Circulation patterns and parking areas should be efficient and should not adversely affect traffic on adjoining right-of-ways. Adequate parking should be provided and maneuvering areas for trucks and for drive-through facilities should generally be contained on-site; within the Town Center District, on-street parking and maneuvering may be permitted consistent with the intent of that District.

(8) Landscaping should be spread over the entire site and be visually interesting all year. Landscaping should screen parking and storage areas and shade parking lots.

(9) The project should consider solar orientation and provide for

(10) Signs should be harmonious with the overall design concept of the project; materials and colors should be consistent with those used for the principal structure.

11.5 Actions of the Planning Commission

The Planning Commission may approve the project as submitted or may recommend changes in the design or the application may be denied. A design review approval may be subject to such conditions as the Planning Commission may prescribe. Conditions may relate to, but are not limited to, site design, including relationship to surrounding uses, functions of buildings or portions of site, setback coverage, amount of opens space, infrastructure improvements; shape, height, and bulk of structures; distances between buildings; architectural design of structure including exterior materials, colors and textures; location of ancillary equipment; locations of points of ingress and egress; location, amount, and design of parking areas; locations and efficiency of truck maneuvering and loading areas; landscaping, including location and general design, and height; lighting; walls and fences, including location, height, and materials; project grading; and project phasing.

11.6 Findings

The Planning Commission shall make the following findings before approving a project with the Specific Plan area:

(1) The project, as approved, will comply with the goals and objectives of the Santa Clarita General Plan and this Specific Plan document.

(2) The project complies with the design directives contained in the Specific Plan section 11.0 Design Review.

(3) Conditions and safeguards pursuant to Section 11.6, "Action of the Planning Commission," including guarantees and evidence of compliance with conditions, have been incorporated into the approval of the subject project to ensure development of the property in accordance with the objectives of this Specific Plan and the area in which the site is located;

(4) All finding and actions taken by the Planning Commission are final except if said findings are appealed to the City Council;

(5) The Planning Commission shall establish its findings and actions by resolution.

11.7 Action by the City Council

A report of the actions of the Planning Commission shall be transmitted to the City Council after the Planning Commission hearing. If approved, the City Council shall receive and file said report. Appeals of any decision by the Planning Commission may be made to the City Council. However, the decision of the Planning Commission shall be final ten days from the date of the decision unless an appeal has been filed with the City Council pursuant to the provisions of the Zoning Ordinance.

11.8 Minor Design Review

Minor Design Review is intended for projects which are significantly smaller in scale than those for which a normal design review procedure is appropriate. The Planning Commission may approve, deny, or conditionally approve the following project subject to the provisions of this sections:

- (1) Additions to commercial and industrial structures or permitted accessory structure in excess of 500 square feet;
- (2) Paired not involving more than a total of four units;

The City Planner or their designed may approve, deny, or conditionally approve the following projects subject to the provisions of this section;

- (1) Single-family detached dwellings, including in-fill and development in approved subdivisions;
- (2) Additions to commercial and industrial structures or the construction of accessory structures of 500 square feet or less in area;
- (3) Additions to residential dwellings;
- (4) Alterations to existing structures which substantially change the appearance of the structure even though the floor area may not be affected (i.e. changes in the shape of exterior materials);

(5) The following residential accessory structures: Unenclosed structures such as carports, gazebos, and patio covers (both trellis-type and solid) as well as enclosed structures containing less than 600 square feet of floor area such as sheds, cabanas, children's play-houses, and work shops;

(6) Reconstruction of commercial structures which has been destroyed or substantially damaged, provided said structures were legally in existence and said reconstruction returns the structure to its original size and appearance;

(7) Signs pursuant to Section 9.0, "Sign Regulations;"

(8) Modifications of site plans for building movement within a range of 0-15 feet;

(9) Modification of setbacks within a range of 0-5 feet;

(10) Substitutions of materials which are in conformance with the original design approval;

The Planning Commission or the City Planner shall be empowered to approve a minor design review as the project was submitted or may require changes in the design or may deny the project and instruct the Building Division to issue no building permits for the project.

The Planning Commission or the City Planner shall only approve a minor design review when the findings required by Section 11.6, "Findings of the Planning Commission," have been made.

The person submitting the project for minor design review may appeal the City Planner decision and/or conditions to the Planning Commission; provide that said appeal be submitted to the City Planner within 10 working days. The Planning Commission shall hear the appeal in the same manner prescribed for a Design Review pursuant to the provisions of this Chapter. Planning Commission decisions may be appealed to the City Council. No construction related to the minor design review shall commence prior to Planning Commission or City Planner action/approval.

The City Planner may also elect to submit any minor design review to the Planning Commission when, in the opinion of the City Planner, any of the following may be applicable:

- (1) The project may significantly affect properties other than the applicant's and additional public notification and input is warranted;
- (2) The project requires an environmental impact report (EIR) or a negative declaration, according to the California Environmental Quality Act (CEQA);
- (3) The project may be generally controversial within the community;
- (4) The project will need a variance.

Unless specific provisions are specified for minor design review, for the general administrative purposes of this Chapter, the terms design review and minor design review shall be interchangeable.

11.9 Lapse of Design Review Approval

A design review approval shall lapse and shall become void one year following the date on which the design review became effective, unless prior to the expiration of one year, a building permit related to the design review is issued and construction commenced and diligently pursued toward completion. Notwithstanding conditions to the contrary, a design review granted pursuant to this Section shall run with the land for this one year period and shall continue to be valid upon a change of ownership of the site which was the subject of the design review application.

11.10 Modification of Design Review Approval

Any alteration or expansion of a project for which there has been a design review approval as well as all applications for modification or other change in the conditions of approval of a design review shall be reviewed according to the provisions of this Chapter in a similar manner as a new application.

11.11 Re-application

Following the denial of a design review application or the revocation of a design review approval, no application for a design review of the same or substantially the same design concept on the same or substantially the same site, shall be filed within one year from the date of denial or revocation of the design review.

11.12 Occupancy

No structure which has received a design review or minor design review approval shall be occupied or used in any manner or receive a Certificate of Occupancy until the City Planner has determined that all conditions of approval have been satisfied.

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- 11.0 *Design Review***

- 12.0 Implementation**

12.0 IMPLEMENTATION

To initiate the implementation of the Specific Plan the following actions must be taken by the Santa Clarita City Council:

- Certify the Environmental Impact Report as adequate and complete.
- Approve the Specific Plan as a non-conditional Specific Plan

12.1 Specific Plan Modifications

Minor modification to the approved Specific Plan will be allowed at the discretion of the Community Development Director. Modifications to the Specific Plan must be consistent with the purpose and intent of the originally approved Specific Plan. All modifications or amendments to the approved Specific Plan, other than minor changes, will be processed as a Specific Plan Amendment and are subject to the Specific Plan approval procedures described in the Santa Clarita Zoning Ordinance.

The following modifications constitute minor changes to the approved Specific Plan:

Planning Unit Size

The gross acreage of a residential or commercial area (including manufactured slopes, and collector, local and private streets as applicable) may vary from the acreage specified in the Specific Plan as long as the total number of units for the Community is not exceeded and the density for a parcel is consistent with the requirements of that area.

Roadways

Minor changes in roadway alignments are allowed, provided such changes are consistent with the streetscape concept for roads.

Minor changes are also allowed as a result of more precise design & engineering as well as changes in land use patterns.

Density Transfer Density transfers of up to 10% shall be allowed between development areas. Owners of all areas subject to the density transfer shall be part of the application, or shall submit a letter of agreement to the conditions of the transfer.

12.2 Future Development Proposals

The following administrative standards apply to the implementation of future development applications (i.e. plot plans, tract maps, or parcel maps) for projects within the Specific Plan area.

- Future tentative or parcel maps and site plan review documents will be consistent with the Specific Plan
- Final subdivision or parcel maps will be in substantial conformance with the approved tentative or parcel map as well as the approved site plan documents.
- Building permits for dwelling units will be issued when a final subdivision map has been recorded. Permits may be issued for model units prior to the final map recordation subject to the requirements of the City.

12.3 Phasing

The phasing program may be modified, provided the objectives of the program continue to be met, and provided that all infrastructure including, but not limited to roads, sewer facilities, water supply, and drainage facilities are available to serve the proposed development. Deviations from the phasing plan will be subject to the approval of the City Planner, and if deemed to be a significant change, the Planning Commission. Such changes may not necessarily be considered a Specific Plan amendment, however.

12.4 Financing of Major Specific Plan Improvements

Various techniques are available for the financing of the improvements associated with the Specific Plan development. The most appropriate financing mechanism for each particular improvement will involve a multi-step process. First, the City and the developer will agree on the various options from which financing will be chosen. Once the City and developer agree on which options are most viable, these options will be approved as part of a development agreement. At this time, the exact financing method need not be specified, however, the City and the developer must mutually agree to the alternatives.

Some of the most viable funding mechanisms are listed below:

Legislative funding mechanisms

- Bridge and Thoroughfare District
- State Transportation Dept. (Rail)
- Redevelopment programs
- Impact fees
- Mello-Roos district
- Other form of assessment districts
- Facilities benefit assessment
- Land reservation, dedication, deeding in fee, or easements
- Hook-up charges
- Reimbursement agreements

12.5 Developer Funding

In many cases, certain facilities are tied directly to individual projects. In these cases, the developer will pay the cost of the facility. Water, sewer and drainage facilities are examples of facilities that are normally required concurrent with development.

Examples of developer funding

- Conventional subdivision financing
- Turnkey construction
- Construction loans
- Equity financing

12.6 Maintenance

Maintenance responsibilities within Porta Bella will be divided between the individual property owner's, the Master Homeowner's Association, a landscape and lighting district and the City of Santa Clarita.

Covenants, Conditions and Restrictions (CC&R's) will be recorded for the plan area to further ensure the maintenance of facilities and landscaping.

Streets

Each of the street rights-of-way identified as public within the plan will be dedicated to and maintained by the City of Santa Clarita in accordance with established City policies. Streets identified as private shall be maintained by the Master Homeowner's Association.

Landscape and Lighting District

A master landscape and lighting district as part of the Master Homeowner's Association will be responsible for maintaining parkway landscaping, including landscape easements adjacent thereto, and project identity signs and monuments. District maintenance areas will be established for the maintenance of street lighting and drainage facilities within the subject area not maintained by the City or other Districts.

Drainage Facilities

Permanent drainage improvements within Porta Bella will be constructed within basic rights-of-way, and dedicated to either the City or appropriate District for maintenance.

Where it is necessary to construct drainage improvements outside of public rights-of-way, drainage easements will be dedicated to the City or appropriate District. Upon dedication, the City or District will assume responsibility for maintenance of underground facilities only; maintenance responsibility for surface improvements within drainage easements will not be transferred.

Drainage facilities on private property in absence of an easement will be considered to be private drains. Maintenance of such private drains will be the responsibility of the landowner or the association charged with the general maintenance of the landscaping and other common improvements of the area in question.

Water and Sewer

The Santa Clarita Water Company, the County of Los Angeles District 26, or other council approved water company or purveyor will assume responsibility for the maintenance and monitoring of water and sewer facilities to be constructed within the public rights-of-way and easement areas containing public facilities. The City of Santa Clarita will monitor the construction of water and sewer facilities located on private property within Porta Bella.

Where it is necessary to construct water or sewer improvements outside of public rights-of-way, easements will be dedicated to the City of Santa Clarita or appropriate District. Upon dedication, the City or District will assume responsibility for maintenance of the underground facilities only; maintenance of surface improvements within easement areas, other than those facilities for which the easement was specifically granted, will not be transferred.

Water and sewer facilities located on private property in the absence of an easement will be considered to be private facilities. Maintenance of such private facilities will be the responsibility of the landowner, association or district charged with the general maintenance of the landscaping and other common improvements of the area in question.

Other Facilities The Southern California Gas Company will maintain natural gas lines within the project site. The Southern California Edison Company will maintain project electrical facilities. The local cable television company will maintain cable television facilities. Pacific Telephone will maintain telephone facilities.

12.7 Relation to the General Plan

The purpose of this section is to ensure the goals and policies of the City of Santa Clarita General Plan are represented in the Master Plan (design) and Specific Plan (implementation) components of the new community being presented by the Anden Group.

The following are the twelve elements included within the Santa Clarita General Plan:

1. Land Use
2. Housing
3. Economic Development
4. Circulation
5. Parks and Recreation
6. Open Space and Conservation
7. Air Quality
8. Public Safety
9. Noise
10. Community Design
11. Public Services, Facilities, and Utilities
12. Human Resources

In the following paragraphs, the issues and goals identified within the Santa Clarita General Plan under each element have been evaluated, and this project's affect or method of complying with the General Plan's goals are stated.

Porta Bella's Relation to the City's General Plan

The purpose of this section is to demonstrate that the goals and policies of the City of Santa Clarita General Plan are represented in components of Porta Bella. The November 1990 General Plan Draft is the basis of information for the design of the Master Plan.

To demonstrate the relationship, this section will 1) state the issues, goals, and policies of the individual General Plan by element, 2) Identify the features and methods of the project which are relevant to the General Plan.

The following are the twelve elements included within the Santa Clarita General Plan:

1. Land Use
2. Housing
3. Economic Development
4. Circulation
5. Parks and Recreation
6. Open Space and Conservation
7. Air Quality
8. Public Safety
9. Noise
10. Community Design
11. Public Services, Facilities, and Utilities
12. Human Resources

In the following pages, the issues and goals identified within the Santa Clarita General Plan under each element have been evaluated, and this project's affect or method of complying with the General Plan's goals are stated.

1.0 LAND USE ELEMENT

Issue 1: Types and mix of Land Use to be designated in the planning area.

Goal 1: To achieve the development of a well-balanced, financially sound, and functional mix of residential, commercial, industrial, open space, recreational, and institutional land uses.

Policies

- 1.1 Encourage the development of a broad range of housing types to meet the needs of the existing and future resident of the planning area, including, but not limited to, the development of single-family detached homes, condominiums, apartments, and manufactured housing.

The Specific Plan area has an unusually broad range of quality housing opportunities ranging from residential above commercial in the Town Center to 10,000 square foot estate lots.

- 1.2 Promote the development of service and neighborhood commercial activities to meet existing and future needs. These centers must be non-intrusive, sensitive to residential land uses, and located adjacent to arterial roadways.

Commercial and service land uses are an integral aspect of this Specific Plan. Commercial and service uses have been selected and located with the context and character of the neighborhood foremost in the design.

- 1.3 Establish a hierarchy of commercial centers, including neighborhood, community, and regional serving centers, together with appropriate and compatible levels of use to serve the population. The centers should be located on arterial thoroughfares, and be non-intrusive and sensitive to residential land uses so as to provide both convenience and compatibility.

Commercial opportunities on site range from community oriented centers near the arterials to fine grain specialty commercial in the project's interior. This range of scale for the commercial areas is designed to blend with and compliment the surrounding neighborhoods.

- 1.4 Encourage agricultural development, light industrial, manufacturing, office, and research and development activities that will not adversely impact the environment, while providing employment opportunities.

The Business Park and areas of the Specific Plan are located within contained environments at the base of the ridge, away from general visibility. Office uses are located near the train station and within the Town Center to capture the possibilities related to both these special employment generating neighborhoods.

- 1.5 Encourage the development of Business Park areas for future industrial/manufacturing land uses, with landscaping and other unified design standards.

The Specific Plan establishes a Business Park land use with special landscape, lighting, signage and development standards which will ensure the quality of the use and its sensitivity to surrounding land uses.

- 1.6 Warehousing and distribution activities should be located in proximity to freeways, rail lines, or other major transportation thoroughfares to facilitate the efficient movement of goods and minimize disruption and congestion on local and commercial streets.

The Business Park area is located near Soledad Canyon Road and the Southern Pacific Railroad.

- 1.7 Encourage complementary land uses which promote the development of hotels/motels, convention facilities, and other visitor-serving uses in the vicinity of Magic Mountain and in other appropriate locations in the planning area.

The specialty commercial Town Center of the Specific Plan area will be the terminus of Magic Mountain Parkway, with the other end extending into Magic Mountain. The Town Center will be highly visible from the elevated attractions at Magic Mountain. The Town Center will accommodate hotels and convention facilities, and is certainly an visitor attraction.

- 1.8 Not Applicable (airport related)

- 1.9 Encourage the use of utility rights-of-way for tree farms, nurseries, and row crops.

The major utility right-of-way on-site is the 100 foot wide aqueduct. This area is planned for open space for the adjacent land uses. No structures will be constructed on this ROW, however, it will have a park-like atmosphere.

- 1.10 Encourage the development of equestrian-oriented housing in areas that are presently equestrian oriented, and ensure that other surrounding land uses are compatible with the adjacent equestrian zone.

Although no equestrian homes are planned within the community, the general use trails of the specific plan area will allow equestrian use.

- 1.11 Promote the development of adequate land for recreational use to meet City park acreage standards.

The Specific Plan area has nearly 42 acres of high quality open space, recreation and parkland.

- 1.12 Provide for the reservation of adequate land to meet projected institutional and infrastructure needs.

An institutional land use designation has been provided for a strategically located property on-site. Infrastructure needs have been analyzed by civil engineers and planners, and land has been provided.

- 1.13 Promote the retention of open space to preserve ridgelines, to provide land use buffers, and to provide for both public safety and oak tree preservation.

This Specific Plan's largest land use area is Open Space & Parklands, which are interwoven into the community as well as largely left intact to allow significant animal and wildlife areas. The oak trees are identified in this Specific Plan as an extremely important element in Santa Clarita's character, and an Oak Tree rehabilitation area has been established to utilize the Specific Plan area's visible landform as an oak tree habitat.

- 1.14 Promote the development of commercial and industrial activities in all communities of the planning area.

Commercial and industrial activities have been provided for within this Specific Plan area.

- 1.15 Discourage the development of additional strip commercial centers and corner mini-shopping centers.

This Specific Plan does not allow strip commercial nor corner mini-shopping centers.

- 1.16 Encourage the establishment, where appropriate, of temporary and permanent outdoor sales of agricultural products.

Outdoor sales of agricultural products are permitted within the areas of the Specific Plan with a Conditional Use Permit.

Issue 2: Distribution of Land Use Development

Goal 2: To achieve a balanced physical environment through sensible land use planning and urban design, while establishing the City's role as a regional center.

Policies:

- 2.1 Promote the development of City centers, where more intensive land uses will be encouraged, including the development of a regional commercial center, office/business park centers, and entertainment complex, and a civic town center.

This Specific Plan establishes all of the above uses in an exceptional manner which will compliment, and not dominate, the existing communities. Regional attraction has been designed at a fine grain scale in context with the rural character of Santa Clarita.

- 2.2 Designate a central commercial core of concentrated and higher intensity commercial activities to serve the region and cultivate population, create an identity and progressive image for the City, and capitalize on related economic and employment benefits.

The Town Center will stand as a high quality, time enduring commercial/business area which is environmentally correct for the location. Economic and employment benefits will certainly be a part of this success.

- 2.3 Identify a primary town center and other centers which can accommodate a clustered mix of commercial, entertainment, recreation, town square/meeting places., and multi-modal transportation activity opportunities.

The Town Center identified in the Specific Plan will accommodate all of the above uses. Connection to the multi-modal transportation facility will be via an escalator lift, which will be an attraction in itself.

- 2.5 Promote Santa Clarita's location along Interstate 5 and Highway 14 as an important link between southern and northern California, as the northern gateway to the metropolitan Los Angeles area, and as an emerging regional center situated between Los Angeles and the high desert communities.

This Specific Plan is progressive, environmentally suited, and mixes land uses in an interesting and positive manner. The City of Santa Clarita will have a high quality identity which it can call its own.

- 2.6 Locate higher density residential development in close proximity to regional and sub-regional centers and public transportation corridors.

The project area's highest density residential is located near and intermingled with the Town Center, as well as within comfortable walking distance to the rail station.

- 2.7 Continue the established pattern of attractive greenbelts, golf courses, and entertainment/recreation amenities along Interstate 5, and promote a similar pattern along State Route 14 to strengthen and enhance the image of the City as a pleasant and fun place to live, work, visit, and play.

This Specific Plan is premised upon the live, work, play, and visit ideal, and the land uses are arranged and mixed to reflect this.

Issue 3: Quality and Maintenance of Development

- Goal 3: To ensure that development in the City is consistent with the overall community character and that it contributes in a positive way toward the City's image.

Policies:

- 3.1 Establish a land use pattern that is constructed around a framework of established greenbelts and a linear system of trails tied to the primary network of the river corridor.

The Specific Plan identifies a system of Open Space/Park and trails which is interwoven into the community and which make logical linkage to the surrounding communities and trail systems, especially utilizing the Santa Clara River trails.

- 3.2 Promote the development of key gateway design identification measure that will promote a positive community image and implement community design themes where appropriate.

Identity signage and monumentation will be used to identify the gateway entrances to the Town Center. In residential areas, focal landscape elements will generally be utilized in place of monuments and signage, in an effort to promote landmarks for the community without the detracting of signage.

- 3.3 Encourage setbacks, landscaping, or other measure to provide physical and visual buffers between land uses to minimize potential land use conflicts between dissimilar uses.

The visual buffering of land uses is provided for within this Specific Plan area. This specific Plan however, purposely strives for connection of land uses wherever possible to promote the other goals of this General Plan, such as pedestrian access and alternatives to automobile traffic. Physical separation of land uses is only encouraged where it is the only solution to minimize conflict between uses, such as loading areas, etc.

- 3.4 Focus revitalization efforts on eliminating blight along the railroad right-of-way adjacent to San Fernando Road, substitute landscaping for existing business, and investigate possible trail uses within the railroad right-of-way along San Fernando Road and elsewhere.

The construction of the extension of Magic Mountain Parkway is anticipated as necessary for this development. This Specific Plan proposed landscape treatment for this connection, as well and landscape treatment for the Parkway into the site. The area along the railroad is also suggested within this Specific Plan area as an Oak Tree landscape area, where oak trees would be the predominate landscape of businesses along this corridor.

3.5 Not applicable - pertains to older retail centers

3.6 Not applicable - pertains to redevelopment off-site

3.7 Not applicable - pertains to existing development off-site

3.8 Ensure that new construction and renovation of existing structures achieve a high level of architectural and site design quality.

This Specific Plan includes site planning, architecture, landscape architecture, lighting, parking, signage and other development standards to ensure a high quality, contextual product.

3.9 Ensure that signage on new and existing development is visually attractive and provides a high quality image for the City.

Sign guidelines are included within this Specific Plan which encourage creative and sensitive signage within the area, while discouraging gimmickry and visual anachronisms.

3.10 Not applicable - pertains to enforcement, which is covered in the Municipal Code

3.11 Not applicable - pertains to enforcement, which is covered in the Municipal Code.

3.12 Maintain the rural setting in those portions of the planning area that are presently rural and designated as such on the land use plan.

The character of the Specific Plan area has been identified as "oak-grassland." This character has been continued in the open

space areas, and the landscape concepts for the developed areas are intended to faithfully compliment this rural atmosphere.

- 3.13 Encourage the preservation of significant architectural, historical, and cultural structures and landmarks within the planning area whenever possible.

Although it is not within the Specific Plan area, the magnificent oak tree which stands on the adjacent City property was included on the conceptual grading plan to help ensure that it would not be destroyed.

- 3.14 Regulate lighting in new and existing development so that it does not unduly contribute to nighttime visual pollution and glare, and is compatible with surrounding land uses.

The Specific Plan includes a lighting standards and guidelines section which prescribes lighting ambience and character.

- 3.15 Consider forming an architectural design review process conducted by City staff to review designs of new developments.

This Specific Plan includes a "Design Review" section, which is intended to comply with a Design Review Board's scope of review.

- 3.16 Maintain and/or enhance the character of the various communities thorough compatible land use standards and design guidelines, while promoting an overall identity for the Santa Clarita Valley.

Land uses and arrangements within the Specific Plan area have been designed to blend with the existing fine grain communities. The proposed development in the Specific Plan area is of the small, rural scale which currently lends identity to the Valley. This area will, however, be of uses and quality which will be appropriate for a Town Center, including entertainment, commercial, and services.

- 3.17 Encourage landscaping, art, and other design amenities that complement and enhance the streetscape and the design of new development.

The landscape plan for this area is a revisitation to streetscapes in pedestrian oriented towns of early America. The streetscapes will be punctuated by landscape focal areas, and major identity elements and signage along streets will have a traditional, indigenous look, lending heritage to the Specific Plan area.

- 3.18 Promote the development of greenscape corridors and setback along major streets and arterials.

All streets within the Specific Plan area will have a traditional landscape palette associated with it. Major Streets will have broader landscape areas.

Issue 4: Preservation of Natural Resources

- Goal 4: To protect and harmonize with the physical environment through land use and urban design that is sensitive to the City's environmental setting and habitat areas.

Policies:

- 4.1 Encourage responsible and sensitive development on hillsides and prohibit development on ridgelines designated as "Significant Ridgelines."

Ridgelines within the development area have not been encroached with development. A "saddle" between the ridges on-site has been located with Town Center development, which is purposefully visible from below. This architecture will be reflective of the important, publicly visible location within which it is situated. This location is necessary to establish character for the entertainment/commercial/Civic/cultural Town Center heart of the City.

- 4.2 Ensure the new development, grading, and landscaping are sensitive to the natural topography and unique landforms in the planning area.

This Specific Plan is based upon an environmental perspective, with sensitivity to the landform including ridges, canyons, landscape, grading, and other natural aspects of the area. All of these elements are discussed within the Plan, with accompanying plans regulations and guidelines.

- 4.3 New development must be sensitive to the significant ecological areas (SEA's) through utilization of creative site planning techniques to avoid and minimize disturbance of these and other sensitive areas.

The project site was previously used for military weapons manufacture and testing. The site is severely degraded with grading and vegetation elimination activities. This Plan proposes that a significant amount of landscape and landform be recreated to match the surrounding natural environment.

- 4.4 Not applicable - pertains to unincorporated areas

- 4.5 Not applicable - pertains to the off-site Santa Clara River

- 4.6 Preserve and protect oak trees in their characteristic habitat, and other important indigenous plant and animal communities, from excessive and incompatible development.

Although the site is relatively unpopulated with oaks and riparian vegetation, some significant stands to exist. These oaks and other vegetation will be preserved if possible, or their removal will be mitigated as outlined in the accompanying Environmental Impact Report (EIR). Effort has been made in the design of this plan to establish buildable areas which would not disturb natural features.

Issue 5: Housing Distribution and Maintenance and Provision for Affordable Housing

Goal 5: To protect and enhance the integrity of existing residential neighborhoods.

Policies:

5.1 Not applicable - pertains to housing rehabilitation

5.2 Continue to provide for the development of new housing while ensuring that the character, scale, and density of new residential development is sensitive to and does not adversely impact existing residential neighborhoods

Development within the Specific Plan area is in harmony of scale, context and character with surrounding communities. Although the development proposed by this Specific Plan area has a purpose within the City of Santa Clarita, it is not intended to compete or overshadow existing communities and their historical character.

5.3 Provide for the retention and maintenance of existing residential neighborhoods which are primarily developed with single-family homes and ensure that new development is compatible with and complementary to existing development in terms of scale, architecture, and density.

Development within the Specific Plan area has a full range of housing product types. These products have been arranged in a complimentary fashion to each other. Where the new products of the Specific Plan meet the established products within the adjacent southerly community, the established pattern of development was carried through to this site.

5.4 Not applicable - pertains to matching character of existing structures

- 5.5 Provide low and moderate income family and senior citizen households with housing opportunities by promoting types of development that can accommodate such households.

Several product types within the Specific Plan area are compatible with the policies described above. Town Center residential units, senior housing in the Town Center, all of the Multiple Family housing products, and several of the single family housing products are quality, low to moderately priced homes. These product types are intermixed throughout the area for a better integrated and interactive community.

Issue 6: Responsible Growth Management

- Goal 6: To preserve the character of the communities and the integrity of the Santa Clarita Valley through orderly development practices and the provision of private and public capital improvements, facilities, and services to support existing and future development.

Policies:

- 6.1 Ensure demand for public facilities and services does not exceed the ability to provide and maintain such facilities and services; necessary facility improvements should precede or be coordinated with future development.

Within the Infrastructure and Phasing Sections of this Plan are strategies and requirements for providing adequate infrastructure prior to the construction of development.

- 6.2 Ensure, within the City's power, that facilities and services are provided in a timely manner as an affirmative obligation of the City.

This Specific Plan is essentially a contract between the City and the Developer(s). Facilities and services will be provided as described within this text, or the City has the legal right to withhold entitlement or development permits.

- 6.3 Establish and implement necessary safety measures and standards to ensure that development is appropriately restricted in areas where natural hazards are present, unless such hazards can be mitigated.

Like in many areas of Southern California, a fault runs through this site. The San Gabriel fault has a setback of 150 feet, and no structured development is permitted in this setback area.

- 6.4 Consider school adequacy when evaluating development proposals under the land use plan.

A central elementary school site with a playground and an adjacent park is proposed within the Specific Plan. The central location of the school within the community was determined early on in the design stage, and children walking to school will have a safe crossing of the Main Street roadway due to an underpass at the school site.

- 6.5 Not Applicable - pertains to annexations

- 6.6 Avoid or offset the adverse impacts of additional residential development as a necessary component of the growth control strategy.

Adverse impacts of growth usually pertain to infrastructure and support services, such as circulation, sewer, water, schools, fire, police, etc.. Within this Specific Plan, a series of strategies for offsetting each of these infrastructure and support elements is outlined.

- 6.7 Not applicable - discusses computer modeling by City

2.0 HOUSING ELEMENT

Issue 1: Opportunities for the development of new housing and the availability of a range of housing types in the planning area

Goal 1: To provide opportunities for the production of a range of new housing in the planning area to meet the needs of all income groups.

Policies:

- 1.1 Not applicable - pertains to City's land use plan
- 1.2 Not applicable - pertains to unincorporated areas
- 1.3 Not applicable - pertains to City monitoring of development
- 1.4 Promote the development of compatible mixed use project in order to create a village concept, with various interacting uses to facilitate the efficient use of facilities and services and to stimulate activity.

The Town Center District and the Soledad District areas are good examples of mixed use projects in the form of villages. The Town Center village intermixes residential uses to further stimulate the 16 hour activity area, while the Rail Station Commercial area has significant residential units within walking distance.

- 1.5 Review and support, as appropriate, programs to increase the supply of housing throughout the region, while considering environmental, market, infrastructure, and other factors.

This Specific Plan offers the City an opportunity to support a program which offers housing while sensitive to environmental, market, infrastructure and other factors.

- 1.6 Develop incentives or other mechanisms to encourage the private sector to provide opportunities for quality housing in the City.

This Specific Plan offers the City the opportunity to support quality living environment within the City.

Goal 2: To identify adequate housing sites which will be made available through appropriate zoning and development standards, and public services and utilities needed to facilitate residential development.

Policies:

2.1 Not applicable - pertains to City methods for providing housing.

2.2 Locate higher density residential development and housing for the elderly in close proximity to public transportation and commercial land uses, and in locations supplied with public services and recreation opportunities.

Higher density residential, including housing for the elderly is located within the Town Center area, and is within walking distance to commercial and support services, in addition to being within walking distance to the multi-modal transportation system via escalator.

2.3 Not applicable - pertains to housing over parking public parking lots.

Issue 3: Availability of affordable housing and the opportunity for development of new housing for all socioeconomic groups.

Goal 3: To provide sites suitable for a variety of housing types for all income levels and assist in the development and provision of affordable and proportionally priced and sized home to meet the needs of all community residents, including low and moderate income, large families, handicapped, female-headed household, and the elderly

Policies:

3.1 Not applicable - pertains to City implementation

3.2 Not applicable - pertains to City review

3.3 Encourage a mix of housing types and densities in new large scale residential developments.

This Specific Plan proposes a broad range of housing, including five multiple family and eight single family land use designations.

3.4 Not applicable - pertains to mobile homes.

3.5 Existing and future infrastructure needs should be addressed in a connection with considerations of new development proposals.

This Specific Plan discusses infrastructure and phasing.

3.6 Seek development which facilitates the efficient use of infrastructure, contributes to solutions of existing deficiencies, and anticipates and facilitates the orderly provision of future development and infrastructure consistent with this General Plan

This Specific Plan provides a major regional road which will ease crosstown traffic congestion, as well as providing internal infrastructure and support services for the Plan area.

3.7 Provide opportunities for the development of adequate housing to provide the City's fair share of low and moderate income households.

This Specific Plan offers a range of multifamily and several single family products which could be considered low to moderate in price.

3.8 Not applicable - pertains to government sponsored elderly care.

- 3.9 Promote the dispersal of low and moderate income housing throughout the Santa Clarita planning area.

This Specific Plan area has a percentage of low and moderate income housing intermixed throughout the area.

- 3.10 Encourage the development of residential units which are accessible to handicapped persons and adaptable for conversion to use by handicapped persons.

Development within the the Specific Plan area will be generally accessible to handicapped persons.

- 3.11 Not applicable - pertains to flexible housing standards within the City.

Issue 4: Maintenance of Existing Affordable Housing

- Goal 4: To maintain and improve the condition of the existing housing stock, particularly the affordable portion of the housing stock, where feasible

Policies:

No policies of this Section are applicable to this Specific Plan as they pertain to existing housing stock, which does not exist on this site.

Issue 5: Meeting Housing Needs

- Goal 5: To address and remove governmental constraints on the maintenance, improvement, and development of housing where appropriate and legally possible.

No policies of this Section are applicable to the Specific Plan as they pertain to governmental flexibility to provide low to moderate income housing, and density bonuses to new development. This project is currently under the low range of density as envisioned by the General Plan, and therefore density bonuses are not applicable.

Issue 6: Equal Housing Opportunities

Goal 6: To promote housing opportunities for all persons regardless of race, religion, sex, marital status, ancestry; national origin, age, physical handicap, or color.

Policies:

- 6.1 Promote safe and secure housing and neighborhoods, and encourage housing design which serves to deter crime.

A fundamental urban design concept within this Specific Plan area is to have interactive communities with minimal barriers such as walls. It is becoming evident that walls and land use barriers create "indefensible spaces" which are difficult to monitor. The reduction of walls and promotion of interaction is anticipated to result in a safer community.

- 6.2 Not applicable - pertains to government monitoring

- 6.3 Not applicable - pertains to government monitoring

- 6.4 Not applicable - pertains to homeless assistance

- 6.5 Encourage housing design standards that promote accessibility by the elderly and disabled.

Exterior spaces within this Specific Plan will be handicapped accessible. Individual structures will be accessible as building codes dictate.

- 6.6 Not applicable - pertains to government review.

- 6.7 Permit, subject to reasonable regulation, the location of residential care facilities in residential neighborhoods, as required by State law.

Within this Plan area, residential care facilities are conditionally permitted within several residential land use areas.

Issue 7: Development in Natural Resource Areas

Goal 7: To provide new housing Opportunities which are sensitive to social, aesthetic and environmental needs.

Policies:

7.1 Restrict housing development in areas containing important resources consistent with other goals and policies pertaining to natural resource areas.

Within the Specific Plan area, the ridgeline, Oak and Quigley and Oro Fino Canyons and almost 43% of the property was retained as Open Space/Parkland.

7.2 Encourage clustering of structures within areas containing important natural resources to preserve those resources.

Development areas of the Specific Plan were determined after determining significant natural resource areas. Development is clustered in areas determined to have least impact upon the environment.

7.3 Not applicable - pertains to government review

7.4 Require residential projects situated in mountainous terrain to preserve major ridgelines and other significant environmental features.

This project permanently preserves the ridgeline as Natural Open Space.

Issue 8: Development sensitive to energy conservation

Goal 8: Provide new housing opportunities which are environmentally sensitive and energy efficient.

Policies:

- 8.1 To the extent feasible, require the incorporation of energy conservation features in the design of all new housing developments and encourage the installation of conservation devices in existing development.

This Specific Plan requires the installation of water saving fixtures in all new residential and hotel developments.

- 8.2 Not applicable - pertains to government programs

- 8.3 Not applicable - pertains to government incentives

3.0 ECONOMIC DEVELOPMENT ELEMENT

Issue 1: Business and Economic Activity Diversification

Goal 1: To achieve a balanced mix of manufacturing, commercial, retail, cultural, entertainment, and service uses that result in a diversified and stable local economic base.

Policies:

- 1.1 Not applicable - pertains to annexation
- 1.2 Not applicable - pertains to City business strategy
- 1.3 Develop standards to allow for multiple economic activities in business and industrial park areas.

Business Parks within the area have a range of uses permitted and conditioned.

- 1.4 Not applicable - pertains to off-site areas
- 1.5 Capitalize on amusement, entertainment, and the arts as a theme for the planning area to create a positive identity and enhance tourist and business opportunities to bring revenues to the City.

The Town Center is designed with entertainment and cultural activities in mind. This specialty commercial area is distinctive in its character, and is anticipated to become a positive attraction and landmark for Santa Clarita.

- 1.6 Not applicable - pertains to existing businesses
- 1.7 Not applicable - pertains to business forecasting
- 1.8 Not applicable - pertains to government assessment of business.

Issue 2: Growth Guidance

Goal 2: To ensure adequate infrastructure and economic base support, the City should seek to stimulate simultaneous development of businesses and housing occurring within its boundaries and within planning area.

Policies:

- 2.1 Not applicable - pertains to government monitoring
- 2.2 Not applicable - pertains to city assessment of development
- 2.3 Not applicable - pertains to city assessment of jobs/housing balance
- 2.4 Not applicable - pertains to City's relationship with Los Angeles County.
- 2.5 Coordinate the timing of development with the phased provision of local infrastructure.

The infrastructure, circulation, and phasing sections of this Plan discuss in detail the development strategies on-site for these elements.

- 2.6 Not applicable - pertains to government regulation
- 2.7 Not applicable - pertains to sphere of influence

Issue 3: Economic Development Organization

Goal 3: Consider the establishment of public/private bodies that will facilitate the implementation of the economic development policies of the General Plan.

Policies:

The policies discussed in association with this goal do not have applicability to this Specific Plan, in that they are related to the creation of a government body.

Issue 4: Provision and Financing of Infrastructure

Goal 4: To serve existing and new economic growth, the City should pursue timely and equitable strategies to provide financing of basic, community, and public safety infrastructure.

Policies:

4.1 Not applicable - pertains to City relationship with Caltrans.

4.2 Work in support of local school districts to determine additional methods of providing local capital for school district facilities development, rehabilitation, and maintenance.

An elementary school site has been provided on-site in accordance with discussions between the applicant and the school district.

4.3 Not applicable - pertains to school priorities

4.4 Not applicable - pertains to funding for schools

4.5 Not applicable - pertains to funding for schools

4.6 Not applicable - pertains to school financial impacts

4.7 Not applicable - pertains to fiscal impact modeling

4.8 Actively participate in all freeway and highway projects that impact the City.

Provided on-site is a secondary highway which will ease the north/south congestion experienced on San Fernando Road.

Issue 5: Fiscal Balance

Goal 5: To ensure the City's present and future fiscal balance of municipal revenue and expenditures is maintained.

Policies:

- 5.1 Seek a mixture of land uses, and the progressive and concurrent development of such uses, so that service costs are provided for in the municipal budget, the General Fund, Capital Improvement Program, Enterprise Funds, and other financing mechanisms.

The Specific Plan proposes a broad array of uses ranging from regional commercial to institutional.

- 5.2 Require new public and/or private developments to demonstrate the ways in which they can contribute to the achievement of municipal fiscal balance.

The commercial activities provided for within this plan area, in addition to the employment opportunities will greatly help in providing fiscal balance to the City of Santa Clarita.

Issue 6: Revitalization

Goal 6: To promote revitalization of the City's long-term economic stability

Policies:

No policies related to this section are applicable to the Specific Plan in that they pertain to existing development revitalization.

Issue 7: City Marketing

Goal 7: To market and promote the city's available resources as necessary to encourage further expansion of its economic base.

Policies:

No policies related to this section are applicable to the Specific Plan in that they pertain to the establishment of a City Government body.

4.0 CIRCULATION ELEMENT

Issue 1: Arterial Street System Capacity, Potential Traffic Conflicts, Neighborhood Encroachment by Through Traffic, and Public Improvement Requirements.

Goal 1: To provide a circulation system to move people and goods safely and efficiently throughout the City of Santa Clarita and the planning area.

Policies:

- 1.1 Not applicable - pertains to upgrading existing roads.
- 1.2 Not applicable - pertains to maintaining existing roads.
- 1.3 Preserve the quality of residential neighborhoods by discouraging the flow of truck and through traffic in these areas consistent with circulation and emergency needs.

The circulation pattern designed within the Plan area is not one of through circulation through the project area. Through circulation is provided for outside of the residential area via the four regional roads surrounding the community.

- 1.4 Not applicable - pertains to establishment of capital improvements program
- 1.5 Encourage consistent through street names

Wherever applicable on-site, through street names have been consistent with off-site streets, such as Rio Vista Road, Magic Mountain Parkway, Santa Clarita Parkway and Via Princessa Road.

- 1.6 Not applicable - pertains to governmental coordination.
- 1.7 Not applicable - pertains to County relationship
- 1.8 Not applicable - pertains to computer based traffic projections

- 1.9 Not applicable - pertains to the existing roadways.
- 1.10 Limit the number of intersections and driveways on major arterials and expressways to promote a safe, efficient, and steady flow of traffic.

The number of commercial driveways public intersections is limited on these arterials and parkways, as is illustrated within the circulation section of the Specific Plan.

- 1.11 Not applicable - pertains to truck routes through City.
- 1.12 Adopt a program of street and highway landscaping to enhance the appearance of the City's circulation system.

Within the Landscape section of the Specific Plan, a comprehensive landscape plan for all streets within the area is illustrated.

- 1.13 Not applicable - pertains to City enforcement
- 1.14 Require access to higher density land uses and commercial development from arterials, and not through low density residential neighborhoods.

Access to higher density commercial areas is from major roads, and does not encroach lower density residential areas.

- 1.15 Establish hillside street standards which are sensitive to topographic constraints, necessary grade separations, and other special needs.

The Santa Clarita Parkway, Magic Mountain Parkway, Rio Vista Road, and Roadway Q all have special design criteria as discussed within the circulation and landscape sections of this plan which ensure the sensitive treatment of the hillsides.

- 1.16 Encourage curvilinear street designs in both level and hillside areas to contour to the topography and to create a more pleasant street environment.

Streets within the Plan area have been finessed into their locations with respect to the topography of the natural landforms. Arcing streets, rather than simple curvilinear streets, have been utilized to compliment the topography and maintain a logical, classic street system.

- 1.17 Not applicable - pertains to City funding for transportation

Issue 2: Need for Local Transit Services

- Goal 2: Promote a public transportation system that is safe, convenient, efficient and meets the identified needs of the City of Santa Clarita.

Policies:

- 2.1 Incorporate accommodations and facilities to support local transit serve in new development, where feasible, that is consistent with local transit planning.

Local transit stops have been identified, and standards for bus facilities, such as shelters have been identified within the circulation element.

- 2.2 Not applicable - pertains to private transportation

- 2.3 Not applicable - pertains to government coordination

- 2.4 Develop a multi-modal transit facility that is strategically located in the City, adjacent to potential public transit rail line, and located convenient to major local residential and employment centers.

The multi-modal transportation facility within the plan area accommodates rail, auto, bus, and pedestrian and bicycle mobility.

- 2.5 Develop adequate pedestrian access and encourage the use of these systems.

An extensive pedestrian system is located along the streets via sidewalks, and in Open Space/Parkland areas via trails. This system is extensive and incorporates the use of an escalator to connect the upper developed area with the lower river valley trail systems.

- 2.6 Require dedication and/or construction of appropriate facilities in support of public transportation system in new development.

Within the development, a multi-modal transportation facility will be constructed, and public transit will be provided for in the forms of shelters and other facilities and required by the local transit company.

Issue 3: Transportation Alternatives

- Goal 3: To promote safe, effective alternatives to the personal automobile that will meet the needs of all planning area residents.

Policies:

- 3.1 Establish a master plan of bikeways that is coordinated with the County plan for the Santa Clarita Valley in order to provide an adequate system for the safe and efficient movement of cyclists.

The character and design of all streets within the Specific Plan area is intended to be attractive and safe to cyclists. Off street trails also are incorporated within the plan area for mountain bike enthusiasts.

- 3.2 Provide a system of sidewalks or pathways, tunnels, and bridges in residential, commercial, and industrial areas that feature a safe environment integrating pedestrian and bicycles in a manner harmonious with the surrounding neighborhood.

The Specific Plan has an extensive sidewalk system which includes tunnels and underpasses for pedestrians and bicycles to separate them from automobiles at major points, especially the school and the rail tracks.

- 3.3 Promote bicycle accessibility to all public facilities, including parks, schools, and centers of civic activity.

Cycles can reach these facilities along the public roadways. Upon arrival, bicycles may be locked at central bicycle parking areas as outlined within the circulation section.

- 3.4 Develop an integrated system of bus service that reaches all major concentrations of residential development and employment.

A comprehensive bus system will be accommodated on site, as described in the circulation system.

- 3.5 Not applicable - pertains to City encouragement of alternative transportation methods.

- 3.6 Not applicable - pertains to right-of-way dedications

- 3.7 Not applicable - pertains to a ride sharing program.

Issue 4: Parking Facilities

- Goal 4: To provide an adequate supply of private off-street and public parking to meet the needs of residents and visitors to the City.

Policies:

- 4.1 Adopt regulations which specify minimum parking requirements for various types of land use. Periodically review and revise these standards as commuting patterns, vehicle sizes, and land uses change over time.

The parking section of this Specific Plan describes in detail a program for minimum parking requirements for all land use areas within the Plan area.

- 4.2 Provide public parking resources in response to the demand for such facilities, through development exactions, special assessment districts or other appropriate funding mechanisms.

Public parking will be provided for on-site within parking garages and facilities as outlined within the circulation and parking sections of this Plan.

- 4.3 Screen and/or buffer large parking areas from the public view.

An intention of this Specific Plan is to minimize the size of outside parking areas. Where parking is visible from public areas, it will be landscaped and screened as described within the parking and landscape sections of this Plan.

Issue 5: Regional System Impacts

- Goal 5: Pursue an aggressive posture in the region in advocating a regional transportation solution.

Policies:

The policies discussed within this Section are not applicable to the Specific Plan area in that they pertain to intergovernmental coordination and lobbying for regional road systems.

5.0 PARKS AND RECREATION ELEMENT

Issue 1: Development of a Comprehensive System of Parks and Recreational Facilities to Meet Existing and Future Needs of Residents.

Goal 1. Provide, develop, and maintain parks with quality recreational facilities dispersed throughout the area.

Policies:

1.1 Provide a combination of local park acreage, park facilities, and recreation programs to serve neighborhood needs.

A connected system of parkland and open space has been provided in Porta Bella as a framework for recreational needs. Also proposed within this Plan is a tennis facility and a golf driving range.

1.2 Develop a variety of park types and sizes (regional, community, neighborhood) which are distributed adequately to serve all area residents and to prevent overcrowding and overuse.

Parks and recreation areas within Porta Bella range from 3.5 to 22 acres. The parks are dispersed throughout the community, with the largest community oriented park located for ease of access to from the regional road.

1.3 N/A - pertains to park activity programs.

1.4 N/A - pertains to existing communities.

1.5 Establish a master plan of parks and recreation facilities, bicycle routes, and hiking and equestrian trails.

Within Porta Bella, the trail systems have been incorporated into the existing and proposed regional trail systems as illustrated on the Vicinity Plan.

- 1.6 Promote the integration of the network of trails and open space to provide linkages to parks within the planning area.

The parks, recreation and open space areas of Porta Bella are interconnected and located for maximum benefit to the community.

Issue 2: Park Standards

- Goal 2: To establish standards and implementation measures to guide future parkland development though the area as provided in this element.

Policies:

- 2.1 N/A - pertains to the establishment of community standards
- 2.2 N/A - pertain to the implementation of community standards
- 2.3 N/A - pertains to infilling parks into “park poor” existing areas.

Issue 3: Park Improvements and Maintenance

- Goal 3: To encourage the improvement, rehabilitation, and maintenance of existing parks and recreational facilities.

Policies:

- 3.1 N/A - pertains to existing athletic fields
- 3.2 N/A - pertains to establishment of funds for park development
- 3.3 N/A - pertains to final park design.
- 3.4 N/A - pertains to the formation of a citizens volunteer group
- 3.5 N/A - pertains to joint powers agreements to establish park standards

Issue 4: Parks Acquisition

Goal 4: To encourage measure to acquire future parkland

None of the policies associated with this issue or goal are applicable within Porta Bella, in that parkland & open space is being provided as an integral amenity of the community.

Issue 5: Recreational Use of the Santa Clara River and Other Natural Features

Goal 5: Utilize the Santa Clara River as a central recreational corridor and identify other significant natural features to be designated as opens spaces, parks, and recreational opportunities.

N/A - pertains to the creation of the Santa Clara River as a recreational corridor. This Specific Plan does recognize, however, the significance of the relationship between the Santa Clara River and the project site, and therefore endorses the provisions outlined in the Santa Clara River Corridor Study by BSI.

Issue 6: Development of Parks and Recreational Design Criteria Which Encourage Public Safety and Sensitivity to Impacts on Existing and Future Neighborhoods

Goal 6: To implement the design criteria for park areas described in the parks and recreation element which consider park access, safety, appropriate signage, parking requirements, and the preservation of natural features.

Policies:

6.1 Design new recreational areas to minimize the visual, noise, and traffic impacts on neighboring communities.

In Porta Bella, the parks & recreation areas have been designed with the above criteria in mind. The illuminated community park is located over the ridge from Porta Bella, to minimize noise, visual, traffic & lighting impacts on the community.

- 6.2 Implement design guidelines which provide for appropriate access, safety, parking requirements and signage.

The Specific Plan calls out standards for each of these elements through design or regulation.

- 6.3 N/A - pertains to park supervision

- 6.4 Provide design guidelines for the preservation of natural features

Within Porta Bella, the Open space areas were determined with natural features as an integral factor. The prominent ridgeline has been preserved as a natural edge, or a backdrop to development depending upon which angle the site is being viewed from.

- 6.5 N/A - pertains to park standard consistency between public & private parks.

Issue 7: Establishment of a Comprehensive Trails System

- Goal 7: Provide an efficient public trails system linking public space and adjacent regional systems to meet transportation and recreational needs of the area.

Policies:

- 7.1 N/A - pertains to the establishment of a regional trail system.

- 7.2 Design trail routes, trail heads, and staging areas and designate trail uses to minimize impact upon adjacent property, neighborhoods, and fragile habitats.

Trail systems within Porta Bella are located in areas which are not anticipated to degrade property nor fragile habitats.

- 7.3 N/A - pertains to intergovernmental coordination for trail systems.

7.4 N/A - pertains to multiple use of easements as trails, which is not necessary in Porta Bella due to the connectivity of its opens space systems.

7.5 Consider the implementation of recommendations for trail alignment and staging areas, as proposed in the adopted Master Trails Plan.

Trails within Porta Bella share linkage with the trails proposed within the City's General Plan and Santa Clara River Study.

7.6 N/A - pertains to jurisdiction between county and city.

7.7 N/A - pertains to the Santa Clara River

7.8 Provide equestrian trails development along routes which are viable to the health and safety of horse and rider.

Equestrian trails in Porta Bella are located within the canyons and ridge areas to connect with other regional trails which circumerference the site.

7.9 Provide pedestrian trail and bikeways which are separate from vehicular traffic and provide maximum safety when the crossing of streets or highways is necessary.

Trail systems within Porta Bella are always separated from streets by either a landscaped parkway, or by its alignment through an open space area. Where the trail system crossed Main Street near the school, an pedestrian underpass will be included to enable children to cross without conflict with the automobile.

7.10 N/A - pertains to Sand and Placerita Canyons

7.11 Provide trail access to scenic viewpoints and provide scenic overlooks and picnic areas along trail routes.

The trails within Santa Clarita have scenic overlook areas provided for within the Master Plan.

- 7.12 Pursue the development of a bike trail that connects with existing and planned trails in Ventura and Los Angeles Counties.

Streets within Porta Bella are designed to accommodate bicycles via an 18 foot break-down/bicycle lane. Also, the multi-modal transportation facility will accommodate bicycles.

- 7.13 N/A - pertains to the development of standards for trails.

Issue 8: Development of Community Centers

- Goal 8: To develop community centers which provide multiple-use opportunities for the residents of the planning area.

Policies:

- 8.1 N/A - pertains to standards for community development

- 8.2 Pursue property acquisition for the development of a recreational, cultural, community center, and athletic center at the Saugus Rehabilitation site or at other appropriate locations.

The design of Porta Bella is purposefully oriented to allow the development of the City Hall site in coordinated, synergistic manner. The location of Santa Clarita Parkway on the eastern side of the project site alleviates the need for a regional road to be located within the City owned property, thereby degrading the properties natural atmosphere.

- 8.3 N/A - pertains to the support of community facilities and programs

- 8.4 Promote joint property agreements for use and development of joint school park sites and facilities.

The school site of Porta Bella is large enough to stand alone as an elementary school with playgrounds, however, the location of the neighborhood park adjacent the school facility compounds the opportunities for recreation in the local community.

Issue 9: Provision of Recreational Opportunities For All Age Groups and Economic Backgrounds

Goal 9: To develop a system of parks and recreational facilities and programs which provide recreational opportunities for all segments of the community.

Policies:

The policies associated with this issue and goal are oriented towards the implementation of volunteer and community organizations for recreation. The Open space, recreation and park lands established within this Plan enable the physical location for these groups and their activities. The Oak tree habitation area is especially applicable for policy 9.3, which encourages a summer youth tree planting program. This policy melds with the idea of reestablishing oak trees within the project and surrounding areas.

Issue 10: Private Developers and Public Agencies Cooperation

Goal 10: To promote public/private cooperation in developing recreational services and facilities.

Policies:

10.1 Promote the expansion of joint-use agreements with the school district to provide recreational programs and facilities in existing and future residential neighborhoods.

The school within Porta Bella has been located adjacent to a neighborhood park and an open space area designated for riparian rehabilitation. The relationship of these amenities is considered to be an educational and recreational opportunity to complement the school site.

10.2 N/A - pertains to joint use agreements between non-profit agencies.

- 10.3 N/A - pertains to the promotion of multiple uses for public rights-of-way.
- 10.4 N/A - pertains to intergovernmental cooperation to establish parks.
- 10.5 Encourage the development of private commercial recreation facilities such as batting cages, miniature golf, driving ranges, aquatic facilities, skate courses, food service concessions, and other commercial activities.

Within Porta Bella, two areas are designated as "Recreational" in land use, which means they are privately owned, but for recreational purposes. The designated uses for these parcels are for tennis and a golf driving range.

- 10.6 Encourage developers to improve parks and recreational facilities in lieu of paying fees as partial fulfillment of park and recreation requirements.

Within Porta Bella, nearly 43 percent of the project area is dedicated to Parks, Recreation and Open Space. Porta Bella contains over 30 acres of active use public park land. Porta Bella is intended to have convenient accessibility for all residents to the Open Space System.

Issue 11: Development of Recreational Facilities and Services for the Area's Business Community

- Goal 11: To develop facilities and services that meet the needs of retail, commercial, and industrial businesses in the planning area.

Policies:

- 11.1 Encourage the development of park and recreation facilities in commercial and industrial areas to provide services and programs for employees.

The heart of the Town Center area of Porta Bella is focused upon the Center Park Circle, which is a passive park area. A linear open space park has been indicated and called out for incorporation into development schemes which will connect through the Town Center area. Near the Business Park area, the 22 acre community park is located. Within walking distance to the Soledad District is the tennis facility and clubhouse, a little further up Club Drive is the golf driving range. The Commercial businesses within Porta Bella all have convenient access to public and private park and recreation facilities within the project area.

- 11.2 N/A - pertains to fee mechanisms for industrially located parks.
- 11.3 N/A - pertains to the establishment of a Industrial/Commercial Parks Advisory Council.
- 11.4 N/A - pertains to the conducting of a survey.

6.0 OPEN SPACE AND CONSERVATION ELEMENT

Issue 1: Preservation of natural Resources

Goal 1: To preserve the special natural features which define the Santa Clarita planning area and give it its distinct form and identity.

Policies:

1.1 Utilize major environmental features (significant landforms, ridgelines, vegetation, ecologically significant areas, other resources) as open space within the planning area.

The Porta Bella Specific Plan was designed to preserve and remediate the natural ridgeline which characterizes the site. In addition, Oro Fino and Oak Canyons were preserved as Open Space, as well as designated for riparian rehabilitation due to the destruction of the vegetation by previous use. An important element of this Plan has been to locate the north to south regional road from the City property to the specific plan property, thereby preserving the beautiful oak meadows on the City property. A grading plan has been prepared which includes the City's property, and also preserves the heritage oak as a part of the City Hall courtyard as illustrated on the conceptual grading plan.

1.2 N/A - pertains to Castaic Lake

1.3 Incorporate standards for clustered development in the Municipal Zoning Code to minimize the disruption of natural resources and/or major physiographic features.

Housing with Porta Bella is a mixture ranging from single family 10,000 square foot lots to Multi-family 40 to the acre. One purpose for the mixture is to enable the clustering of units in strategic locations to allow open space and park areas which are woven into the residential areas.

1.4 N/A - pertains to forest service lands.

- 1.5 Consolidate open space under a public trust to maintain viable natural ecosystems in conjunction with the orderly development of the planning area.

Open space lands within the Plan area are large and connected, to provide viable natural ecosystems.

- 1.6 Link buffer areas, wherever possible, to provide for contiguous areas of open space.

Open space lands within the plan area are large and connected, to provide viable natural ecosystems. Buffer areas such as adjacent to the Neighborhood Commercial area are also connected into the System to provide access and connectivity.

- 1.7 Revegetate graded areas with native, drought-resistant plant species while ensuring that such programs are consistent with fire prevention efforts.

Section 5 of the Specific Plan details the revegetation of Open Space areas in a natural palette of plants. Section 5 also contains a fuel modification portion with a complimentary plant palette.

- 1.8 N/A - pertains to the placement of Open Space lands into State Park.

- 1.9 N/A - pertains to the Santa Clara River.

- 1.10 Implement landform grading standards which minimize the impact of grading operations and foster replication of naturally recurring landforms.

The concept grading plan and the grading standards included in Section 5 detail and require the use of sensitive landform grading techniques.

- 1.11 Encourage the expansion of the paseo systems and the building of paseos or linkages between parks and streets.

Porta Bella is wholly connected for the pedestrian through all land use areas by the use of sidewalks, paseos and trails.

- 1.12 Strongly oppose dumping in undeveloped areas and simultaneously promote approved means of construction and household waste disposal.

The Porta Bella Specific Plan takes a pro-active stance in the provision of facilities for the purpose of recycling and hazardous household waste collection. It is the position of this Plan that said facilities should be made available within the Plan area by conditional use permit.

- 1.13 Encourage the incorporation of unique or significant natural features such as park and open space amenities within new developments.

The structure of Porta Bella is based upon the natural landform. Many of the features of the plan utilize the natural features, such as the "Crescent walk" which follows the crescent shape of the Bowl area in the saddle of the ridgelines, and the "Ridge-runner" trail system, which follows the ridgeline of the Specific Plan, and is accented by lookout vistas and seating.

Issue 2: Ridgeline Preservation

- Goal 2: To preserve designated natural ridgelines in the planning area to maintain the aesthetic character of the Santa Clarita Valley

Policies:

- 2.1 N/A - pertains to the adoption of a ridgeline preservation ordinance.
- 2.2 N/A - pertains to the utilization of a slope rating system.

- 2.3 Identify and incorporate the dominant and special scenic topographic features, landmarks, and other physical characteristics in each community as a component for development a community image.

The Porta Bella Specific Plan suggest an underlying community character based upon the environment. Four landscape treatment areas have been identified in Section 5 which define the incorporation on a community-wide basis of the environmental character.

- 2.4 Provide for scenic vista points, where consistent with other policies of this plan, for protection of ridgelines and sensitive development techniques.

This Plan incorporate scenic vista points as part of the Ridge-runner trail system.

Issue 3: Sensitive Habitat Areas

- Goal 3: To protect significant ecological resources and ecosystems, including, but not limited to, sensitive flora and fauna habitat areas.

Policies:

- 3.1 N/A - pertains to SEA overlay zones
- 3.2 Encourage the preservation of oak woodlands, oak savannahs, and individually significant oak trees.

This Plan is pro-active in the preservation, recreation and enhancement of Oak tree habitat. The oak woodland and oak grassland treatment zones located within Section 5 of this plan further detail the commitment to the oak character. An city wide oak tree mitigation area has been established on-site, as well as two riparian mitigation areas.

- 3.3 Identify and protect areas of significant ecological value, including, but not limited to, significant ecological habitats.

Open Space areas within this Plan are large and connected. They are also located in areas which were considered to be the least disturbed and most ecologically significant on the project site.

- 3.4 Consolidate open space areas that represent regionally significant wildlife corridors to promote continued wildlife productivity and diversity on a regional scale and restrict development and intensive human activity in areas which sustain rare or endangered species, such as migratory bird species, fish, and rare plant species.

In Porta Bella, a rare opportunity to reopen an Open Space Corridor exists. The previous use cut access and degraded wildlife and vegetation habitats from its boundaries. Within Porta Bella an large open space area will be reopened to which will once again be rehabilitated by wildlife and vegetation.

- 3.5 N/A - pertains to existing SEA areas.

- 3.6 N/A - pertains to private programs.

- 3.7 Promote the preservation of natural riparian habitat.

Two areas are designated within Porta Bella as natural riparian mitigation areas.

Issue 4: Open space Areas for Outdoor Recreation Uses

Goal 4: To preserve open space areas for recreational use as a natural buffer to more intensive land uses.

Policies:

- 4.1 N/A - pertains to city park site selection.

- 4.2 N/A - pertains to city purchase of parks.
- 4.3 Provide a diverse mix of recreational use and scenic view areas within open space sites.

Porta Bella's Open Space System contains natural open space, active parks, recreational facilities including tennis and golf driving range, and trail systems with scenic overlooks.

- 4.4 Encourage the cohesive development of trails and open space as a unified system, contiguous throughout the City and Planning area with linkages to County, state federal and other parklands and trial systems.

The Vicinity Map included within Section 5 illustrates the location of trails and their linkage into the established and proposed trails of the General Plan and the Santa Clara River Plan. Also, Section 4, Transportation and Circulation, outlines the proposed trail routes.

- 4.5 N/A - pertains to the Santa Clara River.

- 4.6 Promote the development of equestrian trails along routes which are viable to the health and safety of horse and rider.

Equestrian trails are provided within this Plan as described in Section 4.

- 4.7 Previously discussed.

- 4.8 N/A - pertains to the transfer of trails maintenance.

- 4.9 N/A - pertains to the acquisition of new trails.

- 4.10 N/A - pertains to the acquisition of woodlands

- 4.11 Encourage the open space lineage opportunities throughout the City and adjacent park and forest areas

The Opens Space System of Santa Clarita enjoys relationship to existing vacant land. Depending upon the development disposition of the adjacent land, an opportunity exists to create even larger open space areas for habitats.

- 4.12 Protect adjacent neighborhood areas from noise, visual, and traffic impact so new active recreational areas.

The new 22 acre active recreational park is located away from residential by separation by the Ridge. The tennis facility is located near the "club" atmosphere of the multi family 40 area, as well as the golf facility. The park located within the Central community has adjacency with the school and the multi family 22 area.

- 4.13 N/A - pertains to off-road vehicles in the National Forest.

- 4.14 Promote a coordinated public system of hiking, bicycle, pedestrian, and equestrian trails.

Section 4 of the specific Plan details the incorporation of these trails, as well as their accessibility to the multi-modal transportation system to be located in the Soledad District.

- 4.15 N/A - pertains to a regional trail system establishment.

- 4.16 Ensure that trails are used for their intended purpose and that trail use does not infringe upon or violate private property rights.

Trails within Porta Bella are generally located within populated areas via sidewalks, which link into the open space trail systems at appropriate areas.

- 4.17 Seek park sites having areas of natural scenic beauty which can be conserved and enjoyed by the public, as well as areas having active recreational opportunities.

The park sites of Porta Bella are located with unique views of the Santa Clarita Valley. The 22 acre community park is located with views of the Santa Clarita Valley. The park site near the school overlook Oak Canyon, and the recreational facilities near the MF 40 area are located within the "Bowl" Canyon area, which is an interesting landform.

- 4.18 Promote the establishment of Landscape Maintenance Districts within new developments as a means of preserving and maintaining on-site recreation and open space areas.

Landscape and lighting districts will be created within the Porta Bella Specific Plan Area to maintain common landscape areas owned by the Homeowner's association.

Issue 5: Open Space Designations as Protection Against Natural Hazards

Goal 5: To use the open space designation to ensure the public health and safety and welfare in areas subject to natural hazards.

Policies:

- 5.1 Integrate natural hazard areas, such as floodways, seismic fault zones, and unstable soils, in to the open space network.

The seismic fault which traverses the site along the ridgeline has been incorporated into the open space land use area.

5.2 N/A - pertains to flood zones.

5.3 N/A - pertains to flood zones.

5.4 N/A - pertains to areas of unmitigatable environmental hazard

5.5 Encourage the use of flood control structures which maximize groundwater recharge and the use of floodways as native habitat.

The Porta Bella Specific Plan is pro-active in the area of groundwater recharge. Section 10 encourages the use of natural groundwater recharge methods, and section 5 details the use of natural drainage swales in place of storm drains along several roadways.

- 5.6 Promote the design of slope drainage concepts which:
- minimize the need for slope irrigation
 - Provide measures for groundwater recharge, either on-site or off-site
 - Minimize the construction and placement of slope drainage structures which are intrusive, out-of-scale, and/or incompatible with the surroundings.

Section 5 of the Specific Plan outlines the grading and drainage of slopes, as well as describing the use of natural, low water use plant materials which will require minimal or no artificial watering.

Issue 6: Open Space Area Uses for the Managed Production of Mineral Resources

Goal 6: To encourage the management and protection of valuable mineral resource in a manner which will ensure productivity and utility of these resources for present and future uses while minimizing disturbance, a feasible, to dissimilar/incompatible surrounding uses.

Policies:

None of the policies outlined within this section are applicable to Porta Bella because the plan area does not permit mineral resource extraction.

Issue 7: Water Resources Preservation

Goal 7: To protect the quality and quantity of local water resources, including the natural productivity of all surface and groundwater, and important watershed and recharge areas.

Policies:

- 7.1 N/A - pertains to water supply
- 7.2 N/A - pertains to water supply quality
- 7.3 Maintain the natural productivity of streams, rivers, and other water bodies by supporting regulatory practices which prevent erosion and minimize pollutant content in surface runoff from major development.

By utilizing the natural drainage swales, runoff will have the opportunity to recharge prior to flowing into the drainage system. In addition, the water will be less polluted because it will be filtered through the earth, which is a method of purification. The natural drainage systems permitted and encouraged in Section 10, if implemented, will also compliment the natural water system.

- 7.4 N/A - pertains to manufacturing pollution
- 7.5 N/A - pertains to groundwater recharge areas
- 7.6 Require storm control systems, where necessary, to conform with the natural drainage patterns of the area.

Natural drainage systems have been utilized in the drainage of Porta Bella. The use of drainage swales which direct the flow of water into numerous smaller drainage systems into canyons is an example of this type of drainage.

- 7.7 N/A - pertains to floodways.
- 7.8 N/A - pertains to watersheds
- 7.9 N/A - pertains to wastewater recycling
- 7.10 N/A - pertains to inter-governmental coordination.

- 7.11 Promote water conservation through educational and other programs.

Section 6 of the Specific Plan contains a section entitled Water Conservation which requires the use of water saving devices and methods for all residential (including lodging) development in Porta Bella.

- 7.12 Encourage the use of native and drought tolerant plant species for revegetation and landscaping.

Section 5 discusses and encourages native and drought tolerant plant species. The plant selection along many of the streets also incorporate drought tolerant plant species.

- 7.13 Protect groundwater quality through the establishment of a sanitary sewer system hook-up program to require the connection of all urban uses/densities.

Porta Bella will be served by a sewer system.

- 7.14 N/A - pertains to industry monitoring

- 7.15 N/A - pertains to citywide implementation to water conservation.

Issue 8: Energy Conservation

- Goal 8: To reduce the community's reliance on traditional energy resources through the initiation of energy conservation practices and the utilization of available energy technology.

Policies:

- 8.1 Promote the conservation of energy in the planning area

Porta Bella is pro-active in the area of energy conservation. Section 3 discusses the encouragement of energy conserving techniques within the Lifestyle discussion.

- 8.2 Encourage the incorporation of conservation features, such as solar panels, in the design of new development and the installation of conservation devices in existing developments.

Section 3 discusses the incorporation of energy conserving methods within the Lifestyles section, Section 5 discusses energy conserving landscapes, and Section 6 permits the incorporation of energy conserving techniques such as solar panels and skylights.

- 8.3 N/A - discusses incentives for energy conservation

- 8.4 Encourage the use of passive design concepts to increase energy efficiency.

Porta Bella discusses and encourages the use of passive design concepts to increase energy efficiency.

- 8.5 N/A - pertains to public buildings

Issue 9: Recycling of Natural Resources

- Goal 9: To encourage the sorting and recycling of all possible materials and the reduction of future landfill space requirements.

Policies:

- 9.1 Establish a citywide recycling program of the reuse of newspapers, cans, bottles and other recyclable materials.

Porta Bella discusses, encourages, and permits by conditional use permit recycling and household hazardous waste collection centers in all non-residential areas.

- 9.2 N/A - pertains to intergovernmental coordination

Issue 10: Preservation of Historical, Cultural, and Archaeological Resources

Goal 10: Protect the historical and culturally significant resources which contribute to community identity and a sense of history.

Policies:

10.1 N/A - pertains to existing historical structures

10.2 N/A - pertains to existing historical structures

10.3 N/A - pertains to the support of programs

10.4 N/A - pertains to the development of guidelines to preserve historic structures

10.5 Integrate historic sites with recreational and open space areas whenever possible.

The Porta Bella Specific Plan suggests a concept development plan for the City site which preserves the heritage oak tree as a part of the new City Hall courtyard. This Civic Center Plaza opens into a 200+ acre oak meadow parkland.

10.6 Incorporate historic sites into proposed development in such a manner as to preserve the integrity of the site whenever possible.

This heritage oak tree located on the City property has been conceptually designed into the courtyard of the new City Hall structure.

7.0 AIR QUALITY ELEMENT

Issue 1: Government Coordination on Air Quality Issues

Goal 1: To minimize conflicts between City and other governmental agency air quality policies, plans, and programs.

Policies:

Policies located within this section pertain to Governmental coordination.

Issue 2: Transportation Demand Management

Goal 2: To reduce emissions resulting from work and non-work vehicle trips by private and local government employees.

Policies:

2.1 Promote vehicle trip reduction and other transportation demand management programs.

The Porta Bella Specific Plan includes such transportation alternatives as a multi-modal transportation facility, escalator system, pedestrian linkages to all commercial areas, and mixed land uses to minimize the need for automobile trips.

2.2 N/A - pertains to carpools

2.3 Promote alternative transportation systems throughout the planning area, including, but not limited to, comprehensive bus service, bicycle and pedestrian trails, and associated support facilities.

The Porta Bella Specific Plan includes such transportation alternatives as a multi-modal transportation facility, escalator system, pedestrian linkages to all commercial areas, and mixed land uses to minimize the need for automobile trips.

2.4 N/A - pertains to programs for alternative transportation

2.5 Encourage programs which minimize local traffic congestion at large special events. Programs could include pre-purchase of parking tickets, staggering house of arrival, off-site parking with shuttles.

The 700+ car parking facility for the multi-modal transportation facility will only be utilized during weekday daytime hours for commuters. During the night-time and weekend hours, this parking area will be able to accommodate cars for special events and retail parking for the Town Center area. The convenient use of the escalator will connect the Soledad District in which the parking area is located, with the upper Town Center District, where the events and commercial opportunities area. This will allow two areas of parking where usually there is only one.

Issue 3: Peak -Period Truck Travel

Goal 3: To reduce emissions from peak-period truck travel and number and severity of truck-involved accidents.

Policies:

3.1 N/A - pertains to truck use hours

Issue 4: Parking Management

Goal 4: To reduce transportation source emissions by promoting efficient and creative parking plans which reduce vehicle emissions.

Policies

- 4.1 *The 700+ car parking facility for the multi-modal transportation facility will only be utilized during weekday daytime hours for commuters. During the night-time and weekend hours, this parking area will be able to accommodate cars for special events and retail parking for the Town Center area. The convenient use of the escalator will connect the Soledad District in which the parking area is located, with the upper Town Center District, where the events and commercial opportunities area. This will allow two areas of parking where usually there is only one, therefore reducing congestion and idling time.*

Issue 5: Traffic Flow Improvements

Goal 5: To reduce vehicle emissions through traffic flow improvements.

Policies:

- 5.1 Encourage the implementation of traffic flow improvements in order to reduce congestion, conserve energy, and improve air quality.

At Porta Bella, a hierarchy of streets exists. The regional road systems circumvent the community, collector roads proceed into the community, and local streets web throughout the neighborhoods. This system allows an efficient flow of vehicles.

Issue 6: Design Criteria

Goal 6: To reduce vehicle emissions through promotion of appropriate building and site design criteria

Policies:

- 6.1 Encourage new development, throughout the project review process, to incorporate appropriate building and site design criteria to minimize vehicular emissions, such as those resulting from on-site circulation patterns.

The Town Center area of Porta Bella is designed to utilize on-street and parking garages, which will reduce the amount of emissions by providing central and storefront parking.

Issue 7: Building Materials and Methods

- Goal 7: To reduce reactive organic gas (ROG) and particulate emissions from building materials and methods.

Policies:

- 7.1 N/A - pertains to ROG gases.

Issue 8: Energy Conservation

- Goal 8: To reduce emissions resulting from energy consumption in residential, commercial, and governmental facilities.

Policies:

- 8.1 N/A - pertains to retrofitting existing structures

- 8.2 Encourage the use of alternative energy sources

In Porta Bella, solar panels are discussed, encouraged and permitted.

- 8.3 Promote the use of landscaping, especially trees, to reduce heat buildup, save energy, and help cleanse the air.

Section 3 discusses the use of landscape as an energy conserver, as well as section 5, which discusses and encourages the use of landscape and hardscape to minimize energy use.

Issue 9: Air Quality Education

Goal 9: To increase public awareness regarding regional and local air quality issues

Policies:

9.1 N/A - pertains to education

Issue 10: Land Use

Goal 10: To reduce vehicle emissions by creating an urban form that efficiently utilizes urban infrastructure and services.

Policies:

10.1 Contribute to the reduction of vehicle miles traveled by achieving a more reasonable job/housing balance.

The Porta Bella Specific Plan area contains a commercial town center, office parks, business parks, various satellite commercial areas, and a multi-modal transportation system to promote public transit to and from the work place.

10.2 Encourage efficient transportation systems and land use patterns which minimize total trips and vehicle miles traveled.

The Porta Bella Specific Plan area contains a commercial town center, office parks, business parks, various satellite commercial areas, and a multi-modal transportation system to promote public transit to and from the work place.

Issue 11: Clean Vehicle Fuels

Goal 11: To reduce vehicle emissions by promoting the use of cleaner alternative fuels for vehicles.



Policies:

11.1 N/A - pertains to city vehicles

Issue 12: Transit Improvements

Goal 12: To reduce mobile source emissions by promoting a shift from single occupancy to higher occupancy vehicles.

Policies:

12.1 Encourage the development of local public transit and availability, improved bus service (time schedule, performance, and connections) and actions designed to make the transit system user friendly.

In addition to complying with the local transit agency's requirements upon development, the Specific Plan area has a multi-modal transportation facility which will accommodate other forms of transportation including public and private bus carriers, automobiles, bicycles and pedestrians.

12.2 Encourage the establishment of local and regional multi-modal transportation facilities

In addition to complying with the local transit agency's requirements upon development, the Specific Plan area has a multi-modal transportation facility which will accommodate other forms of transportation including public and private bus carriers, automobiles, bicycles and pedestrians.

12.3 Encourage the development of intercity transportation systems other than buses. Such systems may include light rail, mono-rail, people movers.

The Porta Bella Specific Plan encourages the implementation of the City's Village Center concept within the General Plan, which would link the Town Center area of this plan with the rest of the City via a intercity transportation system.

Issue 13: Particulate Emissions

Goal 13: To reduce particulate (dust) emissions

Policies:

13.1 N/A - pertains to implementation measures to reduce particulates.

Issue 14: Toxic Air Pollutant Exposure

Goal 14: To prevent exposure of people, animals, and other living organisms to toxic air pollutants.

Policies:

14.1 N/A - pertains to toxic air pollutant generators

Issue 15: Criteria Air Pollutant Exposure

Issue 15: To prevent exposure of residents and other sensitive receptors to non-toxic pollutants.

Policies:

15.1 N/A - pertains to pollution generation sources

15.2 N/A - pertains to coordination with SCAQMD.

8.0 NOISE ELEMENT

Issue 1: Noise Level control Standards

Goal 1: To protect the health and welfare of the residents of the City of Santa Clarita and the planning area by the elimination, mitigation, and prevention of significant existing and future noise levels.

Policies:

The policies of this section pertain to City regulation and enforcement of noise generators. The individual projects of Porta Bella will be subject to the standards imposed by the City.

Issue 2: Reduction of Noise from Traffic

Goal 2: To prevent and mitigate adverse impacts of traffic generated noise on the residents of the City and planning area.

Policies:

The policies of this section pertain to City regulation and mitigation of noise generators. The land use relationships of Porta Bella have been arranged in a method to minimize these impacts.

Issue 3: Noise Reduction in Residential Neighborhoods

Goal 3: To prevent and mitigate significant noise levels in residential neighborhoods above 60-65 dBA

Policies:

The policies of this section pertain to City mitigation of sound levels above 60-65 dBA. The individual projects and activities within Porta Bella will be subject to City Municipal Code requirements regarding sound.

Issue 4: Noise Reduction From Commercial and Industrial Activities

Goal 4: To prevent, mitigate, and minimize noise spillover from commercial/industrial uses into adjacent residential neighborhoods.

Policies:

The policies of this section pertain to noise generated from commercial/industrial uses near residential areas. Land uses within Porta Bella have been selected and arranged in a manner complimentary to surrounding land uses. All projects and activities in Porta Bella shall be subject to the City of Santa Clarita's Municipal Code regarding noise.

9.0 PUBLIC SAFETY ELEMENT

Issue 1: Development in Areas Subject to Risk From Natural Hazards

Goal 1: To minimize damage and hazards resulting from seismic activity, unstable soils, flooding conditions, and other geologic hazards.

Policies:

1.1 Ensure that all new projects have adequate water supply, road widths, and reasonable secondary emergency access to minimize health and safety risks.

Two new water tanks for water storage will be constructed on-site. Road widths have been designed with adequate width for their intended uses, and secondary access is available to all project areas.

1.2 N/A - pertains to an emergency preparedness plan to be prepared by City.

1.3 Require geotechnical studies for development proposals as appropriate.

A geotechnical study has been prepared as part of the design plan and the environmental impact report attached to this plan.

1.4 Work with the California Division of Mines and Geology to review development proposals located within or adjacent to the Alquist-Priolo Special Studies Zone, along the San Gabriel Fault and other potential active faults.

The geologic report prepared for the project is subject to review by the above referenced agencies.

1.5 N/A - pertains to City assistance in geotechnical investigation

- 1.6 Review the use of seismic design criteria and standards for linear system facilities, including, transmission lines, water and sewage systems, and highways to ensure that they are adequate in protecting the public. Actual weaknesses or limitations within the system should also be determined and mitigated where feasible.

All engineering within the project areas has been and will be conducted in compliance with earthquake safety standards.

- 1.7 Require that soils containing toxic or hazardous substances be cleaned up to the satisfaction of the agency having jurisdiction prior to the development or redevelopment.

Of the 17 toxic sites originally on-site, 15 have been cleaned, and the remaining two are in the process of being cleaned. Completion of this clean-up is anticipated by Spring of 1993.

- 1.8 N/A - pertains to City review of soils reports.

- 1.9 N/A - pertains to floodways

- 1.10 N/A - pertains to floodways

- 1.11 Take an aggressive stance on clean-up efforts of known contaminated areas.

A result of the development of Porta Bella will be clean-up of the toxic sites within this central community.

- 1.12 All structures should meet or exceed state required earthquake resistant design standards

All structures will meet or exceed state required earthquake resistant design standards.

- 1.13 Develop hillside grading standards to minimize the hazards of erosion and slope failure.

Section 5.0 of the Specific Plan has requirements and guidelines for grading within the project area.

Issue 2: Emergency Preparedness

- Goal 2: To prepare the Santa Clarita planning area to be self-sufficient in the event of major emergency or earthquake.

Policies:

The policies of this section pertain to the City's creation of earthquake preparedness plans.

Issue 3: Fire Hazards

- Goal 3: To minimize potential damage and hazards resulting from fire.

Policies:

- 3.1 N/A - pertains to the creation of a City ordinance

- 3.2 All new development must be served by a water system that meets the fire flow requirements established by the fire department

All new development in Porta Bella will be served by such a system, and be subject to the requirements of the fire department.

- 3.3 Require all public and private roadways to be constructed according to the minimum standards provided for in the General Plan to ensure that vehicular access for emergency vehicles can be maintained.

All roadways within Porta Bella have been designed to accommodate emergency vehicles.

3.4 N/A - pertains to city service for fire protection

3.5 Provide fire resistant landscaped buffer zones between high risk fire hazard areas and urban development, and restrict access from development in to the wilderness areas during periods of high fire risk

Section 5.0 of the Plan contains a "fuel modification section" which requires a buffer zone of 150 feet between development and natural areas. This buffer zone shall be planted with approved plant materials contained in the Master Plant Palette.

3.6 All new development proposals near the designated wildfire hazard zones should identify evacuation/emergency routes.

All development within the project area is serviced by a number of access routes which can be taken in case of emergency.

3.7 Development in or adjacent to wilderness/chaparral areas should have a fuel modification zone to minimize the risk of wildfire as appropriate. fuel modification areas should be encouraged in the fores areas when adjacent to residential development.

Fuel modification areas are detailed in Section 5.0 of the Plan.

3.8 Encourage dual access, particularly in mountainous and high fire risk areas.

Emergency dual access is provided to all neighborhoods of Porta Bella.

3.9 N/A - pertains to a regional fuel modification plan.

3.10 N/A - pertains to existing development

Issue 4: Hazardous Materials

Goal 4: To minimize levels of risk to people and property from hazardous waste.

Policies:

- 4.1 N/A - pertains to city coordination with fire department
- 4.2 N/A - pertains to the transportation of hazardous materials
- 4.3 N/A - pertains to a hazardous waste program to be created by the City.
- 4.4 N/A - pertains to city prohibition of hazardous waste generating facilities.
- 4.5 N/A - pertains to electromagnetic effects of high tension lines.

10.0 COMMUNITY DESIGN ELEMENT

Issue 1: Protection or Neighborhood Identity

GOAL 1: To protect and preserve the scale and character of existing neighborhoods while providing for new development which is consistent with the goals and policies of the General Plan.

Policies:

- 1.1 Maintain or enhance the character of existing neighborhoods with policies and regulations that emphasize compatible architecture and landscaping.

N.A. - Pertains to existing neighborhoods

- 1.2 Ensure that clustering of new development is compatible with the character of the existing surrounding neighborhoods.

The design character of Porta Bella incorporates traditional ideals which are found within the existing communities of Santa Clarita.

- 1.3 Consider all design elements, including building size, height, mass and architectural design in the design review process so that new development does not conflict with the character of the neighborhoods.

A detailed design guidelines and standards section is included within the Specific Plan. The prescribment of standards is intended to reflect and compliment the existing scale of Santa Clarita.

Issue 2: Design Concepts and Quality for the Community

GOAL 2: To encourage design excellence in the development of all public and private projects in the city.

Policies:

- 2.1 Identify important design and aesthetic attributes that contribute to the unique character of the City.

The first phase of the design process for Porta Bella was to identify significant natural, physical and cultural features which shape Santa Clarita. For this reason, canyons and ridgeline were treated with respect to their environment, and the urban form was created in a fine grain manner consistent with the existing communities of Porta Bella.

- 2.2 Provide for residential uses in proximity to business/commercial centers in a manner which promotes the neighborhood/village/town center planning concept.

The communities of Porta Bella offer diverse setting, ranging from above commercial units to single family estate lot. Three commercial opportunities have been planned into Porta Bella, as well as numerous employment and recreational opportunities.

- 2.3 Promote opportunities for greater pedestrian orientation and lifestyles.

Human-scale streets with pedestrian parkways, trails which weave through the neighborhoods and link recreational amenities, and a fine grain mixture of land uses are the primary elements of Porta Bella which will result in a pedestrian oriented community.

- 2.4 Encourage key gateway design themes to the City's major communities consistent with the overall community image.

A landmark monumentation concept has been established in Porta Bella which relies on focal landscapes and relevant architectural expressions, rather than bulky signage, to identify communities. This concept is consistent with the integrated philosophy of the community's design.

- 2.5 Encourage the establishment of design themes while avoiding monotony within individual developments in the City.

The "New Heritage" architecture of Porta Bella is an eclectic mix of architectural design styles which compliment each other, however allow a diversity of character.

- 2.6 Prepare and adopt design guidelines for the City and the individual communities that comprise the City.

Within the Specific Plan, design guidelines have been prepared for City adoption to guide the eventual development of Porta Bella.

- 2.4 Permit non-conforming uses, as appropriate, for buildings of historical and/or architectural significance.

N.A. - No structures exist on-site which are historically significant.

Issue 5: Natural Resources Preservation

- GOAL 5: To preserve and integrate the prominent and distinctive natural features of the community as open space for the use and visual enjoyment of all City residents.

Issue 3: Treatment of Commercial Districts

- GOAL 3: To promote design excellence in the development of business/commercial centers.

Policies:

- 3.1 Improve the appearance and function of business and commercial centers within the planning area through architectural form, landscaping, parking and signage schemes.

The Porta Bella Specific Plan contains design standards for both architecture and landscape architecture, parking and sign standards. These standards will better ensure a high quality environment for this community.

- 3.2 Promote the development of a commercial business and retail City center through appropriate architectural form which encourages a unified theme and a strong sense of place.

The Town Center District of the Porta Bella, and Santa Clarita, will be one of the most distinctive and contextually appropriate City Centers in Southern California. The character of the Town Center draws from an eclectic mix of architectural styles, all of which are complimentary to the existing communities. The pedestrian level opportunities for entertainment, retail, office and residential all within the Town Center will create a unique sense of place within Southern California.

- 3.3 Encourage the establishment of mixed use and village commercial centers throughout the planning area and provide opportunities for plazas, urban open spaces, and the effective use of street furniture in downtown areas.

Within the Town Center and Soledad Districts of Porta Bella will be mixed use areas including commercial, multi-modal transportation, recreation, residential, office and entertainment. The areas will be strongly pedestrian oriented with features such as the Town Center Circle, the romantic Crescent Walk, a storefront Main Street, sidewalk cafes and many more elements which reinforce the pedestrian experience.

- 3.4 Encourage design and uses of commercial districts and related housing that add pedestrian orientation and that provide for safe and secure daytime and nighttime activities, i.e., the Newhall Historic area and the City Center.

Within the Town Center and Soledad Districts of Porta Bella will be mixed use areas including commercial, multi-modal transportation, recreation, residential, office and entertainment. The areas will be strongly pedestrian oriented with features such as the Town Center Circle, the romantic Crescent Walk, a storefront Main Street, sidewalk cafes and many more elements which reinforce the pedestrian experience.

- 3.5 Promote the development of a hierarchy of commercial and related residential uses throughout the planning area.

Porta Bella is comprised of four inter-related Districts, offering a broad range of lifestyle opportunities. Commercial support for residential and community functions is a fundamental planning objective. The manner in which the commercial support is provided in Porta Bella ranges from direct interaction within the Town Center District, to various degrees of interaction within the Soledad, Central and South Districts. All areas, however, have convenient access to commercial areas without further congesting existing arterials or infrastructure.

- 3.6 Encourage the provision of buffering in areas near commercial centers and residential neighborhoods to help separate and delineate business and residential districts and to create visual diversity.

Buffering, where appropriate, is included within Porta Bella. For instance, the Neighborhood Commercial area within the South District is buffered by a 100 foot landscape easement between the neighboring Multi-Family residential area.

Issue 4: Historic Buildings and Features

- Goal 4: To continue to preserve and maintain special historical Features and landmarks as Focal points in the planning area.

Policies:

- 4.1 Encourage design measures for new development in historic areas, such as requiring adequate physical and visual buffers between historical areas and other land uses, and the use of compatible or similar construction materials and architectural styles so as not to detract from the integrity of historical features.

N.A. - Pertains to existing historical areas.

- 4.2 Work to preserve and maintain historic neighborhoods and reinforce the historic theme by requiring new development to be constructed in such a manner so as not to distract from the significant historic structures.

N.A. - Pertains to existing historic neighborhoods.

- 4.3 Allow flexibility in applying building codes to buildings of historical and/or architectural significance.

N.A. - Pertains to existing historic structures.

Policies:

- 5.1 Retain designated major landforms, such as ridgelines, natural drainage ways, streams, rivers, valleys, and significant vegetation, especially where these features contribute to the overall community identity.

The ridgeline, as well as Oakdale and Oro Fino Canyons will be retained. In addition, the canyons will be rehabilitated with riparian plant species which have been removed for the prior land use activities and grading. The ridgeline will be resculpted to reflect its natural condition, prior to the grading activities of the past land use.

- 5.2 Encourage appropriate development guidelines for new development located in view corridors or near prominent/unique landforms.

All development within Porta Bella is located away from major ridgelines. A secondary ridgeline will be home to the new town center, which is appropriate due to its symbolic and physical contribution to Santa Clarita.

- 5.3 Where possible, incorporate attractive natural amenities, such as rock outcroppings, vegetation, streams, and drainage areas, into the development of future projects to protect the environment and provide landscape opportunities, visual interest, scale and/or recreational opportunities.

The natural environment is the greatest asset for the new community of Porta Bella. The incorporation of the significant elements into the fabric of the community is fundamental to the design. In addition to the ridgeline, canyon preservation, there is the crescent bowl walkway, the ridge-runner trail system, and the over 42% of land in open space, parks and recreation, and even the conceptual grading plan of the adjacent City property which preserves the heritage oak tree.

Issue 6: Open Space Areas/Park Design

GOAL 6: To protect and enhance open space areas that provide visual and aesthetic character and identity to the community.

Policies:

6.1 Establish programs and ordinances that will be effective in providing visual relief and separation between development and parks.

In Porta Bella, some parks and recreation areas are located adjacent to residential areas. These areas are more passive in nature, less inviting to organized sports. The major active sports park is located on the opposite side of the ridge, which will buffer the community from the noise and lights of the community park. Access is still convenient, however, with the provision of trails and parking at the park.

6.2 Promote open areas, such as plazas, interior arcades, galleries, rooftop gardens, and scenic viewplaces within intensive urban developments.

These elements are encouraged within the commercial areas of the Specific Plan via the design guidelines section.

6.3 Establish recreational areas for both passive and active activities.

Porta Bella is comprised of over 42% open space, parks and recreation areas. Included in Porta Bella are active sports fields of 5, 3.5 and 22 acres in size, a public tennis club with stadium court, a golf driving range, an elementary school with an attached playground, a natural trail system, and various neighborhood parks within the various districts.

- 6.4 Develop a park classification program (regional, community, neighborhood/local, pocket/mini parks) which conforms to community recreation needs and encourages community identity throughout the planning area.

N.A. - Pertains to the City creating park designations.

- 6.5 Promote the concept of a network of neighborhood parks and open space areas; where possible integrate neighborhood parks with a larger community wide system; incorporate jogging and hiking trails, bicycle path, and equestrian trails links wherever possible.

Porta Bella is interwoven with parks and connective trail systems which conveniently link residences and commercial areas with the extensive system of parks, recreation and open space. The opportunity for recreation and pedestrian mobility is a fundamental concept of Porta Bella.

- 6.6 Promote the use and development of open space areas tied to the Santa Clara River Corridor.

N.A. - pertains to the Santa Clara River Corridor

- 6.7 Promote visual and physical buffers, where appropriate, by use of easements, roadways, trails, ridgelines, and other features to delineate various communities in the valley.

Porta Bella is an extension of the existing communities of Santa Clarita. Effort has been made to integrate, rather than separate its opportunities within the city. Geographically, Porta Bella has a physical separation from the existing communities, which will allow the character of each community to remain individually unique.

- 6.8 To the extent possible, promote the development of equestrian trails in river and stream channels and other open space areas away from urbanization and to connect with trails in the National Forest.

Several "General Purpose" trails are located within the Specific Plan area, which include their use for equestrian purposes. These General Use trails are strategically located to connect with similar trails depicted by the General Plan.

Issue 7: Circulation System Design

- Goal 7: To develop a safe and efficient circulation system that protects and enhances the overall community character.

Policies:

- 7.1 Develop design principles for major roadway types which are consistent with roadway function and which address roadway improvements, landscaping, aesthetics, roadway signage, lighting, and pedestrian enhancements.

The roadways within Porta Bella are designed to the capacity standards as recommended by the traffic engineers, yet they are of the character and arrangement which promotes the human-scale character desired.

- 7.2 Encourage and enhance identifiable entryways for the overall community, individual residential neighborhoods, and unique or principal business/commercial districts of the City.

These entryways have been identified and their character described within various sections of the Specific Plan. The character is that of minimal signage, but ample landmark identification such as landscape and monumentation.

- 7.3 Encourage the protection of view windows along designated scenic highway corridors.

N.A. - Pertains to scenic highway corridors, which do not occur on site. Santa Clarita Parkway, although not officially designated as a scenic highway, will display exemplary aesthetic characteristics in its effort to blend with the Native landscape environment.

- 7.4 Roadways in hillside areas should be developed in accordance with special standards to ensure roadway design consistent with topography and sensitive to local relief.

Roadways within Porta Bella have been designed to minimize grading and aesthetic impacts, yet provide an adequate measure of safety. The terrain of Porta Bella is different than most other areas of Santa Clarita, and therefore their design has been tailored to fit the conditions of the site.

Issue 8: Signage and Billboards

- Goal 8:** To ensure that signage throughout the City is visually attractive and minimizes distraction.

Policies:

- 8.1 Adopt a comprehensive sign ordinance.

A comprehensive sign program has been included in the specific plan.

- 8.2 Work to prohibit billboards along freeway frontages and designated scenic highways.

Billboards are prohibited within the Specific Plan area.

- 8.3 Encourage distinctive signage which identifies principal entries to the City, unique districts, neighborhoods, and public buildings and parks.

These entryways have been identified and their character described within various sections of the Specific Plan. The character is that of minimal signage, but ample landmark identification such as landscape and monumentation.

- 8.4 Prohibit new billboards in the City and encourage the reduction of the number of existing billboards.

Billboards are prohibited within the Specific Plan area.

- 8.5 Prohibit private signs in the right-of-way.

Private signs are prohibited in the public right-of-way except for restaurant menu boards.

Issue 9: Landscape Architecture

- Goal 9: To promote superior landscape design which emphasizes aesthetics, function, and water conservation.

Policies:

- 9.1 Encourage landscaping around residential, commercial, and industrial buildings and parking areas to enhance views from roadways and surrounding uses.

Landscape is required within all of the above mentioned areas. Landscape within the community is viewed as the underlying character unification theme for Porta Bella.

- 9.2 Utilize landscaping techniques to screen incompatible land uses and create transition and buffer zones between conflicting use areas

Landscape for use as a buffer is illustrated in the specific plan, particularly between the Neighborhood Commercial center in the South District, and again between the community park in the Central District. The tennis facility is the buffer between the rail tracks and the multi-family area in the Soledad District.

- 9.3 Encourage major landscape themes to provide visual relief in highly urbanized areas.

The focal area of the Town Center is the landscaped Town Circle roundabout. This focal landscape element will be the physical center of the Town Center.

- 9.4 Develop landscape themes to accentuate the major public gateways to the City.

These entryways have been identified and their character described within various sections of the Specific Plan. The character is that of minimal signage, but ample landmark identification such as landscape and monumentation.

- 9.5 Develop city-wide landscape and tree planting guidelines which promote low maintenance, drought-tolerant and fire-resistant species.

Within Porta Bella, landscape materials have been diligently selected for their overall low water needs. California Peppers, oaks and native shrubs and groundcovers have been extensively utilized within the design. Areas which utilize have been kept to the very visual and residential streetscapes to get the most benefit from the least amount of turf.

- 9.6 Encourage incorporation of indigenous vegetation into landscape themes throughout the planning area.

The landscape palette of Porta Bella is based upon the indigenous character of the Valley. Heavy reliance on traditional Southern California plant materials is a part of the character.

- 9.7 Encourage incorporation of indigenous landscape materials such as native stone, river rock, and Bouquet Canyon Stone into landscape themes.

The specific plan encourages the use of monumentation rather than signage for entries and focal areas. The use of the above mentioned materials could be incorporated into these monument and focal elements.

- 9.8 Promote sensitive landscape treatment of fuel modification zones between development and open space areas.

Fuel modification zones have been identified and a plant palette prepared to specifically blend the landscape zone with the existing vegetation, yet provide fire protection for the improvements.

- 9.9 Encourage consistent application of materials and vegetation within communities and differentiate between communities.

An overall Landscape Plan has been created which is intended to create an underlying unity to Porta Bella, with modifications to individual neighborhoods for interest.

- 9.10 Promote the establishment of landscape maintenance districts or other methods to maintain open space and slope areas around residential areas.

Associations will be created which will maintain the landscape and lighting elements of the community.

Issue 10: Architecture

- Goal 10: To achieve architectural themes and form which promote human scale and provide a comfortable human interaction with buildings.

Policies:

- 10.1 Provide design flexibility for urban design and architectural concepts in order to avoid architectural monotony and lack of design innovation.

The New Heritage architectural theme is an eclectic mix of design characters which promote creativity and imagery in a compatable parameter of design.

- 10.2 Encourage the use of materials that complement adjacent buildings and their surroundings.

The use of indiginous materials such as Bouquet Canyon Stone and similar materials has been encouraged within the specific plan. A detailed list of building materials has also been delin-eated.

- 10.3 Encourage design solutions that consider physical scale of the area and adjacent buildings.

A list of elements which include the proposed structures rela-tionship with adjacent buildings is included within the plan. The scale of the structures within the community has been limited to create appropriate scale and context.

- 10.4 Examine potential opportunities for community theme ele-ments within individual residential areas, neighborhood cen-ters, recreation centers landscaped street medians, and other community facilities.

The specific plan encourages the use of monumentation rather than signage for entries and focal areas. The use of the above mentioned materials could be incorporated into these monu-ment and focal elements.

- 10.5 Encourage visual hierarchy and distinction between community uses reflecting increasing densities, land use, and scale of development.

Land uses within the community have been blended in the spirit of the traditional American town, with emphasis on the pedestrian rather than the automobile. Sound planning principles have also been incorporated to ensure the compatibility of uses with neighboring uses.

Issue 11: Infrastructure

- Goal 11: To achieve a coordinated and efficient infrastructure system which is visually unobtrusive while designed to meet the current and future needs of the planning area

Policies:

- 11.1 Encourage placement of transmission power lines and other mechanical equipment underground, where feasible, to maximize safety and minimize visual distraction.

Power lines within the plan area will be located underground.

- 11.2 Require that new electrical, telephone, cable television, mechanical equipment and other distribution lines be placed underground.

These lines will be placed underground.

- 11.3 Require that all on-site connections and utilities are installed underground where practical.

These lines will be placed underground.

- 11.4 Ensure that, where practical, utilities and connections which are located above ground do not interfere with or adversely impact access, visibility appearance, or the character of the structures near which they are located.

No lines will be located above ground.

- 11.5 Develop coordinated planning programs to ensure the efficient placement and consolidation of utility facilities within new development.

N.A. - pertains to governmental coordination.

- 11.6 Incorporate landscaping, undergrounding, berms, and other techniques and design measures to integrate public facilities, such as water tanks into the community design.

The water tanks will be visually reduced in scale by the use of berms surrounding the tanks. These berms are also a safety factor in the event a tank ruptures, the water will flow away from the homes.

- 11.7 Encourage design solutions that reduce impacts/constraints from railroad rights-of-way within the planning area.

The rail line will be overpasses by Santa Clarita Parkway, and a pedestrian underpass will be incorporated within the Soledad District.

- 11.8 Encourage the use of the land under high power transmission lines for landscaping, tree farms, additional safe recreation areas, and other appropriate feasible uses.

N.A. - No high-power transmission lines exist within Porta Bella.

- 11.9 Encourage single pole transmission towers and cellular poles, and avoid unsightly reinforced structural support bases.

N.A. - These elements will not be located within Porta Bella.

11.0 PUBLIC SERVICES, UTILITIES AND FACILITIES ELEMENT

Issue 1: Adequacy of Public Services, Facilities, and Utilities

Goal 1: Work with utilities and other service providers to ensure adequate and safe public infrastructure and public services for City residents, including upgrading and expansion of existing deficient systems serving new development.

Policies:

- 1.1 N/A - pertains to city cooperation with other agencies
- 1.2 N/A - pertains to city cooperation with other agencies
- 1.3 N/A - pertains to city recordation of service needs
- 1.4 N/A - pertains to Development Monitoring System to evaluate cumulative impacts.
- 1.5 Require that new development be prohibited or delayed unless necessary public services and utilities will be available at the time of occupancy or will be provided within a reasonable period of time as a part of an adopted improvement plan.

Development within Porta Bella will be phased to construct infrastructure prior to development.
- 1.6 N/A pertains to City preparation of Capital Improvements Plan
- 1.7 N/A - pertains to City cooperation with water purveyors
- 1.8 Promote water conservation and reclamation in order to reduce water consumption in existing and future developments.

Development within Porta Bella will set the standard for water conservation in Santa Clarita. All residential development will be required to utilize water conserving fixtures, and Sections 5.0 and 10.0 encourage the use of water recharge mechanisms within the landscape and grading concepts.

- 1.9 N/A - pertains to City trash collection
- 1.10 N/A - pertains to a City-Wide recycling program. Recycling collection points including the collection of household hazardous wastes are permitted by conditional use permit within all commercial areas of Porta Bella.
- 1.11 Develop a drainage master plan that is sensitive to environmental concerns.

Section 10.0 outlines such a plan.
- 1.12 N/A - pertains to city cooperation with other agencies.
- 1.13 N/A - pertains to city media potentials
- 1.14 N/A - pertains to the County library system. A library is proposed by the City on the adjacent parcel to the north of the project site.
- 1.15 N/A - pertains to city maintenance of services. A fire station site is available on-site, and a police station and courthouse are proposed for the adjacent property.
- 1.16 N/A - pertains to city education efforts.
- 1.17 Support the school districts in promoting a high standards of excellence in the local school system.

The school site in Porta Bella is located in an excellent area for community integration and education. This site is 10 acres with an adjacent 5 acre park, as well as being located next to the educational riparian habitat area.
- 1.18 Work and cooperate with school districts and developers to ensure appropriate means to facilitate the development of school facilities to accommodate growth and ensure that the school districts can meet future needs.

The school site in Porta Bella is located in an excellent area for community integration and education. This site is 10 acres with an adjacent 5 acre park, as well as being located next to the educational riparian habitat area.

- 1.19 Enhance the level and quality of community services and facilities, and improve accessibility throughout the Santa Clarita Valley.

Porta Bella enables a high quality setting for cultural, entertainment, office, service, commercial and residential uses, as well as support facilities such as a school, houses of worship, fire station, and park lands. The regional roads proposed within Porta Bella will both provide better accessibility throughout Santa Clarita and quality environmental character on the city hall site.

Issue 2: Allocation of Service, Facility, and Utility Costs

Goal 2: To allocate the cost of public services, facilities, and utilities on a fair and equitable basis based on service demand generated and benefits derived from services/improvements

Policies:

- 2.1 Make use of Specific Plans and development agreements that specify the nature, timing, cost, and financing mechanisms to be used to fund improvements and services.

The Porta Bella Specific Plan, EIR, and subsequent development agreements will specify the above listed items.

2.2 N/A - pertains to City funding mechanisms for improvements

2.3 N/A - pertains to the City fiscal planning computer model.

2.4 Support funding of infrastructure improvements that are consistent with the City's General Plan.

Infrastructure within Porta Bella is consistent with the City's General Plan.

Issue 3: Compatibility of Land Use and Infrastructure Services

Goal 3: Ensure that all public infrastructure improvements are compatible with nearby development

Policies:

There are no policies listed in the General Plan regarding this goal. Infrastructure within the Specific Plan areas has been and will be designed to be harmonious with and/or alleviate the pressures upon existing infrastructure.

Issue 4: Long Range Financial Planning

Goal 4: To prepare a long range financial plan which would contain both an operating and capital plan and be updated on a regular basis

Policies:

The policies of this section pertain to the City's long range financial plan.

Issue 5: Safety of Public Services, Utilities, and Facilities

Goal 5: To ensure that all public services, utility systems, and facilities are designed and maintained as stated in the Goals and Policies section of the Public Safety element to provide acceptable levels of safety and security.

Policies:

5.1 Promote the safe use of toxic materials and their safe disposal as outlined in the Goals and Policies section of the Public Safety Elements.

Refer to the Public Safety Element

- 5.2 Promote the collection of toxic materials and the establishment of recycling center for the safe disposal of toxic substances for residential and commercial use.

Household hazardous waste and general recycling collections points are conditionally permitted in all commercial areas of Porta Bella.

- 5.3 N/A - pertains to public education
- 5.4 N/A - pertains to city support for environmental laws
- 5.5 N/A - pertains to existing toxic materials generators.

12.0 HUMAN RESOURCES ELEMENT

Issue 1: Human Services and Facilities Required to Service Existing and Future Development

Goal 1: To provide adequate social service programming for existing and Future residents responsive to the needs of diverse populations, including, but not limited to, families with children, seniors and the frail elderly, minorities, persons with disabilities, immigrants, and the homeless.

Policies:

The policies of this section are not applicable to the Porta Bella Specific Plan, as they refer to programs to be established by the City of Santa Clarita.

Issue 2: Health Care Services and Facilities

Goal 2: To promote the provisions of a broad range of high quality health care services to meet the existing and future needs of City residents.

Policies:

The policies of this section are not applicable to the Porta Bella Specific Plan, as they refer to programs to be established by the City of Santa Clarita.

Issue 3: Cultural Opportunities

Goal 3: To encourage the development of a wide range of community and cultural activities throughout the planning area.

Policies:

The policies of this section are not applicable to the Porta Bella Specific Plan, as they refer to programs to be established by the City of Santa Clarita.

Goal 4: To encourage improved educational and training opportunities and services for people throughout the planning area.

Policies:

The policies of this section are not applicable to the Porta Bella Specific Plan, as they refer to programs to be established by the City of Santa Clarita.

end.